

WOODLAND RESEARCH & TECHNOLOGY PARK

Specific Plan

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Lead Agency:
City of Woodland
300 First Street
Woodland, CA 95695





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ES | EXECUTIVE SUMMARY







ES.1 | EXECUTIVE SUMMARY

The Woodland Research and Technology Park (WRTP) Specific Plan guides the development of the 350 acre Plan Area in Woodland, California. The Specific Plan Area is envisioned within the City’s 2035 General Plan as a natural extension of the city, ideally located and positioned for jobs and housing development.

The WRTP Specific Plan represents the opportunity to implement the collective vision of the General Plan, the community and City leaders, and the applicants to create a sustainable, healthy and successful community in the southern gateway to the City of Woodland. The establishment of the mixed use Plan Area is a natural progression of the City’s long history of innovation and creativity in food and agricultural productivity.

The Plan Area is a self-sustaining, enterprising mixed use community anchored by a technology and innovation campus, with vibrant retail and supporting uses, integrated residential neighborhoods, recreation, open space and public uses. The projected yields, at full build out, would provide up to approximately 2.2 million square feet of employment equating to approximately 5,000 jobs and 1,600 dwelling units in a range of housing types.

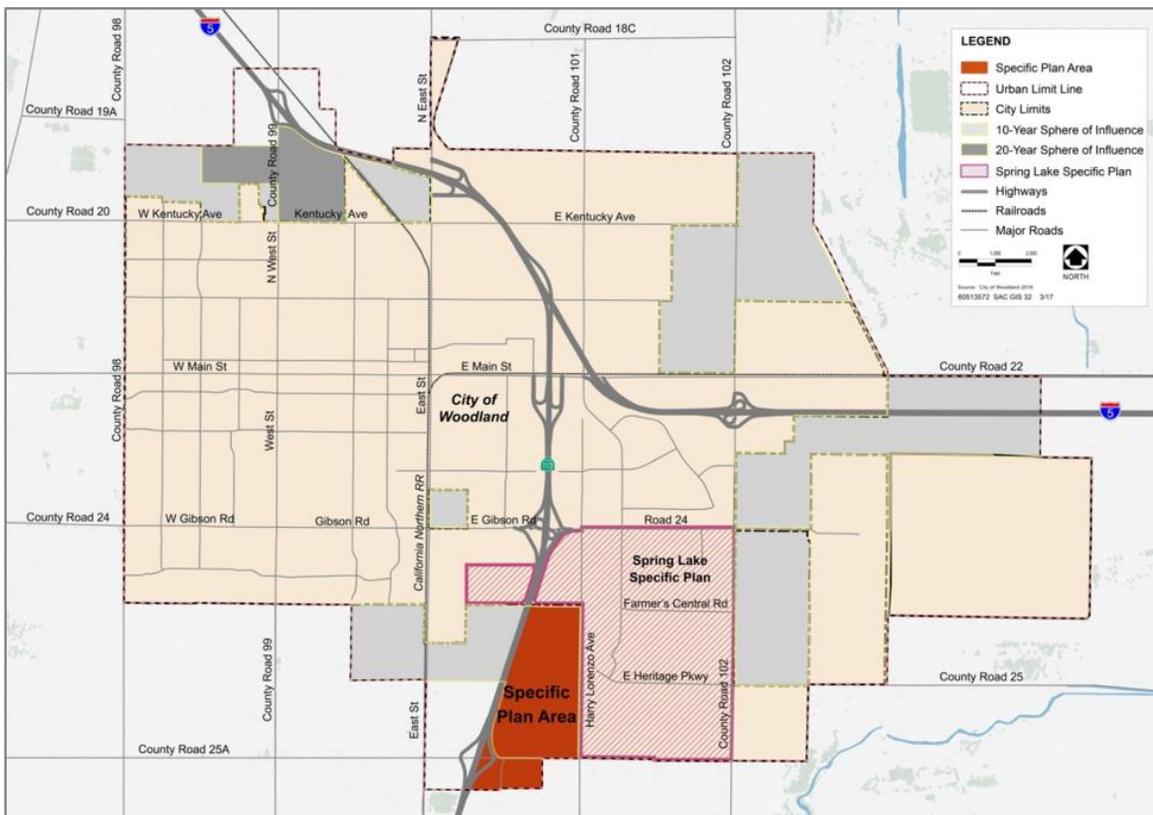


EXHIBIT ES-1: PLAN AREA LOCATION

This specific plan provides an opportunity to creatively implement the intent of the General Plan and serve as a refinement of General Plan policies. The WRTP Specific Plan is the primary land use, policy and regulatory document used to guide development of the Plan Area. The Specific Plan establishes a development framework for land use, community design and character, infrastructure improvements and a subsequent project approval structure for orderly development within the Plan Area. The Specific Plan document is organized as follows:

- **Chapter 1-Introduction and Vision:** identifies the vision and organizing principles that guide the physical form and development patterns of the Plan Area.
- **Chapter 2-Land Use Framework:** presents the Specific Plan Guiding policies, depicts the overall land use plan and districts that constitute the Plan Area.
- **Chapter 3-Land Use Regulations, Development Standards and Design Guidelines:** defines the permitted uses, development standards, regulations and provides design recommendations and guidance for individual projects.
- **Chapter 4-Mobility and Circulation:** describes the network to accommodate the movement of vehicles, pedestrians and bicyclists.
- **Chapter 5-Public Utilities and Services:** describes plan-wide utilities of water, wastewater, storm drainage, electric, natural gas, communications and the public services of parks, schools, law enforcement, fire and solid waste.
- **Chapter 6-Implementation:** describes implementation procedures and strategies for financing and maintenance of public facilities and services.
- **Chapter 7-Administration:** outlines the Specific Plan process and its relationship to the subsequent entitlement processes, describes the administrative procedures to implement and amend, interpret and enforce the Specific Plan.
- **Supporting Documents:** Technical documents/studies provide supporting analysis and detail for many aspects of the Specific Plan, including:
 - Water Technical Memorandum
 - Reclaimed Water Distribution Technical Memorandum
 - Wastewater Collection System Technical Memorandum
 - Stormwater Management Technical Memorandum
 - Traffic Impact Analysis
 - Public Facilities Financing Plan
- **Related City Documents**

Related documents include, but are not limited to:

 - Environmental Impact Report
 - General Plan and Climate Action Plan
 - Zoning Code and Subdivision Ordinance
 - Improvement Standards
 - Development Agreement and Public Infrastructure Reimbursement Rules



1.0 | INTRODUCTION / VISION





1.1 | INTRODUCTION

The Woodland Research and Technology Park (WRTP) Specific Plan guides the development of the 350 acre Plan Area in Woodland, California. The Specific Plan Area is envisioned within the City's 2035 General Plan as a natural extension of the city, ideally located and positioned for jobs and housing development. The establishment of the mixed use Plan Area is a natural progression of the City's long history of innovation and creativity in food and agricultural production.

The foundation for the Specific Plan is defined by a growing recognition that the factors that drive economic development and future growth are rapidly shifting. With the rising importance of innovation and technology development, a region's economic competitiveness increasingly depends on its ability to establish a high-quality system that fosters innovation and promotes the development, transfer, and commercialization of technology. Such innovation ecosystems provide environments in which emerging and existing technology companies can be incubated and grown, researchers and companies can collaborate in a synergistic environment, and access to scientific, engineering, and entrepreneurial talent can be easily facilitated.



**WOODLAND RESEARCH
AND TECHNOLOGY PARK**
BUILDING ON OVER 100
YEARS OF FOOD AND AG
INNOVATION IN THE CITY
OF WOODLAND

This chapter introduces the Vision for the Plan Area, as influenced by the City's General Plan, the physical attributes of the site and the strengths and opportunities of Woodland and the region. Following the detailed vision, this chapter outlines the Guiding Principles, which serve as the guide for the desired outcome of development within the Plan Area. Next is Plan Area Context, which provides the background and setting. The final sections provide the organization for the Specific Plan, lists the Related Documents and provides the list of Entitlements which were approved in conjunction with the Specific Plan.

1.2 | VISION

The Woodland Research and Technology Park will serve as a gateway to connect Woodland, a community that has shaped the future of food and agriculture for over a century, with UC Davis, one of the leading agricultural research universities in the world, through the creation of an enterprising mixed use community. The Plan Area is anchored by a technology and innovation campus, with vibrant businesses and integrated residential neighborhoods. A dynamic “work-live-play” environment will attract highly skilled professionals to the region and will provide new housing opportunities and a long-term economic base for Woodland’s future generations. The Plan Area will be a place where the existing strengths of the City are combined with the opportunities for the future, capitalizing on the technology transfer and research potential the Plan Area’s location and base economy provide.

“Yolo County is a global hub for agricultural research, food production and sustainable technology. The proximity of this site, just five minutes from UC Davis, will offer appropriate facilities for technology startups to put down roots.”

**Lon Hatamiya, former Secretary of Technology, Trade
and Commerce for the State of California**

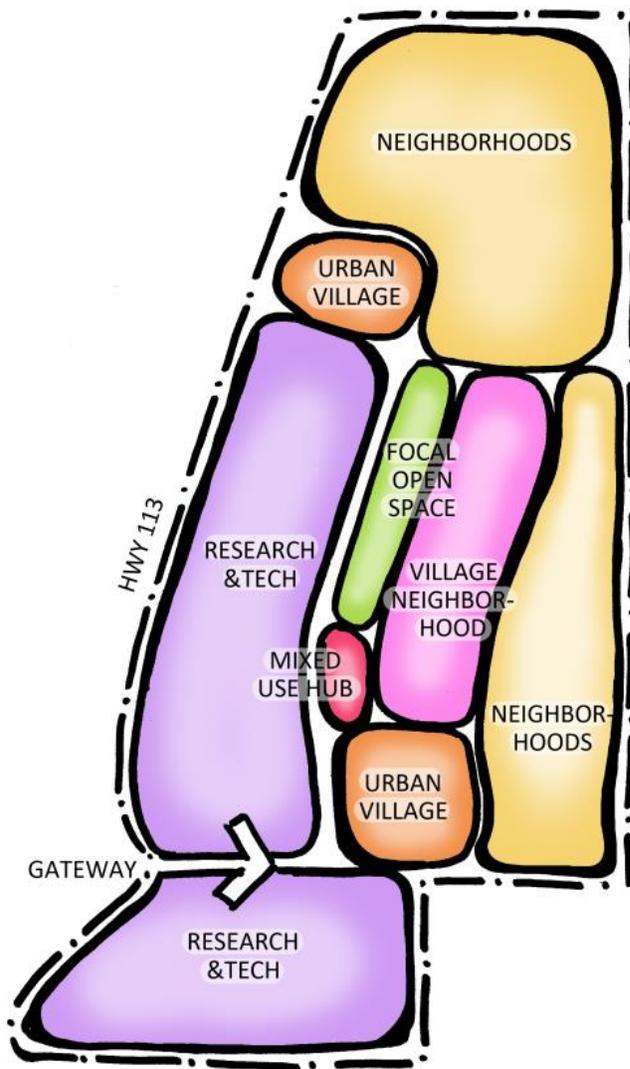


Northern California's agricultural innovation is anchored by the leading edge research and development at UC Davis – the nation's premier agricultural research institution.



This vision and framework responds directly to the 2035 General Plan, Policy 2.L.2, which describes the Plan Area as...“ a mixed-use residential district anchored by a research and technology business park in the Southern Gateway area at CR 25 and SR 113. Concentrate the highest intensity of development within and in close proximity to the business park area, with lower density, largely residential uses to the north.” The development framework of the Plan Area was guided by this policy, translated onto the site to achieve the desired physical form, as shown in Exhibit 1-1, Vision Concept Diagram.

The research/technology campuses are strategically placed on the west edge of the Plan Area to serve as a gateway, the central core provides a village-center focal area with a mix of uses at higher densities (the “urban villages”), and the neighborhoods wrap the east and north edges to provide a compatible transition to the Spring Lake neighborhoods. This framework provides a distinct identity, a well-defined pattern and gradual transitions to the adjacent uses. The three primary districts which evolved from the vision; the Technology Park, the Village Center and The Villages, are introduced in the following section. Chapter 2 provides details on the Land Use Plan and Districts.



provide a compatible transition to the Spring Lake neighborhoods. This framework provides a distinct identity, a well-defined pattern and gradual transitions to the adjacent uses. The three primary districts which evolved from the vision; the Technology Park, the Village Center and The Villages, are introduced in the following section. Chapter 2 provides details on the Land Use Plan and Districts.

This Specific Plan provides the physical and policy framework to implement the vision and serves as a guide for the intentional development of the Plan Area.

More detail on the land use plan and districts is provided in Chapter 2.

EXHIBIT 1-1: VISION CONCEPT DIAGRAM

TECHNOLOGY PARK



Envisioned as a state-of-the-art technology hub specializing in emerging agricultural technologies and related research in energy, transportation and environmental sciences, the 120 acre research and technology campus will be a place of collaboration and exchange within a campus-like setting. Diverse public and private entities interested in collaborative innovation will be drawn to the unique setting provided in the Plan Area, a place where diverse business, job training, and new products, technologies and innovation can take root.

VILLAGE CENTER

The 45 acre Village Center is envisioned as the heart of the Plan Area, serving as the essential “third place” for residents, workers and employers. Outdoor retail displays, restaurant patios and sidewalk cafés will spill out on to open-air hardscape plazas. An 11-acre park “The Yard” framed by mid-rise townhomes and urban-style lofts will offer Tech Park employees, residents and the community a space to recreate, relax, and connect outside of home and work, steps from adjacent residential neighborhoods and less than a block from work.



THE VILLAGES

The 135 acre Village District provides diverse, high quality and attractive new neighborhoods and housing options, which will allow employees to live and work close by and move up and/or “age in place” within the same neighborhood. The Villages also serve to provide a seamless transition to the adjacent Spring Lake neighborhoods.

1.3 | EPICENTER OF AGRICULTURE & INNOVATION

The City of Woodland is at the epicenter of one of the most productive agricultural regions in the world. Woodland’s ideal climate, rich soil, and reliable water supply support a wide variety of crops that sustain a diverse portfolio of businesses. It is home to all facets of the food system from local farms and food processors to seed research and agricultural technology companies.



190 COMPANIES & COUNTING

**SEED RESEARCH AND AG TECH
FOOD PRODUCERS AND PROCESSORS
INDUSTRY SUPPORT AND DISTRIBUTION
MARKETS AND RETAILERS**

1.3.1 Regional Proximity

Woodland is part of the Greater Sacramento Region, which is home to the capital of the 6th largest economy in the world and part of a larger mega-region that is economically linked to the Bay Area technology hubs, including San Francisco, the Silicon Valley, and the I-680 corridor.

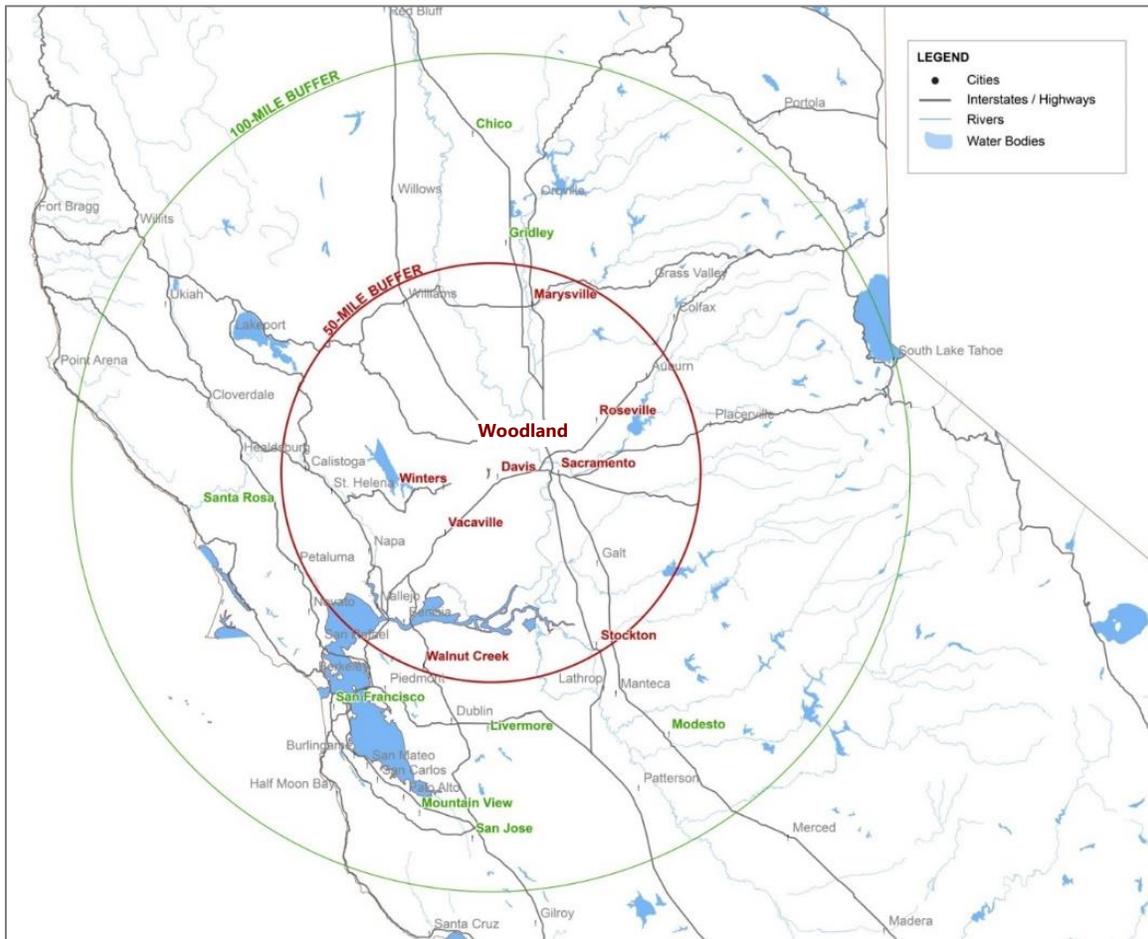


EXHIBIT 1-2: LOCAL REGION PROXIMITY

Woodland is located in the heart of the California Central Valley and at the confluence of abundant natural resources, major interstate highways, an inland port, and international airport. Moreover, one of Woodland’s greatest attributes is its proximity to the University of California Davis, a university on the leading edge of research and innovation, affording connectivity to university resources, facilities, faculty, and students. The Plan Area is just seven miles (ten-minutes by ride share or shuttle) from the UC Davis main campus. In addition to UC Davis, nearby universities of Sacramento State, Chico State, UC Berkeley and Stanford generate a continuous stream of talented graduates across all fields of scientific and engineering disciplines.

1.3.2 The Food Front

Launched in 2017, The Food Front initiative brought Woodland's story as the center point of food and agricultural responsibility, to the forefront. The City of Woodland has historically been economically and socially fortified by the fertility of its surrounding farmlands.

*Connecting growers, food producers and researchers,
The Food Front recognizes the important role Woodland plays in
agricultural and food responsibility and seeks to strengthen
Woodland's leadership capacity in this realm through continued
investment in forward-thinking projects and ideas that will improve
how food is grown, harvested, processed and enjoyed.*

The Plan Area will provide opportunities to expand on existing initiatives such as Seed Central, AgStart, and The Vine, solidifying Woodland's role as a key place for industry and academia to collaborate in to solve challenges of producing more, healthier food with less impact on natural resources.



1.3.3 Balanced Community- Work, Play, Live

The design framework of the Plan Area is intended to create a place that is authentic, socially engaging and that provides a diversity of experiences within a half-mile radius of the plan's featured 11-acre linear park. The Woodland Research and Technology Park will offer a collaborative business environment with supporting research and development in a setting that encourages innovation and entrepreneurship.



The Woodland Research & Technology Park will offer a collaborative business environment with supporting research and development in a setting that encourages innovation and entrepreneurship.

As a new employment center community, the Plan Area includes a range of housing options within close proximity to jobs and a commercial mixed-use Village Center, focused around a linear park. A multi-modal street network and bike-pedestrian trail system connects the entire Plan Area internally and to adjacent uses, including the neighboring Spring Lake residential community. Employees will be able to walk or bike to and from work and to outdoor recreation, retail and food/café venues, making the Plan Area an attractive place to live, recreate, shop and gather.



Technology Park, Road B & The Yard – Artist Rendering

1.3.4 Local Talent

The Plan Area is well positioned to retain and attract professionals in science and technology related fields to live in Woodland, including within the Plan Area. An increasing number of faculty and executive staff from UC Davis choose to reside in Woodland. Local employment training and workforce development opportunities through partnerships with the Woodland Community College, the Woodland Unified School District and UC Davis, are preparing local talent with the skills needed to support the demands of the technology sector, ensuring a steady and reliable employment base. The Plan Area will provide opportunities to connect industry with established programs, create new collaborations to teach needed skills and inspire Woodland's youth to pursue careers in a wide range of STEM and applied technology fields.

“Greater Sacramento is where innovators are revolutionizing agricultural technology. The region combines agricultural abundance from farmers to processors, highly skilled science and tech talent and the #1 agricultural sciences university in the nation – UC Davis.”

Greater Sacramento Economic Council

**308K STUDENTS
ENROLLED IN A
4 YEAR UNIVERSITY
WITHIN 90 MILES**

Greater Sacramento
Economic Council



1.3.5 Sustainability

The Woodland Research and Technology Park will serve as a leader in eco-innovation and sustainability. Plan Area development will employ sustainable design principles, minimizing impacts of the project on the environment. Sustainable building design, the use of low impact materials and construction techniques, on site water management and planning features that reduce commute trips and vehicle miles traveled will carry through the plan to help ensure the long-term health of the community and the environment.



It is anticipated that progressive companies and businesses locating within the Plan Area will take these efforts to the next level, incorporating sustainability practices in day to day operations through the integration of clean and renewable energy, sustainable sourcing, use of recycled materials and minimization of hazardous chemicals, waste management, green packaging, attention to a sustainable product life cycle, and through the development of energy-efficient “green” products.

The Plan Area, through development policies and standards, will play an important role in furthering the city’s greenhouse gas (GHG) reduction goals. It is the goal of the Plan to strive for zero net energy consumption as outlined in the City’s 2035 Climate Action Plan. The City’s Climate Action Plan provides strategies and tools for addressing GHG emissions from development and aim to reduce Woodland’s GHGs by 2020 and 2035 consistent with the State of California’s own targets.

Specific policies and development standards outlined in Chapters 2 and 3 address the sustainability requirements and implementation.

It is the goal of the Plan to strive for zero net energy consumption as outlined in the City’s 2035 Climate Action Plan.



1.4 | BUILDING ON LOCAL ASSETS

The Plan Area will provide an attractive place for residents and employees to live, recreate, shop and gather for neighborhood events, while also enjoying the amenities and opportunities a City rich in history, entertainment and community pride has to offer, including the resurgence of its Historic Downtown Main Street.

It is Woodland's rich heritage and small town charm that continue to attract people to the city and the reason so many families have stayed for generations.



Annual Dinner on Main Event

VIBRANT DOWNTOWN

Woodland has experienced a resurgence of investment and planned growth. In particular, Woodland's Historic Downtown has seen many of its historic buildings renovated and given new life. High end home décor and clothing boutiques dot Downtown's "Fabulous First Street," while hip new restaurants representing a variety of tastes stretch from one end of Main Street to the other. The City's *First Friday Art Walk* and *Thursday Night Live at the Plaza* concert series, as well as its annual festivals and holiday events draw thousands to Main Street throughout the year. Woodland's Annual California Honey Festival, spanning five blocks of downtown, draws 30,000 attendees from across the state.



Woodland's 3rd Annual California Honey Festival, spanning five blocks of Main Street, drew over 30,000 attendees from across the state and country in 2019.

HISTORIC AND NEW NEIGHBORHOODS

In addition to its historic downtown, Woodland boasts two honorary historic residential districts. Over 20 historic architectural styles are represented, dating from the 1870's to the 1950's and 60's. Spring Lake, located immediately east of the Plan Area, is a master planned community of 4,000 residential homes. Spring Lake carried the traditional neighborhood feel of Woodland's historic districts, through gridded and well connected streets, a mix of residential housing types and interconnected Class 1 bike trails and parks.



The Gable Mansion, built in 1885, is a California Historical Landmark

PRIME LOCATION

When it's time to get away, Woodlanders have access to sea or snow within one hour in either direction, east or west. Lake Tahoe, San Francisco and the Napa Valley are all within a two-hour drive. Sacramento, the State's Capital, is located 20 minutes to the east as well as the Golden One Arena where the NBA Kings call home. The Sacramento International Airport is an easy 10-minute drive or ride share from the city.



Velocity Island Wakeboard Park

For those desiring to stay closer to home, the Velocity Island Wake Board Park located in north Woodland, is a great place to cool off during the summer. Other's may choose to explore the Yolo countryside, Dunnigan Hills (an American Viticultural Area, "AVA"), or Capay Valley - the birthplace of the organic food movement, and taste the bounty of fresh, locally grown / made products or simply marvel at the ever-changing agricultural landscape.

1.5 | SPECIFIC PLAN GUIDING PRINCIPLES

In May 2017, the City of Woodland adopted a comprehensive update to its General Plan, setting the stage for future growth and development within its Urban Limit Line (ULL) through the year 2035. A key focus of the General Plan is economic development and job growth. The Plan Area is specifically envisioned within the General Plan as a natural extension of the city, ideally located and positioned for jobs and housing development.

The 2035 General Plan designates the Plan Area as a New Growth Area, SP-1A. Policy 2.L.2 provides further guidance for the Plan Area:

"Promote development of SP-1A as a mixed-use residential district anchored by a research and technology business park in the Southern Gateway area at CR 25A and SR 113. Concentrate the highest intensity of development within and in close proximity to the business park area, with lower density, largely residential uses to the north. Encourage sustainable development through the use of renewable energy sources and water conservation tools with the goal of striving to achieve zero net energy at the building and neighborhood level to the extent feasible."

Based on the guidance provided by the Woodland General Plan and the desired vision of a state of the art technology hub with sustained economic growth and competitiveness in today's global, knowledge based economy, the following Guiding Principles provide the envisioned outcome and overarching vision for development within the Plan Area.

1. INNOVATION

The Plan Area will develop as a state-of-the-art innovation center campus for technology, research and development, and office uses. Flexibility in design and implementation is supported, allowing businesses to respond to market demand through phasing of construction and the ability to offer a variety of building types and sizes. Complementary uses within immediate proximity to the business park, including hotel, commercial, employee-serving retail and recreational opportunities will support day-to-day needs of businesses, their clients, and their employees.

2. TECHNOLOGY CAPTURE/ TALENT RETENTION



Collaboration with UC Davis, Woodland Community College and others will bolster start-up businesses and growing mid-to-large size companies through technology transfer and IP sourcing. The Plan will accommodate advanced technology-related jobs and training that allow a greater number of Woodland residents and college graduates from the Woodland

Community College and throughout the region to live and work in the community, generating an infusion of intellectual capital.

3. BUSINESS PARTNERSHIPS

Companies locating in the Tech Campus will have the opportunity to take positive advantage of the existing and thriving seed, food, and agricultural-based industries currently located and doing business in and around Woodland. Access to additional resources and new markets, new ideas, materials, and expertise will grow through strategic partnerships with new and existing businesses in Woodland.

4. SUSTAINABLE AND RESILIENT

The Plan Area will lead in energy efficiency and sustainable design. Development within the Plan Area will incorporate cutting edge green building practices. Land use strategies and transportation demand management will reduce vehicle miles traveled and facilitate the use of alternative fuel vehicles. The city's urban forest canopy will be increased and projects will incorporate naturalized stormwater management. These and other measures will contribute to meeting City goals for greenhouse gas reduction by 2035 contained in its 2035 Climate Action Plan.

5. GATHERING PLACE

A successful Village Center and featured 11-acre linear park will provide a mix of social gathering spaces for employees, residents, and visitors to connect, recreate, and relax. These informal networking opportunities will foster greater innovation and engagement among the workforce and allow for the balanced integration of work and life that the next generation of professionals seek.

6. CONNECTIVITY/MOBILITY

A combination of well-designed complete streets, protected bicycle lanes, and pedestrian / bicycle greenways will prioritize the pedestrian experience throughout the Plan Area. Well-connected parks, open spaces and greenbelts will encourage residents and employees to walk, bike, or scooter rather than drive to work, home, and play. Existing bike trails and greenbelts will extend from and connect to the adjacent community including nearby schools, community center and shopping center. A shared mobility hub will serve as a point of connection for those arriving and departing the Tech Campus by various forms of alternative transportation – including micro transit stops and fixed bus routes with frequent service to Downtown Woodland and UC Davis. Amenities to support last mile active transportation alternatives are featured, including bike and scooter share services.

7. HEALTHY COMMUNITY

Connected streets with bicycle and pedestrian facilities, trails, accessible parks, and open spaces with passive and programmed recreation will facilitate and encourage active, healthy living. Access to healthy foods through community gardens, a farmer’s market and/or fresh produce market in the Village Center will be promoted. A mix of social gathering places will enable employees and residents to come together for fun and relaxation, boosting emotional wellness.



8. NEW NEIGHBORHOODS/SEAMLESS TRANSITIONS

Diverse, high quality and attractive new neighborhoods and housing options, including single- and multi-family residential units and mixed-used projects will allow Tech Park employees to live and work close by and “move up” within the same neighborhood as families grow or nests are emptied. Land use and circulation planning, coupled with design and development standards will ensure a thoughtful transition between the Plan Area and the adjacent Spring Lake neighborhood, complementing the established community.

1.6 | PLAN AREA CONTEXT AND CHARACTERISTICS

1.6.1 Plan Area Location

Exhibit 1-3 shows the location of the Specific Plan. The Plan Area is bound on the north and east by the Spring Lake Specific Plan Area, a 4,000-unit residential community. It is bound on the south by CR 25A and the City’s Urban Limit Line (ULL) and on the west by State Route 113 (SR 113). The Specific Plan Area consists of approximately 350 acres that are located just outside of the Woodland city limits but within the ULL.

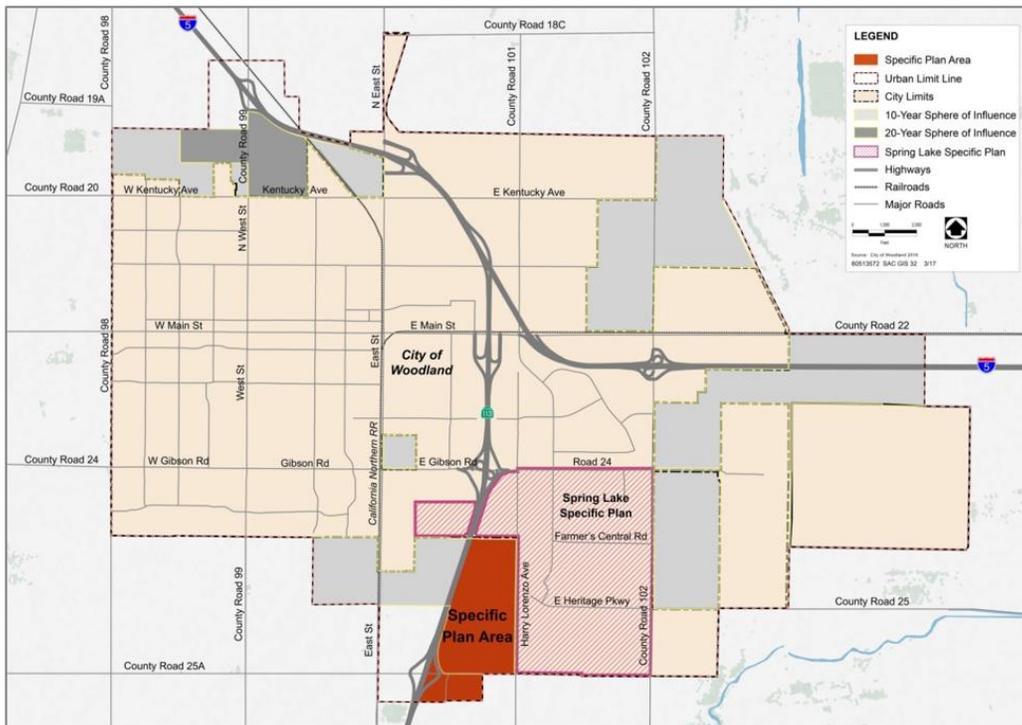


EXHIBIT 1-3: PLAN AREA LOCATION

1.6.2 Pre-Specific Plan Site Conditions

The Plan Area is visible from the SR 113 and is characterized by relatively flat actively farmed agricultural land, with one house and one barn located along County Road (CR) 25A. Lands within and adjacent to the Plan area are classified as Prime Farmland, as determined by the Farmland Mapping and Monitoring Program (FMMP), California Department of Conservation. Pre-Specific Plan access to the Plan area was from Gibson Road to the north and from the CR 25A interchange, via Harry Lorenzo Avenue (portions of which are unimproved). No natural streams, waterways or agricultural drainage canals exist in the Plan Area. Several existing trees in the Plan Area occur mainly adjacent to existing roadways. Existing utility poles, with overhead power lines, are located along SR 113 and Harry Lorenzo Avenue.

1.6.3 Surrounding Uses

The Plan Area is located and surrounded by agricultural land to the south, SR 113 and agricultural land to the west, and urban development within Spring Lake on the north and east. A sports park and the Woodland Community and Senior Center are located within one-half mile west of the northern boundary of the Plan area, from Sports Park Drive.

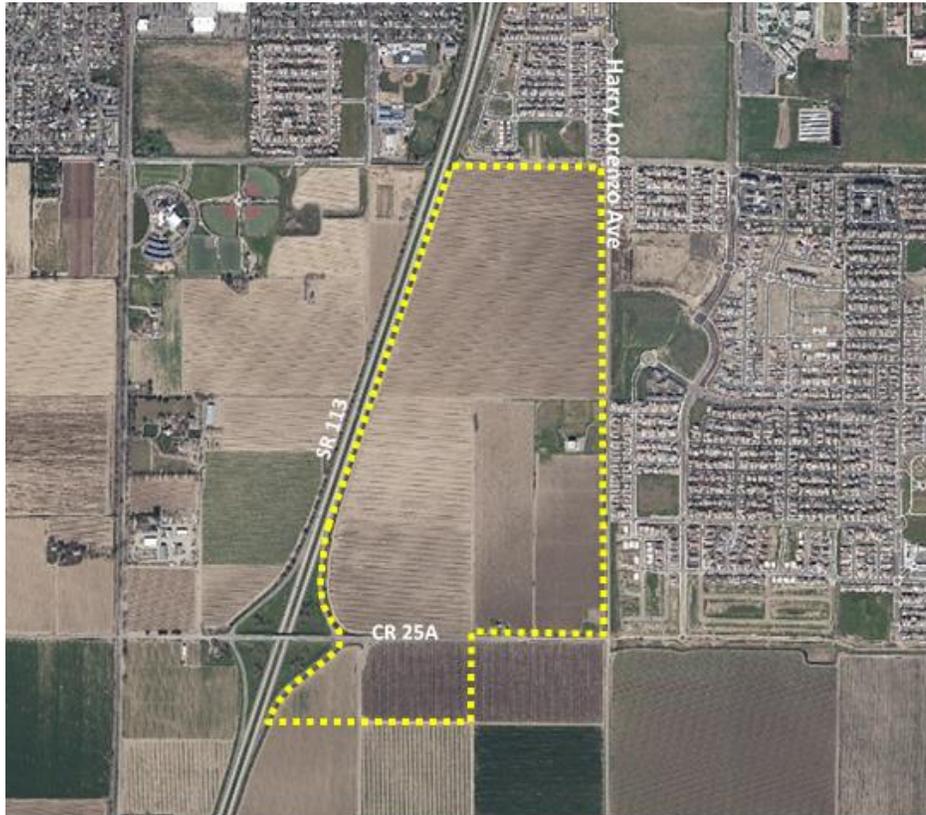


EXHIBIT 1-4: AERIAL PHOTO OF PLAN AREA AND SURROUNDINGS

1.6.4 Adjacent Infrastructure

In anticipation of future development of the Specific Plan, the backbone utility lines in Spring Lake were oversized and stubbed out at the border of the two planning areas, to ensure efficient service to the Plan Area through extension of those backbone utility lines from Spring Lake. Spring Lake's existing and planned bike network was also designed to connect seamlessly with the Plan area and includes a mix of dedicated on-street lanes and off-street paths. Similarly, the Plan Area extends and connects to the bikeways, pedestrian ways and open space corridors within Spring Lake.



subsequent CEQA review, residential, mixed-use, and employment center/office projects (with floor area ratios of 0.75 or greater) that are consistent with an adopted Specific Plan.

California Government Section 65451(a) defines the following content requirements for specific plans, consisting of text and diagram(s), which specify all of the following:

- The distribution, location, and extent of all land uses, including open space, within the area covered by the Specific Plan (Chapters 1, 2 and 5).
- The proposed distribution, location, extent, and intensity of major components of public and private transportation, sewage, water, drainage, solid waste disposal, energy, and other essential facilities proposed to be located within the area covered by the plan and needed to support the land uses described in the plan (Chapters 4 and 5).
- Standards and criteria by which development will proceed and standards for the conservation, development, and utilization of natural resources, where applicable (Chapters 2, 3 and 6).
- A program of implementation measures, including regulations, programs, public works projects and financing measures necessary to carry out paragraphs 1, 2, and 3 (Chapter 6).

1.8 | RELATED DOCUMENTS

A specific plan works in conjunction with other regulatory documents to implement the development process. The following section provides a brief description of these related documents. Chapter 7, Administration, provides more detail on the functions of these documents.

1. GENERAL PLAN

The California Government Code requires that Specific Plans be consistent with the local jurisdiction's adopted General Plan. The General Plan is a statement of the community's land use values and is the underlying basis for its vision and direction. An analysis of the consistency of the Specific Plan with applicable General Plan policies and actions is provided in the EIR.

2. WOODLAND ZONING CODE

The Specific Plan includes Development Standards and Design Guidelines, which are adopted into Ordinance (Zoning Code Title , Chapter , Article__) These regulations shall govern development, improvements and construction within the Plan Area.

3. IMPROVEMENT STANDARDS

The Improvement Standards establish basic standards and detail sheets for construction of public infrastructure. These standards and specifications apply to all construction within

the Plan Area, unless otherwise addressed herein. To the extent that a component or regulation of the Specific Plan differs from a requirement of the Standards, the Specific Plan will take precedence. Where the Specific Plan is silent, the Standards will be used for purposes of interpretation, and/or directly applied as appropriate.

4. ENVIRONMENTAL IMPACT REPORT

The City of Woodland was the lead agency in the preparation and certification of an Environmental Impact Report (EIR), pursuant to the California Environmental Quality Act (CEQA). The EIR for the WRTP Specific Plan (State Clearinghouse Number #2017062042) examines the environmental impacts of the proposed plan and identifies potential changes in the environment that would result from implementation of the plan. CEQA Guidelines (Title 14, California Code of Regulations) Section 15183 provides that projects consistent with development density, zoning, community planning, and the general plan for which an EIR was certified do not trigger a new environmental review unless there are new impacts that have not already been properly addressed. Section 65457(a) of the California Government Code and CEQA Guidelines Section 15182(a) provide that no EIR or negative declaration is required for any residential project undertaken in conformity with an adopted Specific Plan for which an EIR has been certified.

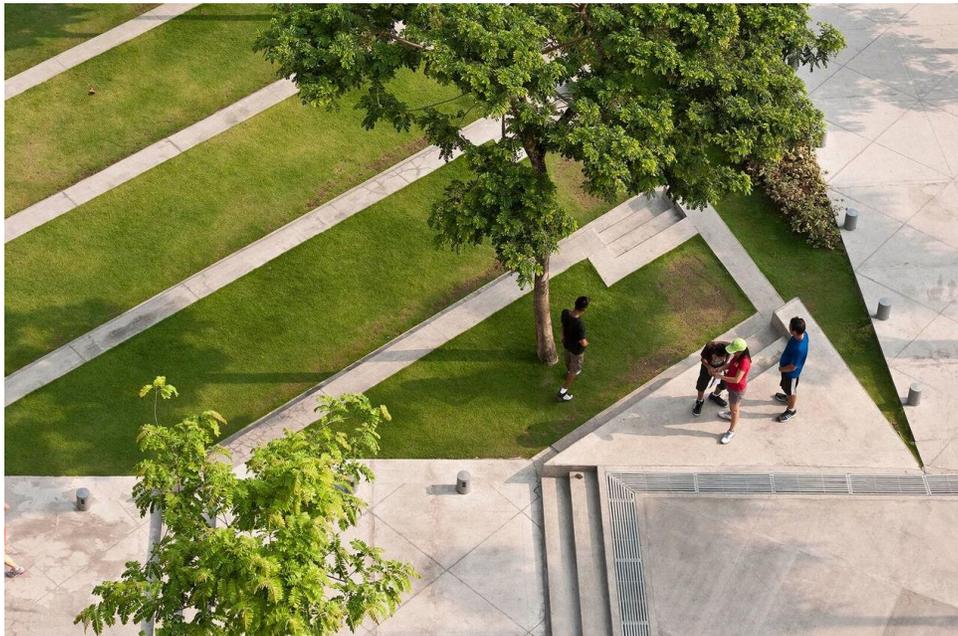
The EIR for the Specific Plan is intended to qualify subsequent projects within the Plan Area for these exemptions, assuming those projects are consistent with the adopted Specific Plan, and fulfill all applicable conditions and CEQA mitigation measures.

1.9 | PROPOSED ENTITLEMENTS

The Specific Plan and related documents were approved by the City of Woodland as part of the entitlement process. These entitlements included certification of the EIR, adoption of the Specific Plan, approval of a General Plan Amendment, and Pre-Zoning of the Plan. In addition to the above listed entitlements, separate annexation requests to Yolo County Local Agency Formation Commission (LAFCo) are required prior to the processing of individual developments. Individual development projects are subject to review and approval of subsequent permits and entitlements by the City of Woodland, other local, State and Federal agencies. Chapter 7, Administration, provides more detail on the Specific Plan Administration processes.



2.0 | LAND USE FRAMEWORK





2.1 | A FRAMEWORK FOR INNOVATION

The Woodland Research and Technology Park Specific Plan lays the framework for a place-based technology center that will accelerate the exchange of ideas between innovative companies and global research institutions. The convergence of land use types that link progressive work environments with a diversity of housing, retail and recreational opportunities sets the Plan Area apart from other traditional business parks and research centers. The development framework of the Plan Area seeks to capture the energy and excitement embodied within an energetic, urbanized setting while maintaining a feeling and appreciation for the small town charm and rural character that forms the foundation and heart of the Woodland community.

The convergence of land use types that link progressive work environments with a diversity of housing, retail and recreational opportunities sets the Plan Area apart from other traditional business parks and research centers.

The Plan Area consists of 350 acres poised for development of 2.2 million square feet of office, commercial and research/technology space, approximately 1,600 residential housing units and 21 acres of parks and open space, network trails and greenbelts. Approximately 5,000 jobs are anticipated at build-out. The Plan Area will bring new jobs to the City while reducing vehicle use and resultant emissions by providing an employment center and daily amenities within steps of attractive and diverse housing options.

This chapter outlines the Specific Plan Policies, which are used to guide subsequent sections of the Specific Plan including land use, circulation, sustainability and implementation. The Land Use

**350 ACRE
PROJECT AREA**
±2.2M SF RESEARCH/TECH
±1600 RESIDENTIAL UNITS
21 ACRES PARK/OPEN SPACE
5,000+ JOBS

Plan and Land Use Summary are provided in Section 2.3. The Land Use Plan is the “blueprint” of the community, which evolved from the vision, the application of applicable General Plan policies and in response to the opportunities and constraints of the site. The chapter concludes with the Districts, which provide the framework of the Land Use Plan and highlight the unique features that make up the Plan Area community.



BIRDSEYE VIEW LOOKING NORTH - ARTIST RENDERING

MULTI- USE TRAILS AND PATHS 5 MILES

DEDICATED ON-STREET BIKE LANES 3.5 MILES

LINEAR PARK & GREENBELTS 17.6 ACRES

2.2 | SPECIFIC PLAN POLICIES

Policies for the development of the Plan Area are derived from the Guiding Principles outlined in Chapter 1.5. Several policies closely mirror those adopted in May 2017 as part of the General Plan 2035 update (as indicated in parentheses), while others are specifically crafted to implement the vision of the Plan Area.

The following land use, design, sustainability, open space, streetscape and mobility, and economic development policies help to define and implement the land use framework outlined in Section 2.4 of this chapter.

2.2.1 Land Use

1. **Epicenter of Food and Agriculture.** Promote the development of the Plan Area as a premier food and agriculture industry hub by providing appropriate infrastructure, adequate land with compatible land uses and by supporting research and innovation. (Policy 2.D.2)
2. **Atmosphere of Collaboration.** Promote an atmosphere of collaboration and innovation by designating an appropriate mix of land uses, building types and formats with an engaging public realm.
3. **Active Ground Floor Uses.** Facilitate the opportunity for active commercial and ground floor uses such as meeting rooms, exhibition and maker-spaces, cafés, fitness studios within the RTP/Transitional Overlay zone fronting Road B. Provide architectural design and streetscape standards and guidelines that help frame the public realm as an inviting urban, walkable environment.
4. **Flexibility within Development Standards Framework.** Include flexible zoning, site and development standards that will allow companies and businesses within the Plan Area to easily adapt to changing market demands and to grow and evolve over time.
5. **Housing Mix.** Design neighborhoods to include a mix of housing types at a range of densities and affordability levels that accommodate residents at all stages of life. Residential uses must achieve an overall minimum average density of eight dwelling units per gross acre across the Plan Area.
6. **Variety and Quality.** Ensure that residential development provides variability and quality design to distinguish individual homes from one another and create identifiable neighborhoods. (Policy 2.M.8)

2.2.2 Design

1. **Compact Form.** The Land Use Plan shall promote compact development patterns, mixed land use, and higher-development intensities that conserve land resources, reduce vehicle trips, improve air quality, and facilitate walking, bicycling, and transit use. (Policy 2.C.1)
2. **View Corridors.** Create attractive view corridors that frame the streets with distinctive buildings, trees, and other landscaping complemented by well-designed and integrated signage. At community entry points, provide a clear, physical sense of arrival into the community. (Policy 2.E.5)
3. **Building Facades.** Encourage a mix of building patterns, frontages, ground floor transparency, and the use of horizontal and vertical building articulation to break up building mass, create visual interest and design to activate street level frontages. (Policy 2.E.6/2.I.1)
4. **Transition Areas.** Promote the sensitive design of transition areas between different land uses in order to ensure compatibility, and encourage a gradual and compatible shift in scale between different densities and intensities of various uses. (Policy 2.E.3)

2.2.3 Sustainability

1. **Consistency with the Climate Action Plan.** Ensure new development is consistent with the objective and targets of the City's Climate Action Plan with the goal of striving to achieve zero net energy at the building and neighborhood level. (Policy 2.C.2/2.L.2)
2. **Resource Efficiency.** Encourage and incentivize building construction that consumes less energy, water, and other resources; allows natural ventilation; uses daylight effectively; reduces stormwater runoff; and facilitates the use of clean energy whenever possible. Use trees for cooling, energy conservation and to improve air quality. (Policy 2.C.4)
3. **Electric/Alternative Fuel Vehicle Parking.** Parking facilities in non-residential zones shall provide for alternative fueling and electric vehicle charging. Residential development shall provide EV-capable facilities in all garages and parking lots. (Policy 3.H.7)
4. **Agricultural Interface.** Where development within the Plan Area occurs at the edge of the Urban Limit Line adjacent to agricultural land, a buffer of 150-feet shall be incorporated, where feasible, to minimize potential incompatibilities of the adjoining uses. (Policy 7.C.4/7.C.5)
5. **Agricultural Uses.** Where agricultural activity exists within the Plan Area boundary, the agricultural uses shall be permitted to continue until urban development occurs on the active agricultural properties. (Policy 7.C.2)

2.2.4 Open Space

1. **Greenbelt/Trail System.** Extend and integrate a greenbelt/trail system that provides recreational and transportation benefits for residents and employees of the Plan Area. (Policy 2.M.4)
2. **Public Gathering Space.** Provide interesting public gathering spaces and corridors including small public plazas, courtyards, pocket parks and paseos within the North and South Campus and the Village Center to provide opportunities for social interaction within the built environment.
3. **Private Open Space.** Provide private open space as part of large residential, commercial or mixed-use development projects.

2.2.5 Streetscape and Mobility

1. **Bike and Pedestrian-Oriented.** Create walkable, pedestrian-scaled blocks that feature sidewalks and bikeways that are safe, comfortable, and inviting. Encourage site design for mixed use zones to minimize driveway, curb cuts and parking visibility. (Policy 2.E.4/2.I.1)
2. **Green Streets.** Provide continuous shade trees along key corridors, integrate low-impact development (LID) drainage facilities to manage stormwater runoff within the public right-of-way, and include Class I or Class II bike facilities where possible. (Policy 2.I.3)
3. **Multimodal Access.** Require convenient, attractive and safe pedestrian, bicycle and transit connections throughout the Plan Area Districts. (Policy 2.J.6)
4. **Roundabouts.** Provide landscaped roundabouts as an intersection traffic control option with demonstrated air quality and safety benefits, where deemed feasible and appropriate. (Policy 3.B.9)
5. **Parking.** Shared parking facilities are encouraged throughout the Plan Area and shall be designed to reduce heat island effect, providing for significant landscaping and tree canopy. Onsite parking along/adjacent to street frontages, particularly Road B, is strongly discouraged. (Policy 3.H.2/3.H.3/3.H.6)
6. **Transportation Demand Management (TDM).** Businesses within the Research and Technology Park campus should incorporate Transportation Demand Management (TDM) tools and programs to encourage and create incentives for the use of alternative travel modes and disincentivize single-occupancy vehicle use. (Policy 3.A.5)

7. **Emerging Technologies.** Plan for and accommodate next generation transportation technology and advancements, where feasible, in the Plan Area design and street network. (Policy 3.A.6)
8. **Transit Services.** In coordination with the Yolo County Transportation District and UC Davis, plan and implement transit and express-bus service between the Plan Area and UC Davis campuses that is both sustainable and cost-effective. (Policy 3.G.1/3.G.6)

2.2.6 Economic Development

1. **Business Environment.** Support a welcoming business environment within the Technology Park through streamlined and consistent application of regulatory and permitting procedures and through financially feasible development costs. (Policy 4.A.2)
2. **Emerging Industries.** Provide flexible development regulations and guidelines that allow for efficient adaptation and accommodation of emerging industries and new market opportunities. (Policy 4.C.6)
3. **Agricultural Industry.** Accommodate agriculture-related industries through supportive ag policies and programs including ag-technology accelerators, agricultural processing facilities and through coordination with Yolo County, UC Davis, AgStart and other organizations invested in advancing global innovation in ag and food-tech. (Policy 4.C.9)
4. **Workforce Development.** Provide for educational and workforce training opportunities within the Plan Area through partnerships with employers and business educators, the Woodland Community College and the Woodland Joint Unified School District. (Policy 4.C.15/4.G.5)

2.3 | LAND USE PLAN

The Land Use Plan, shown on Exhibit 2-1, illustrates the arrangement of land uses and the transportation network which provides the backbone of the Land Plan. Table 2.1 provides a detailed summary of the land use designations, acres, dwelling units and square footages allocated on the Land Use Plan and evaluated in the EIR analysis. At build out, the land use plan is estimated to result in approximately 2.2 million square feet of research/technology and commercial land uses, producing approximately 5000 jobs and 1,600 residential dwelling units. The mix of land use types and intensities will provide for a vibrant, sustainable and balanced new community addition which complements and supports the surrounding and planned uses within the City of Woodland.

LEGEND

- LDR LOW DENSITY RESIDENTIAL
- VCLDR VILLAGE CENTER LOW DENSITY RESIDENTIAL
- MDR MEDIUM DENSITY RESIDENTIAL
- VCMDR VILLAGE CENTER MEDIUM DENSITY RESIDENTIAL
- HDR HIGH DENSITY RESIDENTIAL
- HDR/CCO HIGH DENSITY RESIDENTIAL WITH COMMUNITY COMMERCIAL OVERLAY
- VCMU VILLAGE CENTER MIXED USE
- RTP RESEARCH & TECHNOLOGY PARK
- RTP/TO RESEARCH & TECHNOLOGY PARK WITH TRANSITIONAL OVERLAY
- RTP/CCO RESEARCH & TECHNOLOGY PARK WITH COMMUNITY COMMERCIAL OVERLAY
- RTP/RFO RESEARCH & TECHNOLOGY PARK WITH RESEARCH FLEX OVERLAY
- HC HIGHWAY COMMERCIAL
- OS GREENBELTS, OPEN SPACE
- PP PEDESTRIAN PROMENADE
- VCOS VILLAGE CENTER OPEN SPACE
- DET POND DETENTION POND



EXHIBIT 2-1: LAND USE PLAN

Table 2.1 Land Use Summary

Land Use Designations	Zones	Gross Acres	Density Range	Floor Area Ratio Range	Estimated Dwelling Units ¹	Estimated Non-Res Square footage ¹
RESIDENTIAL						
Low Density Residential	LDR	74.8	1.0-8.0		500	
Village Center Low Density Residential	VCLDR	13.1	1.0-8.0			
Medium Density Residential	MDR	35.5	8.1-19.9		600	
Village Center Medium Density Residential	VCMDR	16.7	8.1-19.9			
High Density Residential	HDR	9.7	20.0-40.0		500	68,000
High Density Residential/Community Commercial Overlay ²	HDR-CCO	15.9	20.0-40.0	0.2-2.0		
SUBTOTAL RESIDENTIAL		165.7			1,600	68,000
RETAIL / COMMERCIAL						
Village Center Mixed Use ³	VCMU	3.5		0.6-4.0		75,000
Highway Commercial	HC	8.2		0.25-2.0		70,000
Subtotal Retail /Commercial		11.7				145,000
RESEARCH AND TECHNOLOGY PARK						
Research and Technology Park	RTP	50.3		0.2-2.0		1,955,000
Research and Technology Park/Transitional Overlay	RTP/TO	19.3		0.2-2.0		
Research and Technology Park/Community Comm. Overlay	RTP/CCO	6.8		0.2-2.0		
Research and Technology Park/Research Flex Overlay	RTP/RFO	35.9		2.0 max		
Subtotal Research and Technology		112.3				1,955,000
Subtotal Commercial/RTP		124.0				2,100,000
PARKS, OPEN SPACE, DETENTION POND						
Village Center Open Space (includes Pedestrian Promenade) ⁴	VCOS	11.6		0.5		32,000
Greenbelts, Open Space, Promenades	OS	6				
Detention Pond/Facility	OS (DET POND)	4.2				
Subtotal Parks/OS		21.8				32,000
RIGHT OF WAY						
Collectors & Arterials	ROW	38.0				
GRAND TOTALS		349.5			1,600	2,200,000

NOTES:

1. Assumed average yields for planning purposes and EIR analysis.
2. 25% (~4.0 Acres) of HDR/CCO Acreage assumed to be developed as Community Commercial
3. Village Center Mixed Use may include residential units not included herein
4. Village Center Open Space allows for 1.5 acres of commercial/retail uses

2.3.1 Introduction of the Land Use Zones

The Land Use Plan identifies the location of the land uses with a zoning designation. In general, the Land Use Plan includes three broad categories of land uses. The Research/Technology Park and Commercial category are the primary employment focused zones. The Residential category is the primary housing zones and the Open Space/Park category includes the open space, parks, recreation and public zones. Chapter 3, Land Use Regulation, Development Standards and Design Guidelines, provides a detailed description of each zone, the allowed density and intensity, the permitted uses and the physical standards such as setbacks, building heights and other development standards.

1. RESEARCH/TECHNOLOGY PARK AND COMMERCIAL ZONES

RTP- Research Technology Park

RTP/TO-Research Technology Park/Transitional Overlay

RTP/CCO-Research Technology Park/Community Commercial Overlay

RTP/RFO-Research Technology Park/Research Flex Overlay

HC-Highway Commercial

VCMU-Village Center Mixed Use

2. RESIDENTIAL ZONES

LDR- Low Density Residential

VCLDR-Village Center Low Density Residential

MDR- Medium Density Residential

VCMDR-Village Center Medium Density Residential

HDR-High Density Residential

HDR/CCO-High Density Residential/Community Commercial Overlay

3. OPEN SPACE/PARK ZONES

VCOS- Village Center Open Space

OS-Greenbelts, Promenade and Detention Pond/Facility

2.4 | THE DISTRICTS

Districts are used to identify the geographic and form types within the Land Use Plan and embody the overarching theme of work, play, live. The three Planning Districts are the Technology Park, the Village Center and the Villages. Each of the three Districts have sub-districts, as outlined below and described in the following sections. The sub-districts assist in defining desired elements of intensity, scale and form for each sub area. The envisioned form and character of each district is described in the following sections and more detail is provided in Chapter 3, Development Standards and Design Guidelines.

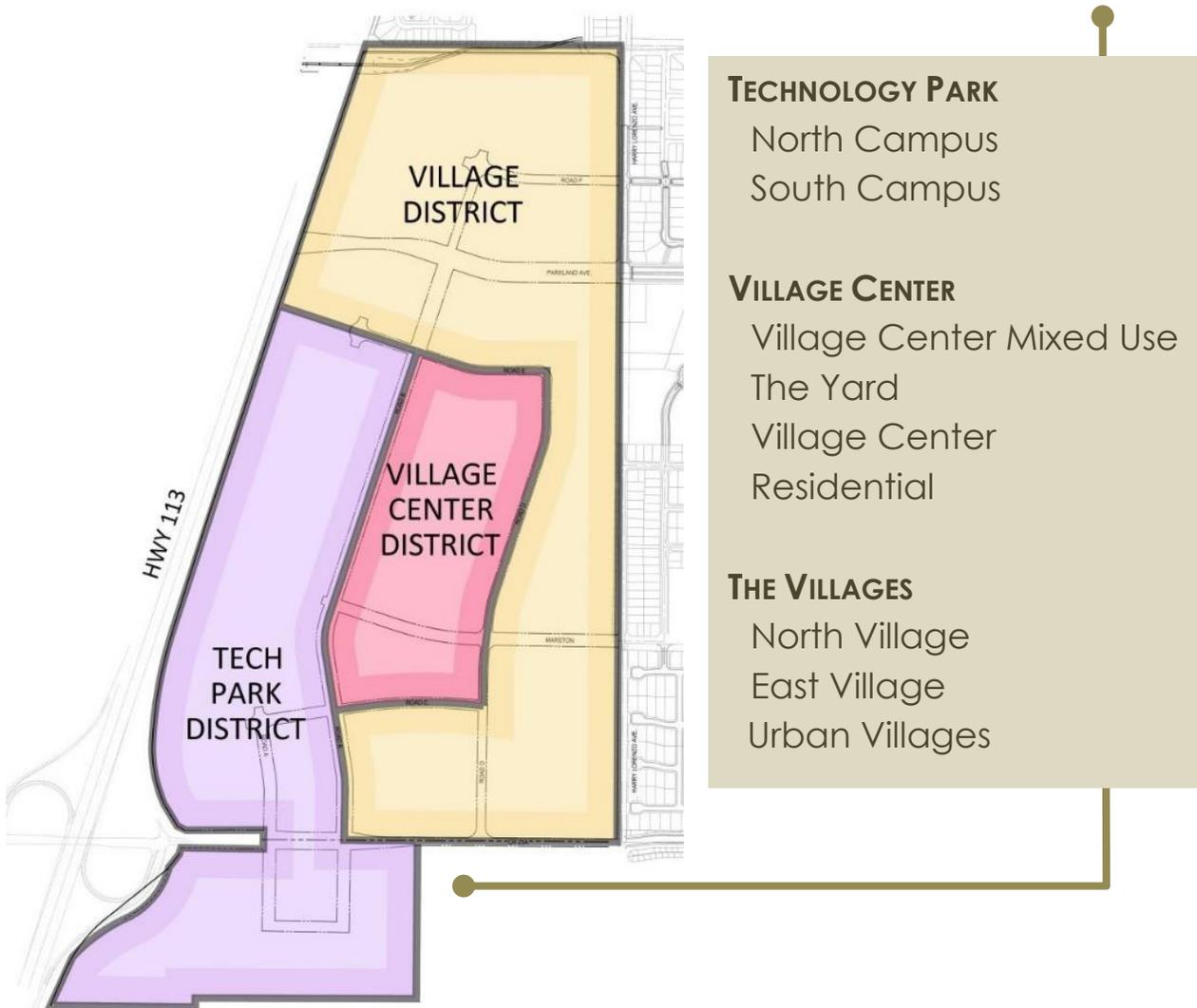
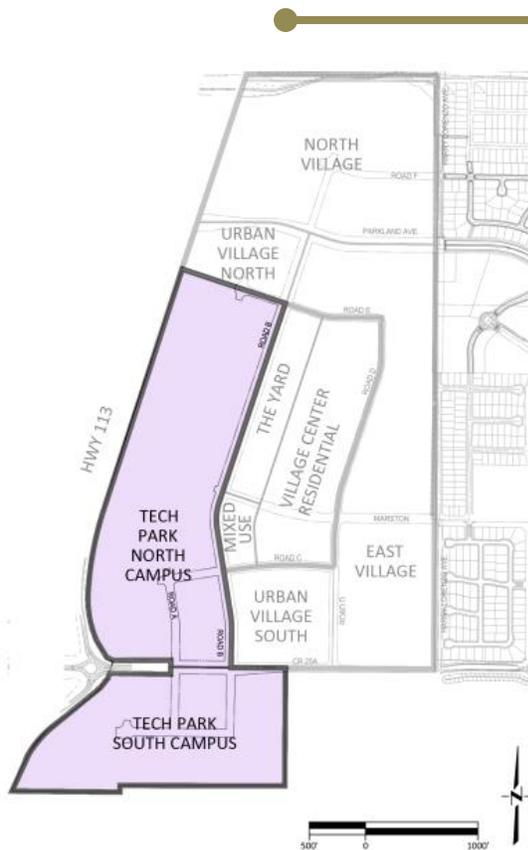


EXHIBIT 2-2: DISTRICTS



WORK ■ LIVE ■ PLAY

2.4.1 Technology Park District



TECHNOLOGY PARK DISTRICT
 ... North Campus | 76.4 acres
 ... South Campus | 44.1 acres
TOTAL 120.5 acres

Consisting of a North and South Campus, the Technology Park District will accommodate approximately 2 million square feet of office, research and light industrial “flex” space for a variety of users in a range of sizes and scale. The North Campus, described in detail below, serves as the core business hub for both established and large-scale companies as well as enterprising start-ups and incubator spaces. The South Campus will carry forward this collaborative environment while offering additional opportunity for light manufacturing/industrial flex space and immediate proximity to real-time, field research sites. Sustainability principles will be incorporated throughout the North and South Campus as a key and defining attribute of the Technology Park

District. Energy efficient building design and mobility features that reduce auto dependency will minimize carbon impact with a goal of achieving net-zero energy and safeguarding the long-term health of the environment.

1. NORTH CAMPUS

The North Campus will serve as the primary gateway and innovation hub within the Plan Area. Comprised of approximately 98 acres, the North Campus has a base zoning of Research and Technology Park (RTP). In order to allow flexibility to mix and adapt use types in response to development opportunities and shifting market demands, two areas of the North Campus have an overlay zone of Community Commercial (RTP/CCO) and Transitional (RTP/TO). Detail on the permitted uses within these zones are found in Chapter 3.



The Tech Campus will house a variety of uses and building types including corporate headquarters and mid- to large sized offices.



The North Campus is intended to evolve as a diverse, leading edge research and technology park designed to support adaptable site and building formats and to accommodate a variety of users in a campus environment. Flexible regulations will allow for innovative design solutions that support evolving trends and advances in the fields of science and technology. Diverse uses and building types, from mid- to large-size offices, corporate headquarters, incubation spaces for small start-up firms, and spaces for the commercial development of pioneering products and technologies will contribute to a broader knowledge ecosystem within the North Campus.

Buildings may require large formats/footprints and range from two to five stories in height, yet the pedestrian experience will remain comfortable and engaging. Frequent street-facing entrances and glazing, active ground floor uses, appropriate building setbacks and properly framed public spaces will enhance the public realm, inspiring creative thinking both within and outside of the built environment. Well designed and landscaped pedestrian paseos including areas for pocket parks and other amenities, will break up longer blocks and encourage pedestrian activity and interaction.

A welcome center, leasing office, technology innovation demonstration areas, landmark and gateway features exhibiting the technology and sustainability themes are envisioned to be located at the main entrance of the North Campus, along Road B. Wayfinding will be integrated throughout the North Campus to ensure easy navigation through the Plan Area starting at the Gateway.

A commercial center including a hotel and conference center is envisioned at the southern end of the district, providing daily service amenities for businesses, visitors, employees and residents.



The incorporation of ground floor commercial and other active uses such as cafés, fitness studios, exhibition and maker-spaces, meeting rooms, and similar uses, will provide a vibrant and engaging building-to-streetscape interface.

The easternmost edge of the North Campus will serve as an important transition zone between the predominantly larger format uses and the mixed-use Village Center and residential areas to the east. A greater emphasis on the pedestrian experience, design and architectural detailing are placed on buildings directly fronting Road B. The incorporation of ground floor commercial and other active uses such as cafés, fitness studios, exhibition and maker-spaces, meeting rooms, and similar uses, will provide a vibrant and engaging building-to-streetscape interface. Appropriate building frontages, massing, setbacks, sidewalk widths, and overhead coverings/awnings in addition to thoughtfully placed street furniture, landscaping and lighting will help frame the public realm as a walkable and comfortable urban environment.

Businesses fronting Road B will have the advantage of being located directly adjacent to and across from The Yard, an 11-acre linear park, providing the associated views and opportunities for enhanced social engagement. Businesses located across from The Yard are encouraged to integrate outdoor seating and gathering spaces that take advantage of the viewshed and further activate the street.

2. SOUTH CAMPUS

The 44-acre South Campus serves as a continuation of the North Campus, but with a greater emphasis on light manufacturing uses and research facilities and the addition of Highway Commercial zoning. South Campus businesses will benefit from both freeway frontage and immediate proximity to prime agricultural land to the south and east of the Plan Area boundary that will allow field trials to be conducted proximate to research. Approximately 36 acres of the South Campus is zoned Research Flex Overlay (RTP/RFO), which allows flexibility to mix and adapt use types in response to development opportunities and shifting market demands. Detail on the permitted uses within this zone are found in Chapter 3.3.

Approximately 8 acres of Highway Commercial (HC) zoning is provided at the northwest corner of the South Campus allowing commercial uses that serve the traveling public and the community.

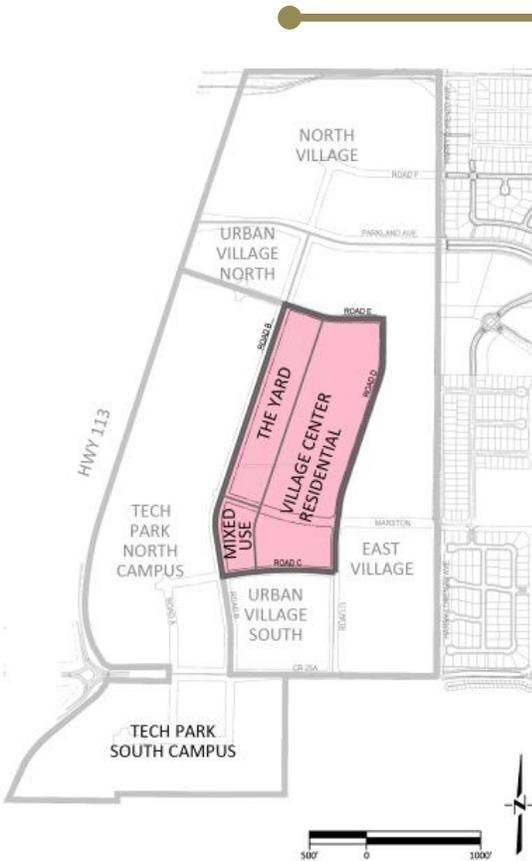


Proximity to prime agricultural land will allow for convenient access to field trials and research. Greenhouses are permitted in the South Campus.



WORK ■ PLAY ■ LIVE

2.4.2 The Village Center



VILLAGE CENTER DISTRICT

- ... Village Center Mixed Use | 3.5 acres
- ... Village Center Residential | 29.8 acres
- ... The Yard | 11.5 acres

TOTAL 44.8 acres

The Village Center, consisting of approximately 45 acres, will provide the essential “third place,” for the younger workforce as well as the opportunity for experienced employers to connect with and mentor the next generation of industry leaders and thinkers. Outdoor retail displays, restaurant patios and sidewalk cafés will spill out on to open-air hardscape plazas. An 11-acre park termed “The Yard,” framed by mid-rise townhomes and urban-style lofts will offer employees, residents and the community a space to recreate, relax, and connect outside of home and work,

steps from adjacent residential neighborhoods and less than a block from work. A variety of mobility services and amenities will similarly converge at the Village Center Shared Mobility Hub, providing a transit destination in the heart of the Tech Park community.

1. THE VILLAGE CENTER

The place to “see and be seen,” employees will find opportunities for social engagement as well as a space to work, collaborate, and think in a vibrant, transparent and inviting setting.

Within the Village Center’s 3.5-acre Mixed Use (VCMU) zone, smaller format commercial spaces and mixed-use buildings will house retail/food venues and co-working spaces on the ground floor, with residential and office uses permitted on second and third floors. The place to “see and be seen,” employees will find opportunities for social engagement as well as a space to work, collaborate, and think in a vibrant, transparent and inviting setting. Architectural guidelines and development standards call for buildings that promote a positive connection and seamless transition from the built environment to outdoor public spaces.

2. THE YARD



“The Yard,” an 11-acre linear park, located immediately north of the mixed-use retail zone, is the physical center and social heart of the Village Center and the broader Plan Area. Envisioned as a largely passive green space, The Yard will serve as a shared “front porch” and lawn for the broader Tech Park community. The relaxed park setting will accommodate both active and passive recreational activities, such as a disc golf pick-

up game or yoga class. Flexible seating and movable furniture will allow park users to adapt their park environment for comfortable use as a temporary outdoor office or for an after work gathering space with colleagues. Bike/pedestrian and jogging trails winding through and around the park connect the residential Villages to the east with the Tech Park North Campus on the west. Landscaping throughout The Yard will incorporate native plant species and grasses while a canopy of shade trees will allow for the comfortable enjoyment of the outdoors during hot summer months. The park will support stormwater management within the Plan Area, providing areas of natural biofiltration and retention.

The Yard’s central location within the Plan Area also provides for a natural relationship between the park setting and the Village Center’s mixed-use retail zone located immediately south of the park. Plaza and courtyard features at the southern end of the park will enable a continuation of the Village Center café and restaurant patio dining experiences as well as a covered pavilion that will host weekly farmer’s markets, evening music venues and other community or company sponsored events. Up to 1.5 acres of The Yard will be available for private commercial/retail uses including pop-up coffee kiosks, container breweries and eventual full-scale eateries that complement an urban park setting.



The Yard at the Village Center will include passive recreational opportunities as well as a covered pavilion for a farmer’s market and other outdoor events.

Predominantly residential townhomes, walk-ups, and loft style units are envisioned to front on an 20-foot pedestrian promenade running the length of the eastern boundary of the park. The promenade will include lighting and enhanced, textured concrete paving providing a distinctive experience that also offers a buffer between the public park and private residential realm. Balconies and porches overlooking the promenade and park will increase activity at night and provide 24-hour “eyes on the park” for added security. Together, the combined Village Center and linear park space will be the pulse of the community; a shared space for intellectual exchange, social engagement and activity.

3. VILLAGE CENTER RESIDENTIAL



A mix of low density (VCLD) and medium density (VCMD) residential units are located on the eastern portion of the Village Center District. Contemporary row house, townhouse, small lot and detached units will provide a variety of housing opportunities for those that desire a more centrally located and lively residential experience. Units will be located within steps of the local café and outdoor recreational opportunities at The Yard and just a block from the Tech Park North Campus. Residential units within the Village Center will help foster an active

mixed-use District well past traditional office hours, bolstering the viability of retail and commercial uses.

4. VILLAGE CENTER STREETSCAPE

The Village Center streetscapes, including portions of Road B and C and Marston Drive, will transition from the active North Campus Gateway and Tech Park to a calmer, pedestrian-oriented environment. Thoughtfully placed street furniture, public art, landscaping and wide sidewalks designed to accommodate outdoor seating will enhance the public realm within and around the Village Center. Similarly, architectural guidelines will ensure building frontages invite pedestrian activity and reinforce the urban character of the district.

Enhanced intersections and pedestrian mid-block crossings along Road B will allow for safe, high visibility and efficient access to the Village Center from the North Campus. Narrow travel lanes and on-street parking will help calm traffic through the district while minimizing the need for large off-street parking lots. Decorative sidewalk paving and treatment will set the Village Center apart as a recognizable pedestrian dominant district.



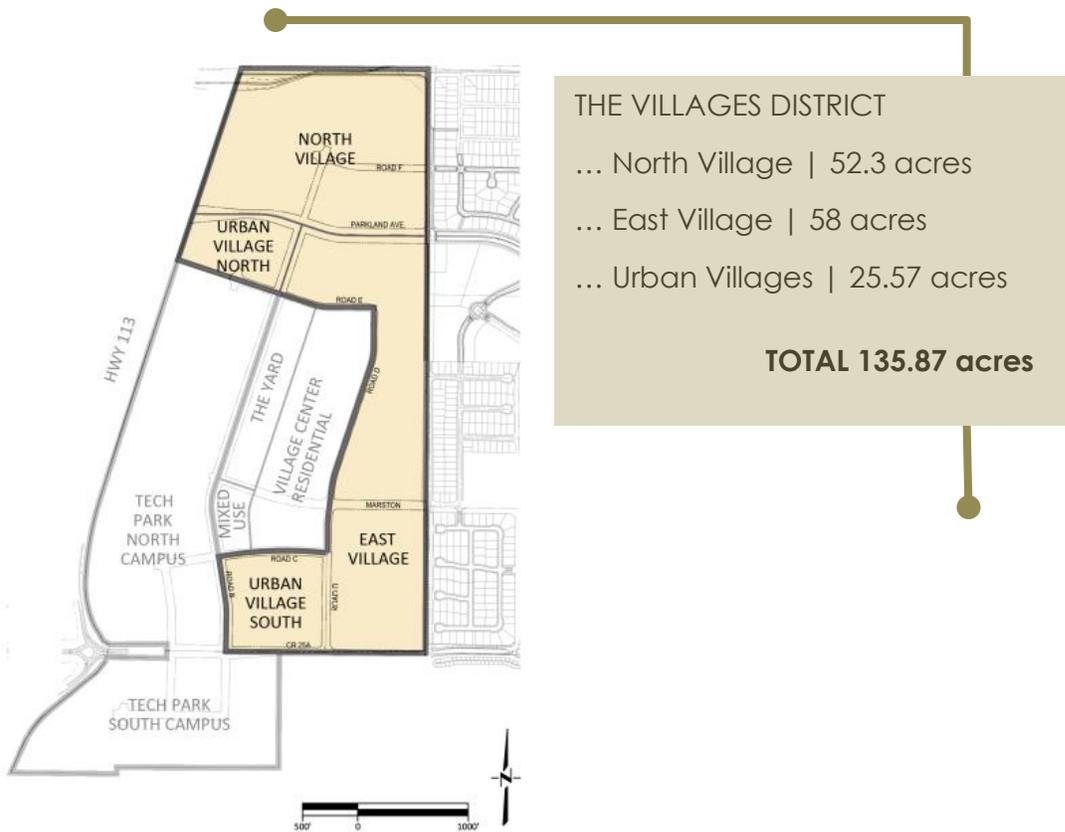
Village Center & Mobility Hub – Artist Rendering

5. SHARED MOBILITY HUB

The Shared Mobility Hub (The Union), located along Road B immediately adjacent to the Village Center commercial zone, will provide an engaging point of arrival or departure from the Tech Park for employees and residents traveling to, from and within the Plan Area. The Union will serve as the point of convergence, seamlessly integrating traditional and innovative modes of travel. For regional trips to/from Sacramento and the Bay Area, the Union will offer a stop for intercity fixed route transit. Higher frequency bus or autonomous shuttle services will use The Union as the primary point of connection between the Tech Park and primary destination points such as UC Davis Campus, the Amtrak Station, and Downtown Woodland. Those arriving to the Village Center will be able to choose from an array of options including micro transit, bike/scooter share or ride hailing service to link their “last mile” whether it be to home or work. The Union’s plaza-like “loading zone” will be where it all comes together and where visitors can grab a cup of coffee while they wait for their ride or simply sit and enjoy the energy of the place.



2.4.3 The Villages



The Villages District features three distinct residential neighborhood types; the North Village, East Village and the Urban Villages. The Village District will accommodate approximately 1,600 residential housing units of varying sizes, densities and product types from detached single family homes to urban style lofts. Plan Area employees will have the opportunity to live within close proximity to their work, in many cases, within a 10-minute walk or less. A network of well-connected streets, bike trails and greenbelts will allow for easy and safe routes to work, home and to the Village Center, reducing long commute times and auto-dependency for a majority of daily trips.

1. NORTH VILLAGE

The Plan Area’s North Village will consist primarily of single family homes on a range of lot sizes. Larger lots will accommodate executive and multi-generational style homes as well as “move-up” housing opportunities for those growing families seeking more room to play. Smaller to mid-sized lots will offer housing options for first time homebuyers to empty nesters. Although not strictly prescribed, architectural styles including contemporary farmhouse and cottage will establish a sense of place reminiscent of the area’s agricultural roots. A landscaped roundabout centrally located within the North Village along Road F will serve as a focal feature within the neighborhood.

Builders are encouraged to explore nontraditional pedestrian pathways and circulation elements within the North Village that incorporate natural drainage, allow for a softer interface between the street and residential lots, while reducing the concrete hardscape typical of traditional sidewalk/curb/gutter schemes.



Contemporary farmhouse and cottage style residential design will establish a unique sense of place within the North Village.

2. EAST VILLAGE

The East Village, running the length of the Plan Area’s eastern boundary, south of Parkland Avenue, is comprised of a mix of low and medium density housing types including detached single family products, alley products, cluster and courtyard homes. Residential densities within the East Village are intended to provide a transition from the Plan Area community to the established Spring Lake neighborhoods to the east.



Non-traditional subdivision design is encouraged in the East Village, including alley load and cluster design, and layouts with shared common space.

A continuous north-south running greenbelt with Class 1 bike path will serve as the adjoining ribbon between the two neighborhoods while Spring Lake bike paths and paseos will extend westward through the Plan Area, connecting the two communities. A stormwater detention basin located at the southern end of the East Village, just north of CR 25A, may dually serve as passive recreational open space.

3. URBAN VILLAGE



High quality apartments and residential lofts will provide housing for young professionals and empty nesters within close proximity to Tech Park jobs.

Two High Density Residential zones within immediate proximity of the North Campus will accommodate urban style, high quality workforce housing for the young professional as well as empty nesters. These units may also serve as temporary housing for researchers and scientists sponsored by companies locating within the district.

The incorporation of thoughtful design elements and consideration of the pedestrian experience will ensure that high-density development remains synonymous with high-quality of life and that the Urban Village actively contributes to a lively and engaging Technology Park campus and environment.

4. INTERNAL STREET NETWORK

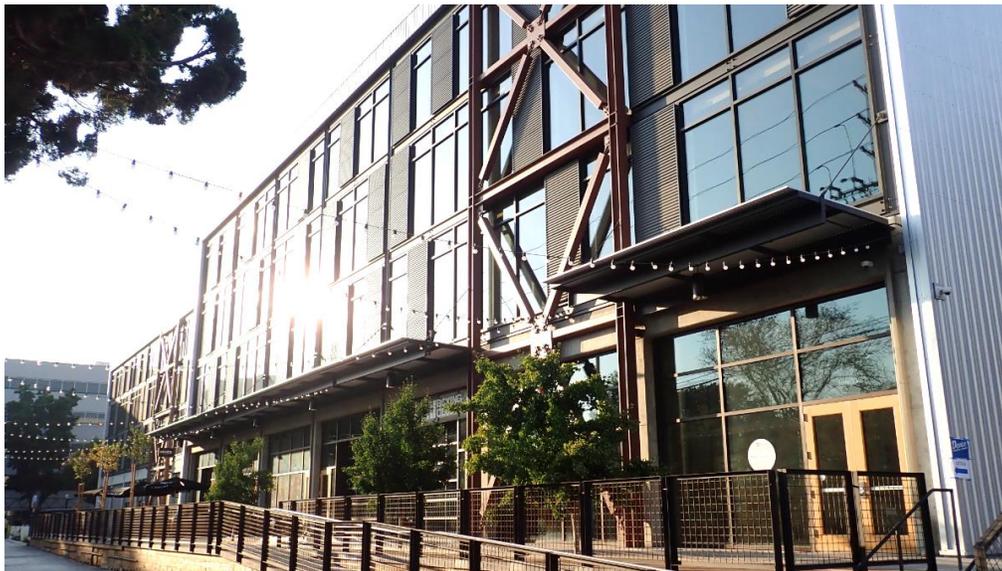
The internal street network within each of the Villages will reflect the residential nature of the neighborhoods. Narrow two lane streets will accommodate both vehicles and bicyclists at comfortable speeds. Ample tree canopy along residential street segments will provide shaded sidewalks and walkways during warm summer months.

Roads D and E, which serve as collectors through the Urban and East Village districts, as well as the Village Center residential areas, will similarly reflect the residential make-up of the adjoining land use. These collector roadways will consist of two travel lanes accommodating vehicles as well as bicyclists through the use of painted on-street sharrows/shared lane marking. Traffic calming will be achieved through on-street parking on both sides of the street as well as other measures such as speed tables or enhanced crossings.

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3.0 | LAND USE REGULATIONS, DEVELOPMENT STANDARDS AND DESIGN GUIDELINES







3.1 | PURPOSE AND INTENT

Land use and development within the Plan Area is subject to conformance with the permitted uses, the site development regulations, development standards and design guidelines as outlined in this chapter. The permitted land uses and development standards/design guidelines are informed by the Vision, by the District Concepts, Character and Specific Plan Policies described in Chapters 1 and 2. These elements work together to create a use-compatible, visually distinct and vibrant community while retaining appropriate design flexibility at the project level. Engaging, yet seamless transitions between innovative research spaces, multi-functioning parks and network trails, and new and existing neighborhoods within the Plan Area will result in a sense of place that is both internally distinctive and a natural extension of the Woodland community. The intent is to provide a clear and concise set of regulations, standards and guidelines that builders, developers, property owners and City staff can rely on to implement development within the Plan Area in an efficient manner.

This chapter establishes the permitted land uses and primary development standards within the Plan Area and supersedes the requirements of the Zoning Ordinance (Title 17) of the Woodland Municipal Code (WMC) and the Woodland Community Design Standards. Where direction is not provided in this Specific Plan, the provisions of the WMC may be considered. The Community Development Director shall have the authority to determine the most appropriate zoning correlation between the Specific Plan and WMC where necessary. If an issue or situation arises that is not sufficiently provided for in the Specific Plan, those regulations of the Zoning Ordinance that are most applicable shall be used by the Community Development Director as guidelines to resolve unclear issues. Additional information pertaining to the implementation of this Chapter including interpretations and modifications can be found in Chapter 7, Administration.

3.2 | ZONING CLASSIFICATIONS

3.2.1 Purpose

To carry out the purpose and provisions of the Specific Plan, the Plan Area is divided into land use designations. This chapter contains general information such as statements of purpose and expectations for each land use type. The precise use regulations for each land use designation are found in Section 3.3. The Development Standards are found in Section 3.4.

The Land Use Plan, Exhibit 2.2, depicts the land uses assigned to all portions of the Plan Area. The 14 land use zones fall under 4 sub-categories; Research/Tech Park, Commercial and Mixed Use Zones; Residential Zones and Open Space/Park Zones. A general summary of the zoning designations is provided in Section 3.2.4.

3.2.2 Applicability

No building, structure, vehicle, sign, or area shall be used, altered, moved, enlarged, or stored in any zoning district except as specifically allowed in underlying zoning district and by the regulations of any applicable combining district.

3.2.3 Prohibition

No property in any zoning district shall be used in any manner so as to create problems contrary to public health, safety, or general welfare or so as to constitute a public nuisance.

3.2.4 Description of Zoning Categories

A. Employment: Research Technology Park and Commercial Use Zones

The employment zones include the Research Technology Park, Highway Commercial and Village Center Mixed Use. The Research Technology Park category has the primary zone of RTP and three (3) RTP overlay zones: Transitional, Community Commercial and Research Flex. The purpose of the RTP overlay zones is to “fine-tune” the list of uses and development standards in the designated locations to provide compatibility with surrounding uses and to purposely target specific uses to certain locations within the Technology Park Districts.

- **RTP-Research Technology Park:** The RTP zone provides for a campus-like environment with a variety of parcel sizes designed to accommodate a wide variety of technology and research based businesses, including but not limited to large research and development operations with production and point-of-sale facilities, laboratory, light manufacturing and assembly, professional/corporate offices as well as a limited list of other business support uses.
- **RTP/TO-Research Technology Park/Transitional Overlay:** The RTP/TO zone is similar to the RTP base zone, however with the RTP/TO location more internal to the District and abutting the Village Center, the more intense industrial uses are limited and the professional offices, retail and personal services uses are expanded.
- **RTP/CCO-Research Technology Park/Community Commercial Overlay:** The RTP/CCO zone is similar to the RTP base zone, however the RTP/CCO location at the primary gateway on Road B anticipates integration of commercial uses that complement the RTC uses.
- **RTP/RFO-Research Technology Park/Research Flex Overlay:** The RTP/RFO zone is similar to the RTP base zone with a greater emphasis on light manufacturing uses and research facilities.
- **HC-Highway Commercial:** The HC zone accommodates uses such as hotels, vehicle service stations, restaurants and other retail or service uses serving the Plan Area, surrounding community and region.
- **VCMU-Village Center Mixed Use:** The VCMU zone accommodates smaller format commercial spaces and mixed-use buildings to attract retail/food venues and co-working spaces with integrated residential and office uses permitted. The density range for residential uses in the VCMU is 20.0-40.0 dwelling units per gross acre (du/ac).



B. Residential Zones

The 6 residential zones include 2 types of Low Density Residential, 2 types of Medium Density Residential and 2 types of High Density Residential.

- **LDR-Low Density Residential**: The LDR zone accommodates single family detached homes on standard size lots. Alternative lot configurations such as alley, cluster or halfplex lots may also occur. The density range is 1.0-8.0 dwelling units per gross acre (du/ac).
- **VCLDR-Village Center Low Density Residential**: The VCLDR zone accommodates a variety of home types for those that desire a centralized location in the Town Center. Alternative lot configurations such as alley, cluster or halfplex lots are anticipated in addition to detached standard lots. The density range is 1.0-8.0 dwelling units per gross acre (du/ac).
- **MDR- Medium Density Residential**: The MDR zone accommodates a range of attached and detached housing types, including small-lot subdivisions, duplexes, triplexes, zero-lot-line developments, townhouses and other housing types. The density range is 8.1-19.9 dwelling units per gross acre (du/ac).
- **VCMDR-Village Center Medium Density Residential**: The VCMDR zone accommodates a variety of housing opportunities for those that desire a more centrally located and lively residential experience in the Village Center. Contemporary row houses, townhouse, small lot, cluster and detached units are permitted. The density range is 8.1-19.9 dwelling units per gross acre (du/ac).
- **HDR-High Density Residential**: The HDR zone accommodates a broad range of attached housing types, including triplexes, fourplexes, row houses, and stacked flats (apartments or condominiums). Units can be contained in single structures or in a collection of cohesive structures with common open spaces and amenities. The higher density range provides for the opportunity for multi-story structures with centrally located parking in order to provide urban intensity. The density range is 20.0-40.0 dwelling units per gross acre (du/ac).
- **HDR/CCO-High Density Residential/Community Commercial Overlay**: The HDR/CCO zone accommodates the same uses as described for HDR but additionally permits retail, business and personal services, grocery stores, restaurants to promote a direct, walkable linkage between the adjacent technology park uses and surrounding neighborhoods to enable convenient access to goods and services. The density range is 20.0-40.0 dwelling units per gross acre (du/ac).

C. Open Space/Park Zones

- **VCOS- Village Center Open Space**: The VCOS zone applies to the park, known as The Yard. The uses in the VCOS are primarily recreation, however the zone does allow a variety of commercial uses which complement and support the Village Center. This includes the Promenade adjacent to the Yard.
- **OS-Greenbelts, Open Space and Public Drainage Facilities**: The OS zone applies to the greenbelts, linear open space corridors, and the 4.2-acre detention pond located at the southeast corner of the Plan Area. The uses allowed in the OS are generally passive recreation and stormwater management.

3.3 | PERMITTED USES

The purpose of this section is to establish land use classifications and to explain how land uses are regulated in this document. This section explains the Use Classification system, the allowed use and permit requirements, how uses not listed are regulated, and how similar uses are determined. It is not feasible to list every possible use, so general categories are provided, specific uses are identified as needed, and a process is provided to classify uses that do not clearly fit into a use classification.

In order to simplify land use regulations, land uses listed in the use have been grouped into general categories on the basis of common function, product, or compatibility characteristics. These general allowed use categories are called "Use Classifications." Use classifications describe one or more uses having similar characteristics but do not list every use or activity that may appropriately be within the classification. The following rules apply to use classifications:

A. Illegal Uses. No use that is illegal under local, state, or federal law shall be allowed in any Zone within the Plan Area.

B. Similar Uses. When a use is not specifically listed in this document, it shall be understood that the use may be permitted if the Director determines that the use is similar to other uses listed based on established criteria and required findings. It is further recognized that every conceivable use cannot be identified in this document and, anticipating that new uses will evolve over time, the Director may make a Similar Use Determination to compare a proposed use and measure it against those uses listed.

3.3.1 Permitted Land Uses and Requirements

Table 3.1 identifies permitted uses in each of the zoning designations. Generally, a use is allowed by right, allowed through issuance of a zoning administrator permit, a conditional use permit, is an ancillary use or is not permitted. Refer to the Woodland Municipal Code (WMC) for detailed description of each approval process.

Minimally, staff level Site Plan and Design Review approval is required for all new construction that requires a building permit with the exception of Tenant Improvements not visible from the public right of way. For residential uses, and for non-residential uses that are permitted by-right, Site Plan and Design Review shall be considered a ministerial action for the purposes of CEQA when plans conform to applicable Development and Design Standards. Site Plan and Design Review for non-residential uses that require a Zoning Administrator or Conditional Use Permit will be considered in conjunction with the review of the required permits, which are considered discretionary for the purposes of CEQA.

All uses are subject to the Performance Standards listed in Section 3.3.2 below and shall



demonstrate consistency with the 2035 Climate Action Plan (CAP) as outlined in Section 3.3.3.

The permitting requirements identified in these tables are:

A. Permitted (P). A land use shown with a “P” indicates that the land use is permitted by right in the designated Zone, subject to applicable provisions of this Specific Plan (e.g., development standards, Design Review) and subject to the Performance Standards listed in Section 3.3.2.

B. Zoning Administrator (Z). A land use shown with a “Z” indicates that the land use is permitted in the designated Zone upon issuance of a Zoning Administrator Permit from the designated Approving Authority, subject to compliance with all applicable provisions of this Specific Plan (e.g., development standards, Design Review) and subject to the Performance Standards listed in Section 3.3.2.

C. Conditional (C). A land use shown with a “C” indicates that the land use is permitted in the designated Zone upon issuance of a Conditional Use Permit from the designated Approving Authority, subject to compliance with all applicable provisions of this Specific Plan (e.g., development standards, Design Review) and subject to the Performance Standards listed in Section 3.3.2.

D. Ancillary (A). Ancillary to or in support of an approved primary use.

E. Not Permitted (X). A land use shown with an “X” in the table is not allowed in the applicable Zone.

3.3.2 Performance Standards

All uses as described in Section 3.3.1 are subject to the following Performance Standards. Any deviation or requested exceptions for the standards below are subject to elevated discretionary review and/or environmental review; the Community Development Director shall, at his/her discretion, elevate the permit to a Zoning Administrator or Conditional Use Permit. The performance standards are intended to ensure that uses and activities are conducted in a manner that protects the public health and safety and do not produce adverse impacts on surrounding properties or the community at large.

A. Lighting. All lighting, reflective surfaces, or any other sources of illumination shall be designed and located in a manner that produces no substantial glare on public streets or on any other parcel that could impact public safety or the enjoyment of private property. Lights shall be of the minimum illumination necessary for a given application and shall be directed downward and shielded at lot lines so as to confine all direct rays to the premises on which it is located. Exterior lighting shall be shielded as necessary to reduce offsite light/glare impacts.

B. Fire Hazards. The storage, use, transportation, or production of products which, either in the raw or finished state, constitute a flammable or explosive material shall be subject to approval of the Fire Department (as example, flour milling or nut shell processing). Burning of waste materials

in open fires or unapproved incinerators is prohibited.

C. Liquid or Solid Waste. The use, handling, storage, and transportation of waste materials, including hazardous wastes, shall comply with the provisions of the California Hazardous Materials Regulations and any other applicable laws. Discharge at any point into a public or private sewage disposal system, stream, or the ground, of any material that could contaminate any water supply, or otherwise cause the emission of dangerous or offensive elements is prohibited. No exceptions are allowed unless in accordance with regulations, licenses or approvals of the various local and state agencies having jurisdiction over such activities.

D. Odor, Particulate Matter and Air Contaminants. No continuous, frequent, or repetitive odors are permitted that are perceptible on or beyond adjacent lot lines or in the public right-of-way with the exception of food/beverage related odors typical of an urban/mixed use setting such as coffee shop, bakery, brewery related odors. An odor detected no more than a total of 15 minutes in any one day shall not be deemed to be continuous, frequent or repetitive as used in this subsection. No dust or particulate matter shall be emitted that is detectable beyond property lines by a reasonable person without instruments. Exhaust air ducts shall be located or directed away from abutting residentially-zoned properties.

E. Vibration. Machinery used for manufacturing and industrial processes, including oil and gas collection, processing, and distribution must be designed and housed to ensure that vibration will be reduced to an amount undiscernible without the aid of instruments by a reasonable person at the lot lines of the site. Vibrations from temporary construction, demolition, and vehicles that enter and leave the subject parcel (e.g., construction equipment, trains, trucks, etc.) are exempt from this standard.

F. Noise Standards. Noise produced by commercial and/or industrial machinery and/or equipment detected at the property line must be restricted to Monday through Saturday and not begin before 7:00AM and must cease after 6:00PM. Applicants for proposed projects may be required to provide evidence that all of the applicable requirements relating to noise may be satisfied by the project prior to approval. Provisions contained in Chapter 8 of the 2035 General Plan shall apply in addition to other applicable sections of the Municipal Code that relate to noise and nuisance considerations. Table 8.6 of the General Plan provides noise level performance standards that apply to the noise sources themselves for new projects and existing non-transportation sources.

E. Truck Trips. For any individual use, a limit of 25 diesel-powered trucks or 50 one-way diesel-powered truck trips, with no more than 40 one-way truck trips having diesel-powered transportation refrigeration units, shall be permitted per day. Logistics/Distribution uses are prohibited within the WRTP Plan Area as a primary use. Where ancillary to permitted uses, storage areas shall be limited to not more than fifty percent (50%) of gross building square footage and not more than 89,000 square feet, whichever is less.



3.3.3. Climate Action Plan Consistency

- A. All new development projects and major expansion projects shall provide a summary of incorporated conservation measures, consistent with the City's 2035 Climate Action Plan (CAP), adopted with the General Plan in 2017. For each CAP strategy and related "action" relevant to new development projects, the City will determine: (a) the project is consistent; (b) the project with conditions or when modified would be consistent; (c) the strategy is relevant for new development, but not the subject project; or (d) the project includes one or more replacement strategies that would be equally or more effective in reducing GHG emissions and such replacement strategy or strategies are not included in the CAP or required by any other regulation, standard, design criteria, or other existing requirement.
- B. All projects shall complete and submit the City's CAP Consistency Checklist for review and approval by the City prior to project approval. The CAP Consistency Checklist allows proposed development projects to demonstrate consistency with the CAP.
- C. Consistent with General Plan Policy 2.L.2, all projects shall strive to meet net zero energy consumption through the incorporation of conservation measures above Title 24 standards and shall, at minimum, demonstrate consistency with CalGreen Tier 1 standards.
- D. Additional GHG reductions strategies and sustainability measures shall be considered in major expansion projects and in the ongoing operations and use of all commercial and residential projects within the Plan Area including, but not limited to, Energy Conservation, Water Conservation/Quality and Low Water Landscape measures as outlined in Sustainability Guidelines Sections 3.5.3.B for commercial uses and Section 3.5.12.B for residential.

3.3.4 Temporary and Transitional Uses

The following special uses are encouraged in the Plan Area subject to the permitting requirements, performance standards and in the use descriptions below. The following limited term/temporary uses are intended to provide dynamic and diverse retail opportunities and increased foot traffic throughout the Plan Area, while supporting small startup companies, artists and entrepreneurs.

A. Pop-Up Uses and Mobile Food/Retail Vendors

Pop Up uses refer to the temporary activation of a vacant/underutilized storefront or property for commercial/retail or art/design showcase and sales purposes. Food/Retail Vendors refers to food trucks and mobile retailers.

1. Pop-Up uses and mobile food/retail vendors are temporary in nature, and subject to operating guidelines established by the Community Development Department and subject to a Pop Up and/or Mobile Food/Retail Vendor Permit ("Vendor Permit") issued by the Community Development Department.
2. Pop-Up uses located within a permanent structure do not require a final certificate of occupancy, but do require a temporary certificate of occupancy.
3. Unless otherwise approved in writing by the City's Community Development Department, pop-up and mobile vendors shall be located on private property, not within the public right-of-way. A current City business license and all applicable health and safety permits shall be maintained at all times by any pop-up and mobile vendors.

4. Pop-up hours of operation shall generally be limited to 8am to 10pm, unless otherwise approved by the City.
5. The permitted duration of the use shall be outlined in the Vendor Permit based on the type of use, location and whether they are associated with a specific event or function. Generally, mobile food and retail vendors shall be limited to 2 hour parking, unless associated with an event or function of longer duration. Pop-up uses located within permanent or semi-permanent structures may be permitted for up to 1 year, subject to annual renewal.
6. Pop-Up and mobile vendors are not required to provide off-street parking.
7. Pop-up uses must adhere to Specific Plan Design Standards and Guidelines, Building Code requirements for signage, restroom and life/fire safety and all Fire and Health Code requirements as applicable.

B. Temporary and Transitional Uses

1. Uses such as coffee kiosks, shipping container eateries/beer gardens, and other “low barrier” entry, transitional and temporary uses may be located within the commercial, Research and Technology Park and Village Center Districts subject to a Zoning Administrator Permit and Transitional/Temporary Use Agreement. Such uses are intended to activate underutilized parcels as more permanent projects are completed over time. The Zoning Administrator Permit shall establish the appropriate timeline for such uses.
2. Transitional Temporary uses must adhere to Specific Plan Design Standards and Guidelines, and Building, Fire and Health Code requirements for signage, restroom and life/fire safety as applicable.



TABLE 3.1: PERMITTED USES		ZONING DESIGNATIONS														
USE TYPES		RESEARCH AND TECH PARK				COMMERCIAL/MIXED USE			RESIDENTIAL					OPEN SPACE		
LEGEND P= Permitted Z = Zoning Administrator Permit C = Conditional Use Permit A = Ancillary to or in support of an approved primary use Blank-- Not Permitted		RTP	RTP/TO	RTP/CCO	RTP/RFO	HC	VCMU	HDR/CCO	HDR	VCMDR	MDR	VCLDR	LDR	VCOS	OS/GB	Foot notes
Research/Technology, Light Industrial/Manufacturing/Ag Production and Processing																
Accessory buildings	P/A (1)	P/A (1)	P/A (1)	P/A (1)	P/A (2)	P/A (2)			P (34)	P(34)	P (34)	P (34)	P(34)			1,2,34
Agricultural Production – Light (3)	P/A			P/A												3
Agricultural Uses/Structures (3)	P/A			P/A										P		3
Agricultural or Seed Processing/Packaging and Manufacturing	P	P	P	P												
Brewery/Distillery (see also Microbrewery)	P	P	P	P		P(4)										4
Commercial Cannabis Testing	C(5)			C(5)												5
Commercial Cannabis Manufacturing and/or Distribution				C(5)												5
Hemp Distribution	C(5)			C(5)												5
Employee oriented services/ amenities (e.g. fitness studio, cafeteria, day care center, ATM)	P/A	P/A	P/A	P/A	P/A	P								P (6)		6
Light Industrial	P	P(7)	P(7)	P												7
Medium Industrial	Z(8)			Z												8,9

LEGEND P= Permitted Z = Zoning Administrator Permit C = Conditional Use Permit A = Ancillary to or in support of an approved primary use Blank-- Not Permitted	RTP	RTP/ TO	RTP/ CCO	RTP/ RFO	HC	VCM U	HDR/ CCO	HDR	VCMDR	MDR	VCLDR	LDR	VCOS	OS/GB	Foot notes
Research and Development including agri-tech, seed research, plant breeding, and similar uses	P	P	P	P		P(4)									4
Specialty Food Processing	P	P	P	P											
Offices, Business and Professional															
Banks		P	P			P	P								
High-Tech Office and Corporate Headquarters	P	P	P	P		P(4)	P(4)								4
Medical or Dental Offices		P/A	P			P(12)	P								12
Professional and Administrative Offices	P	P	P	P	P/A	P	P								
Retail and Personal Services															
Beauty/Salon/Spa		P	P		P/A	P	P	P(24)	Z(13)		Z(13)		P(6)		6,13,24
General Personal Services		P/A	P		P/A	P	P	P(24)					P(6)		6,24
General Retail (small 25,000sf or less)		P/A	P		P/A	P	P	P(24)	Z(13)		Z(13)				13,24
General Retail (large 25,000-60,000sf)		P	P		P	Z(14)	Z(25)								14,25
Health/Fitness/Recreation Facility	P/A	P/A	P	P/A	P	P(15)	P(25)	P(15)	Z(13)		Z(13)				13,15,25
Large Format Retail (over 60,000sf)			Z(16)		P										16
Service/Gas Stations					Z(17,18)										17,18

Chapter 3 | Land Use Regulations, Development Standards and Guidelines



LEGEND P= Permitted Z = Zoning Administrator Permit C = Conditional Use Permit A = Ancillary to or in support of an approved primary use Blank-- Not Permitted	RTP	RTP/TO	RTP/CCO	RTP/RFO	HC	VCMU	HDR/CCO	HDR	VCMDR	MDR	VCLDR	LDR	VCOS	OS/GB	Foot notes
Eating and Drinking Establishments															
Bar/Night Club		C/A			C	C									
Micro-brewery/Wine Bar/Brewpub		P	P		P	P	P						P(6)		6
Restaurant/Café		P	P	P/A	P	P	P		Z(13)				P(6)		6,13
Restaurant w/ Drive-Thru					Z										
Food and Beverage Sales															
Convenience Market	P/A	P/A	P	P/A	P/A	P	P								
Farmer's Market / Produce Stands		Z/A(19)	P(19)	Z/A(19)		P/A(19)	P(19)						P(19)		19
General Grocery		P	P			P(20)	P								20
Visitor Accommodations															
Bed and Breakfast						Z		Z	Z						
Conference Center	Z	Z	Z	Z	P										
Homestay or Short Term Rental								Z(21)	Z(21)						21
Hotel	Z	Z	Z	Z	P	P (33)									33
Residential Uses															
Accessory Dwelling Unit							P(36)	P(36)	P(36)	P(36)	P(36)	P(36)			36
Single Family Detached									P	P	P	P			
Single Family Attached									P	P	P	P			
Duplex/Halfplex									P	P	P	P			
Townhouse/Row House						P(22)	P	P	P	P	P	P			22

LEGEND P= Permitted Z = Zoning Administrator Permit C = Conditional Use Permit A = Ancillary to or in support of an approved primary use Blank-- Not Permitted	RTP	RTP/ TO	RTP/ CCO	RTP/ RFO	HC	VCM U	HDR/ CCO	HDR	VCMDR	MDR	VCLDR	LDR	VCOS	OS/GB	Foot notes
Multi-Unit Dwelling (3 or more units)						P(22)	P	P	P	P	P(37)				22,37
Home Occupation,Live/Work							P(35)	P(35)	P(35)	P(35)	P(35)	P(35)			35
Family Day Care Homes Less than 14 Children									P	P	P	P			
Family Day Care Homes More than 14 Children									P	P	P	P			
Public, Education, Civic and Governmental Uses															
Religious Institution							C(23)	Z		Z		C			23
Commercial Daycare	P(26)	P(26)	P(26)	P(26)			P(26)	P(26)	Z	Z	Z	Z	P(6)		26,6
Education and Training Services / Vocational School	P	P	P	P		P(4)	Z(29)								4,29
College/University	P	P	P	P			P								
TK/K-12 Public/Private School										P(28)					28
Parks, Recreation, Open Space															
Public or Private Park/Open Space	P	P	P	P	P	P	P	P	P	P	P	P	P	P	
Water impoundment/detention	P	P	P	P	P									P	
Community Garden							P	P	P	P	P	P	P	P	
Temporary or Other Uses															
Mixed Use	P(27)	P(27)	P(27)	P(27)	P(27)	P(27)	P(27)								27
Mixed Use w/ Residential						P(27)	P(27)	P(27)	P(27)	P(27)	P(27)				27
Outdoor Materials or Vehicle Storage	P/A(30)	P/A(30, 31)		P/A(30)											30,31



LEGEND P= Permitted Z = Zoning Administrator Permit C = Conditional Use Permit A = Ancillary to or in support of an approved primary use Blank-- Not Permitted	RTP	RTP/ TO	RTP/ CCO	RTP/ RFO	HC	VCM U	HDR/ CCO	HDR	VCMDR	MDR	VCLDR	LDR	VCOS	OS/GB	Foot notes
Transportation/Passenger or Park-and-Ride Facility	Z/A	Z/A	Z/A	Z/A		Z/A									
Pop-Up Food/Retail Vendors (i.e. Food Truck/Mobile Retail)	P(32)	P(32)	P(32)	P(32)	P(32)	P(32)	P(32)						P(32)	P(32)	32
Special Events	P(32)	P(32)	P(32)	P(32)	P(32)	P(32)	P(32)						P(32)	P(32)	32

Table 3.1 Footnotes

1. Limited to 25% of the total net leasable building area
2. Limited to 15% of the total net leasable building area
3. Existing agricultural uses may continue until the area is required for the development of infrastructure or other allowed uses. Agricultural operations shall comply with applicable laws and regulations. Greenhouses and agricultural test plots are permitted indefinitely when associated with Research and Development.
4. If less than 10,000 square feet net leasable building area.
5. Subject to applicable Woodland Municipal Code Commercial Cannabis Business provisions
6. Up to 1.5 acres of retail/commercial uses permitted in the park consistent with master park design plan.
7. Outdoor storage/processing shall be located at least 75 feet from Road B, be screened and clearly in support of and incidental to a primary building and use of property.
8. Zoning Administrator Permit required if conducted wholly or partially outdoors
9. Outdoor manufacturing/assembly shall be located at least 75 feet from Road B and shall be screened from Road B.
10. Placeholder
11. Placeholder
12. If under 5,000sf and part of a mixed-use development project.
13. Adjacent to VCOS/The Yard only; 2,000 s.f. max.
14. Permitted as part of a comprehensive mixed-use project.

15. Maximum 5,000 s.f. In HDR, maximum of 5,000 s.f. and must be ancillary to a multifamily residential project.
16. Permitted as part of a master planned commercial center requiring a Zoning Administrator Permit.
17. A carwash is permitted as an ancillary use to and specifically a component of a permitted gas station. Standalone carwashes are not permitted.
18. A maximum of one service/gas station is permitted within the Highway Commercial Zone.
19. Subject to Woodland Municipal Code Section 5.12, Farmer's Market License.
20. Permitted if under 20,000 s.f. Zoning Administrator Permit required for larger format grocery store up to 50,000 s.f. max.
21. Allowed subject to implementation of citywide ordinance.
22. Residential unit(s) shall be located above ground floor non-residential uses as part of a mixed-use project.
23. Churches/Religious Institutions shall be located along Road D or Road C within the HDR/CCO zone, away from Road B.
24. Commercial/retail uses within the MDR and HDR zones are permitted as part of a mixed-use project along the perimeter of a subdivision/development project, limited to not more than 2,000 s.f. and shall be compatible with residential uses. Appropriate uses include the following or similar: general retail (small format), personal services (i.e. salon/spa, medical clinic), business related services (i.e. mail/copy center, tech retail), professional offices, tech firms, café/restaurant, convenience market. At the discretion of the Community Development Director, conditions that limit noise, odor or other potential impacts to adjoining residential uses may be required and/or the Director may elevate review/approval to a Zoning Administrator Permit or Conditional Use Permit.
25. Large format retail and health/fitness/recreation facilities up to 60,000sf are permitted within the HDR/CCO zone along Road B only.
26. Permitted (ancillary to) as part of a multifamily project or commercial development; not permitted as a standalone use on a parcel.
27. Mixed uses are permitted, consistent with the manner in which the individual uses are otherwise allowed in the zone. The review required will be dependent upon the most restricted use intended for the development.
28. A public school is permitted only within the MDR zone located south of Parkland Avenue, north of Road E, between Road B and the HLA greenbelt.
29. Trade or Professional Schools compatible with adjacent residential uses (i.e. produce low noise, odor, dust, vibration, etc.) are permitted within the HDR/CCO zone along Road B only, subject to a Zoning Administrator Permit.
30. All materials stored outside must be screened from public view.
31. Outdoor storage shall be at least 75 feet from Road B, be screened and be clearly in support of and incidental to a primary building and use of property.
32. Subject to Woodland Municipal Code Special Events provisions and Special Use Regulations, Section 3.3.3 as applicable.
33. Boutique hotel only.
34. Subject to Woodland Municipal Code Accessory Buildings and Uses provisions.
35. Subject to Woodland Municipal Code Home Occupation provisions.
36. Subject to Woodland Municipal Code Accessory Dwelling Unit provisions.
37. Not more than 4 units shall be permitted on a single parcel.



3.4 | SITE DEVELOPMENT STANDARDS

3.4.1 Format and Use of Development Standards and Design Standards and Design Guidelines

This document provides both **Site Development Standards and Design Standards and Design Guidelines (DS/DG)**. Together, the development standards and DS/DG contained in this chapter seek to promote coordinated, sound development, effective use of land, and high quality site planning that considers the unique character and context of the community. These development standards and DS/DG implement the goals and policies of the W RTPSP and the General Plan.

The **Site Development Standards**, Section 3.4.2, define the dimensional requirements that apply to structures built within each of the land use designations, including required lot sizes, lot coverage; setbacks; building and structure heights; and other requirements related to the building envelope, location, and configuration of buildings and structures which are unique to the Plan Area. The Design Standards and Design Guidelines, found in the following Section 3.5, address the desirable features of the land uses identified in the Specific Plan to achieve a desired level of quality for the physical environment.

3.4.2 Site Development Standards

The site development standards contained herein set the basic, quantitative requirements for development that support the goals and policies of the SP. The site development standards included herein are intended to establish the minimum design parameters. The intent of these standards is to permit flexibility to encourage a wide range of building types and innovative designs. It is anticipated that over the life of the Specific Plan, modifications to development standards and introduction of additional building types may be requested as part of specific site design, tentative map and design review process. Modification(s) to the standards as proposed by a developer/builder during site development and design review of individual Planning Areas and/or Tentative Maps, may be considered subject to the procedures outlined in Chapter 7, Section 7.4.1 via a Specific Plan Amendment or a Minor/Administrative Modification. The Site Development Standards are provided in Tables 3.2 for the Research Technology Park and Commercial Zones and Table 3.3 for Residential Zones.

Table 3.2: Site Development Standards for RTP, Commercial and Mixed Use Zones						
ZONING	RTP	RTP/TO	RTP/CCO	RTP/RFO	HC	VCMU
Site and Building Criteria						
Max Lot Coverage(1)	70%	70%	70%	70%	60%	100%
Floor Area Ratio(2)	0.20 min 2.0 max	0.20 min 2.0 max	0.20 min 2.0 max	no min 2.0 max	0.25 min 2.0 max	0.6 min 4.0 max
Max Building Length	650' (20)	400' (3)(20)	400' (3)(20)	650' (3)(20)	200'	50' (4)
Min/Max Lot Size	None	None	None	None	None	None
Streetwall Frontage Minimum(10)	None	Road B 60%(20)	Road B 60% (20)	None	None	Road B and Marston 70 %
Minimum Common Open Space(5)	5%	5%	5%	5%	5%	No fixed standard
Setbacks and Separations						
Front Setback(6)(7)	10' min no max (20)	5' min (8) 20' max along Road B(20)	10' min 25' max, 20' max along Road B(20)	10' min no max(20)	10' min no max	5' min 15' max (8)(9)
Side (interior) and Rear Setback	10' side and rear (11)(21)	10' side and rear (21)	10', 20' max from Road 25A	10' (11)(12)	10'	No fixed standard (13)
Building Separation	15' min (1-3 stories) 30' min (4+)	15' min (1- 3 stories) 30' min (4+ stories)	No fixed standard	15' min (1- 3 stories) 30' min (4+ stories)	No fixed standard	No fixed standard
Building Height⁽¹⁸⁾						
Min/Max Height (19)	No min 65' max	25' min 65' max	25' min 65' max	No min 65' max	No min 70' max	25' min 55' max
Vehicular and Bicycle Parking						
Vehicle Parking	Per Parking Use and Needs Statement (14)	Per Parking Use and Needs Statement (14)(16)	Per Parking Use and Needs Statement (14)(16)	Per Parking Use and Needs Statement (14)	Per Parking Use and Needs Statement (14)	Per Parking Use and Needs Statement (14)(17)
Bicycle Parking (15)	10% of vehicular spaces	10% of vehicular spaces	10% of vehicular spaces	10% of vehicular spaces	10% of vehicular spaces	10% of vehicular spaces

**Table 3.2 Footnotes for RTP and Commercial Zones:**

1. Lot coverage shall be determined by the total area of the footprint of all structures on a lot divided by the total gross lot area. Greater lot coverage may be permitted subject to compliance with minimum development standards, including, but not limited to parking, landscaping and storm drainage.
2. Floor Area Ratio (FAR) may be exceeded as determined by the City Council if a proposed project is within 500 feet of publicly accessible and usable open space and/or provides a community benefit not otherwise required by the City, such as additional publicly accessible open space; public art; landscaping that exceeds minimum City requirements; buildings of exceptional architectural quality; building massing and articulation that serves to reduce the visual impact of higher FAR beyond minimum requirements, and/or other community benefits.
3. Buildings over 300' in length shall demonstrate exceptional design and architectural detail where fronting Road B to actively engage the streetwall and pedestrian realm. Enhanced design elements shall include, but are not limited to well-articulated entryways, variation in massing, authentic architectural detailing and transparent glazing at the street level.
4. Maximum distance between building entrances; building breaks not required.
5. Usable open space may include a combination of shared paseos/pathways as well as courtyards/plazas or private greenspace. Calculations based on gross floor area of building(s).
6. Setbacks shall be measured from the edge of the ultimate right-of-way adjoining the lot, as identified by street sections in Chapter 4 of the Specific Plan.
7. Decorative landscaping or enhanced pedestrian walkways/gathering spaces shall be incorporated within front setback.
8. Upper floors over three stories should step back a minimum of 5 feet from the required minimum setback, if fronting Road B.
9. Within the VCMU, the maximum 15-foot front setback may be increased to allow for a well-designed outdoor dining/gathering space that includes vertical and horizontal elements which simulate a streetwall feel such as a pergola, landscaped trellis, umbrellas, and string lights.
10. Zone standard represents the minimum percentage of the parcel/property that shall be developed at the minimum setback line. Flexibility to increase the minimum setback may be granted to allow for well-designed outdoor space that includes vertical and horizontal elements and other circumstances where additional building setback is necessary.
11. Buildings adjacent to Hwy 113 shall maintain a 20-foot landscaped buffer between the building and the Cal Trans right-of-way.
12. A minimum 150-foot buffer is encouraged, where feasible, along the southern edge of the Plan Area, adjacent to agricultural lands along the urban limit line.
13. 10' minimum setback from residential uses on adjacent parcels (i.e. MDR zone)
14. On-site parking shall be determined by business owner/applicant based on information provided by owner/applicant in a parking use and needs statement. Maximums shall be based on the standards outlined in Section 3.5.3.F for RTP/HC, or 3.5.9.G.6 for VCMU.

Table 3.2 Footnotes for RTP and Commercial Zones (Continued):

15. The number of bike parking spaces provided shall be a minimum of 10 percent of the total number of vehicular spaces provided or credited to the project.
16. Not more than 30 percent of the Road B frontage shall be permitted for on-site vehicle parking, including aisles and backup area.
17. On-site parking shall not be permitted along Road B within the VCMU zone.
18. The building height measuring point (BHMP) shall be established at the existing grade at the mid-point of the building façade of the principal building that is closest to a street.
 - a) The height of a building with a flat roof shall be measured from the BHMP to the highest point of the roof excluding parapets and balustrades not exceeding 4 ft. in height.
 - b) The height of a building with a roof that is not a flat roof shall be measured from the BHMP to the average level between the highest eave, not including the eave of a dormer and the highest point of the roof; and where there are no eaves, the average level shall be measured between the top of the highest wall plate and the highest point of the roof.
19. Height limitations shall not apply to architectural features such as tower elements, cupolas, domes, or similar, or parapets that do not extend more than 10 percent above the height of the building as defined in footnote 18.
20. Exceptions to the Maximum Building Length, Streetwall Frontage and Front Setbacks in the RTP, RTP/TO, RTP/CCO zones may be granted for large RTP users of buildings 100,000 square feet or larger subject to approval of the Planning Commission and a finding of consistency with the Specific Plan goals and policies and with the Specific Plan EIR. Building and site design elements shall include, but are not limited to: well-articulated entryways, inclusion of expansive common open space that provide visual and physical linkages to the VCOS, variation in massing, authentic architectural detailing and transparent glazing at the street level.
21. Parcel and building configurations in the RTP/TO zone may incorporate portions of the RTP zone in order to accommodate larger buildings, provided that the buildings achieve the design objectives of both the RTP and the RTP/TO zone, particularly the street frontage, the relationship to the public realm and creating pedestrian linkages.



TABLE 3.3: SITE DEVELOPMENT STANDARDS FOR RESIDENTIAL ZONES

ZONING	LDR-NORTH VILLAGE	LDR-EAST VILLAGE	VCLDR	MDR	VCMDR	HDR	HDR/CCO	FOOT NOTES
Site and Building Criteria								
Max Lot Coverage	50%	60%	60%	70%	70%	75%	75%	
Density(dwelling units /gross acre)	1.0-8.0	1.0-8.0	1.0-8.0	8.1 -19.9 (19)	8.1 -19.9 (19)	20.0-40.0 (20)	20.0-40.0 (20)	19, 20
Min. Lot Width	60'(9)	50'(9)	50'(9)	25'(9)	25'(9)	None	None	
Minimum Lot Size Interior/corner Detached lots	5000 int/ 6000 corner	4000 int/ 5000 corner	4000 int/ 5000 corner	None	None	None	None	
Minimum Lot Size interior/corner Duplex lots	6000 int/ 7000 corner	6000 int/ 7000 corner	6000 int/ 7000 corner	None	None	None	None	
Suggested Block Length/ Maximum	±400' to 600' / 800' max (4)	±400' to 600' / 800' max (4)	±400' to 600' / 800' max (4)	±400'/500' max (4)	±400'/500' max (4)	±400'max (5)	±400'/800' max (5)	4,5
Setbacks and Separations (1,2)								
Front Setback Min/Max (9)	20'/no max	15'/no max	10'/no max (6,8)	10' /20' (7)	5'/20' (6,7,8)	10'/30'	10'/30'	1,2,6,7,8,9
Min. Side Setback int/corner (9)	5'/15'	5'/15'	5'/15'	0'/10' (13)	0'/5' (13)	10'/10'	10'/10' (14)	1,2,9,13,14
Min. Rear Setback (9,10)	20' min (16)	15'	15'	5' (15)	5'(15)	0' (16)	10' (17)	1,2,9,10,15,16,17
Minimum Building Separation	n/a	n/a	n/a	10' for 1-2 stories 15' for 3+ stories	10' for 1-2 stories 15' for 3+ stories	10' for 1-2 stories 15' for 3+ stories	10' for 1-2 stories 15' for 3+ stories	
Building Height								
Min/Max (3)	0'/35'	0'/35'	0'/35'	0'/45'(11)	0'/45' (11)	2 story 25'/55'	2 story 25'/55'	3,11
Vehicular Parking								
Minimum spaces	2 garage spaces	2 garage spaces	2 garage spaces	1 garage space per 1 bedrm, 2 spaces /2+ bedroom	1 garage space per 1 bedrm, 2 spaces / 2+ bedroom	1 space /studio or 1 bedrm, 1.5 spaces /2 bedrm 2 spaces /3+ bedrm(18)	1 space /studio or 1 bedrm, 1.5 spaces /2 bedrm 2 spaces /3+ bedrm(18)	18
Bicycle Parking								
Minimum spaces	n/a	n/a	n/a	1/unit	1/unit	1/unit	1/unit	

Table 3.3 Residential Site Standards Footnotes:

1. Where applicable (i.e. adjacent to public right-of-way), setbacks shall be measured from the edge of the ultimate roadway right-of-way adjoining the lot, as identified by street sections in Chapter 4 of the Specific Plan.
2. Minimum setbacks shall be governed by the Uniform Building Code and Fire Code for use or occupancy and type of construction.
3. The maximum building height measurement is as noted in the table, except where modifications/exceptions are permitted in the City's Zoning Ordinance for such design features as chimneys, cupolas, flag poles or similar.
4. Where residential block sizes exceed 600 feet in length, a mid-block paseo shall be provided. See Section 3.5.12.A.18.
5. Max 300' between pedestrian access points/mid-block paseo
6. Residential units adjacent to The Yard shall front or appear to front on to the park (i.e. units may include double frontages or provide secondary access from a rear alley).
7. If fronting Marston, min. 15' front setback from back of path, including 2' clear recovery zone (see Chapter 4, Street Section F and C-1).
8. When adjacent to The Yard (VCOS), setback shall be 5' min/15' max as measured from the eastern edge of the Pedestrian Promenade.
9. For rear/alley load garages, courtyard/T style unit layouts or other non-traditional subdivision layout, setback and/or lot width deviations will be considered on a case by case basis based on a finding that adequate unit separation for light, air and ventilation and private open space is still maintained as determined by the Community Development Director. For non-traditional subdivision layout, alleys or shared space may be counted in setback calculations. Where projects are located adjacent to existing residential development, setback standards shall be maintained except as provided for in Chapter 7, Section 7.4.1.2, Minor/Administrative Modification.
10. Residential units shall not back on to greenbelts trails or pedestrian paths/paseos.
11. Maximum height may be increased for exceptional design quality. Massing and design shall consider pedestrian scale and proximity/adjacency to lower story residential units.
12. A minimum of 60 percent of the Road B frontage ground floor uses within the HDR/CCO zone shall consist of commercial/retail uses. Commercial use is required at the corner of CR25 and Road B. See Section 3.5.13.A.3. Exception provided for affordable housing project (see Section 3.5.13.A.3)
13. 5' minimum interior side setback if adjacent to LDR/VCLDR Zone.
14. 20' maximum side yard setback if adjacent to Road B or CR 25A.
15. 10' minimum rear yard setback if abutting LDR/VCLDR zone.
16. 10' minimum rear yard setback if abutting Road B; 30' minimum rear yard setback if abutting Hwy 113.
17. 20' minimum rear yard setback if abutting CR 25A.
18. HDR and HDR/CCO shall provide 1 guest space per every 5 units and 1 manager space.
19. Within the VCMDR and MDR Zones, units fronting The Yard or fronting Marston Drive shall



be developed at or above 15 du/ac, consistent with a townhome or row house product type.

20. See Chapter 6, Section 6.5.3, RDZ Density Requirements.

3.5 | DESIGN STANDARDS AND DESIGN GUIDELINES

3.5.1 Purpose

The design standards and guidelines contained in this chapter seek to encourage innovation in development, to provide a quality community that meets anticipated demands for housing, services and employment while allowing a variety of approaches to design that establishes a sense of place in a healthy, safe and sustainable manner. Together these Design Standards and Design Guidelines (DS/DG) address the desirable features of the land uses identified in the Specific Plan and to achieve a desired level of quality for the physical environment. Design Standards are a set of threshold requirements. Design Guidelines are a set of discretionary statements/recommendations.

The purpose of the DS/DG is to provide design recommendations and guidance for projects within the Plan Area, with the overarching goal to result in sustainable developments that are cohesive and well designed. The DS/DG inform development in ways that are environmentally conscious, economically sound and, when properly applied, achieve quality design, improve the community's safety, health and livability. The DS/DG emphasize projects that complement the character of the Plan Area and surrounding community, support pedestrian and bicycle friendliness and embrace a broad spectrum of sustainable practices.

3.5.2 Format of the DS/DG

Required Design Standards provided in this section are recognizable by the words "shall". Design Guidelines provide direction and vision for preferred outcomes and are recognizable by the words "should". The DS/DG are organized into three sections by District- Research and Technology Park, Village Center and The Villages. The General DS/DG for each District are provided first, then Special Character Guidelines follow for selected zones within the District. The content is as follows:

Part 1:

- General DS/DG for zones within the Research and Technology Park District (RTP, RTP/TO, RTP/CCO, RTP/RFO, HC)
- Special Character Guidelines for RTP, RTP/TO, RTP/CCO, RTP/RFO, HC

Part 2:

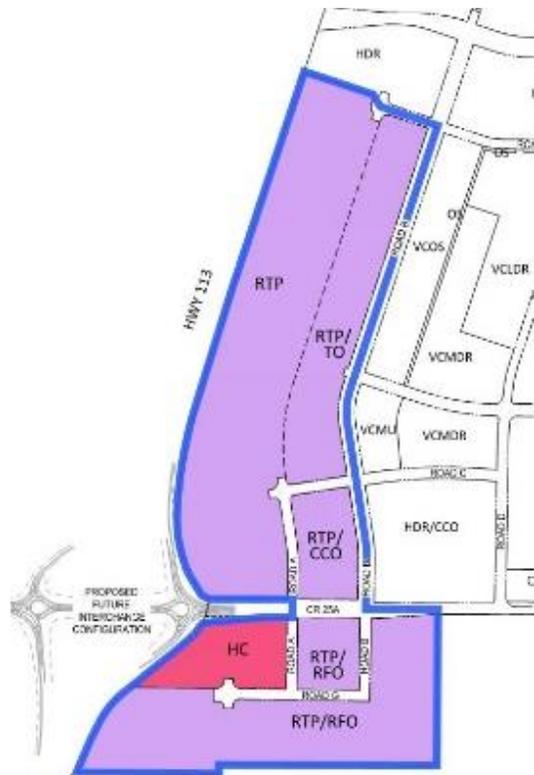
- DS/DG for zones within the Village Center District (VCMU, VCLDR, VCMDR, VCOS)

Part 3:

- General DS/DG for zones within The Villages District (LDR, MDR and HDR, HDR/CCO)
- Special Character Guidelines for HDR, HDR/CCO, MDR and LDR East Village and LDR North Village

PART 1: DESIGN STANDARDS AND DESIGN GUIDELINES FOR RESEARCH AND TECHNOLOGY PARK DISTRICT

The Research and Technology Park District encompasses the North and South Campuses. Section 3.5.3 addresses the general standards and guidelines which apply to all zones within the Research and Technology Park (RTP) District, as shown in the figure to the right. In addition, five (5) Special Character Guideline Sections are provided. Special Character Guidelines address the unique characteristics or features in all 5 zones within the District and are intended to complement and enhance the broader RTP DS/DG in Section 3.5.3. If conflicts arise between the General Guidelines and the Special Character Guidelines, the Special Character Guidelines shall take precedence.



3.5.3 General DS/DG for all Zones within the Research and Technology Park District

The following standards and guidelines apply to all development within the RTP, RTP/TO, RTP/CCO, RTP/RFO and HC zones.

A. Architecture and Massing

Architectural Style and Building Materials

1. Building design within the North and South Campus should incorporate and convey a clean, contemporary, ag-tech/ag-industrial architectural style that takes inspiration from the agricultural roots of Woodland.
2. Simple yet varied architectural design is preferred. Materials and colors should be well thought out to provide a unified and attractive exterior, but should generally be limited to one base material and not more than two accent materials to avoid an overdone or busy appearance.
3. High quality and durable materials such as solid wood, metal, glass, stone, concrete, and brick, shall be used on all buildings, with particular attention paid to finish types and



Buildings within the North and South Campus shall utilize high quality and durable materials and convey a clean, contemporary or ag-tech/ag-industrial style.



- surface textures that enhance visual interest and variety particularly at the pedestrian level.
4. Naturally finished metal, concrete (board-formed encouraged), and wood are appropriate as accent materials.
 5. The use of stucco siding is permitted but should be limited. Heavy knock-down or “lace” stucco finish is not appropriate.
 6. Matte finishes are preferred; highly reflective surfaces are generally not appropriate and should be used sparingly as accents only.
 7. A neutral, yet contemporary color scheme that incorporates limited, bold accent colors is encouraged.

Entryways



Building entrances should incorporate distinctive design elements as well as awnings and lighting for employee and visitor comfort and security.

8. Entryways should be harmonious in design with the overall building and have a defined relationship with fronting streets or pedestrian-oriented common areas where secondary access is provided.
9. Building entryways should be distinct, simple and clearly visible to pedestrians.
10. Primary entrances should be recessed with proper overhead covering and lighting as well as distinctive design elements, landscaping or art. Entryways should be properly sized to provide covering and shelter year-round.

Highway 113 Interface

11. Exterior building facades facing SR 113 shall be of the same or similar design quality as the primary building frontage including materials, massing and fenestration to the extent feasible. Long expanses of blank walls and unarticulated facades should be balanced in the overall scale of the building where visible from the public right-of-way.
12. Loading bays and parking areas along SR 113 must be substantially landscaped or screened from public view.

Limit Blank Walls

13. Building facades shall provide distinction and interest through the use of varied wall planes and materials, awnings, fenestration and other appropriately scaled architectural details that create a sense of depth on the wall surface.

Windows

14. For commercial and retail storefronts, windows should be a minimum of 9' feet in height and inset or recessed from the wall plane for visual interest and shadow line.
15. Highly reflective or opaque window glazing is generally not appropriate.
16. Proper glazing should enhance visibility and connection between indoor and outdoor spaces, particularly at the pedestrian/street level.



Proper window glazing should enhance visibility and connection between indoor and outdoor spaces, particularly at the street level.

Rooflines and Cornices

17. Rooflines should be considered in the overall building design and should provide an attractive termination of the building through the use of a decorative cornice or parapet, continuous banding or trim or other architectural features that help clearly define the building edge.
18. Shed and flat roofs are more typical of contemporary buildings, however a simple gable roof can also appear modern with appropriate pitch and use of materials. A mix of simple, projecting cornices and overhangs as well as rooflines that feature a clean edge profile are encouraged.
19. When applicable, decorative parapets should be extended around the sides of a building and contain necessary depth to appear as an integral part of the building design and form.
20. Articulation of the building facade and roof line should be considered when a flat roof is provided to help break up rectangular massing.

Light Fixtures

21. Building and on-site lighting shall reinforce and enhance the architectural style of the development project.
22. Where appropriate and complementary, up-lighting or rooftop lighting is encouraged to accentuate major architectural details, building form and/or landscape elements at night.
23. Lights shall be properly shielded to preserve a "dark sky."
24. Required illumination shall, at minimum, meet Uniform Security Code requirements as set forth in Title 15 of the Woodland Municipal Code.

B. Sustainable Design

Energy Conservation

1. A comprehensive approach to energy conservation should be employed in individual projects (new construction and/or expansion), in ongoing operations and use, as well as in collaboration with other campus-wide initiatives that may be developed, including the following strategies:



- Work spaces should be designed to support direct access to natural light for as many occupants as possible to reduce the demand and use of artificial lighting and improve worker comfort and efficiency.
- Use of street trees, shading devices, cool pavements in parking lots and cool roofs, to reduce heat gain and reduce the urban heat island effect.
- Use of high-performance building envelopes, including insulation of floors, walls, and ceilings that exceed Title 24 standards, use of low-e glass and other high-performance curtain wall or glazing systems.
- Employment of heat recovery ventilation within residential or non-residential buildings, which rather than venting warm exhaust air outside, captures the exhaust air and uses it to preheat incoming air.
- Use of energy-efficient appliances and lighting that meet, at minimum CalGreen Code Tier 1 standards.



Natural and energy efficient lighting should be incorporated where feasible to improve building efficiency and employee comfort.

Sustainable Materials and Construction Practices

2. Sustainable, local and renewable building materials should be used and verified whenever feasible.
3. Projects shall strive to reduce or eliminate waste during design, construction, operation and end of life in order to conserve natural resources.
4. The use of low-carbon cement is encouraged whenever feasible and/or the use of alternative materials.
5. Consider prefabricated building/structural components when feasible.
6. Leftover material stock should be recycled, donated or returned for alternative use at the completion of construction.

Water Conservation/Quality



Low impact development techniques including bioswales allow for the collection and filtration of stormwater runoff.

The following guidelines are recommended to minimize water use and/or improve water quality:

7. Use high-performance water fixtures that reduce water consumption and reduce demand on the water supply and the amount of waste-water entering the waste-water system.

8. Design streets, parking lots, and building surfaces to collect stormwater run-off using low impact development techniques, such as bioswales or other biofiltration systems.

9. Capture rainwater in cistern or other devices to supply water for landscape irrigation during dry months or channel rainwater from building rooftop drains to landscaping in common areas.

Low-Water Landscape

10. Landscaping should consist of climate-appropriate plantings, including drought-tolerant and native species suited to Woodland.
11. Use of permeable and porous pavement in parking areas to treat and attenuate stormwater flows should be used, where feasible, to reduce stormwater run-off. Trees and plants conducive to absorbing stormwater particulates, are encouraged within these planting areas.
12. Landscaping should consist primarily of mulch, flower planting beds, and naturalized groundcovers, including native grasses and shrubs. Non-living groundcovers, such as bark, cobbles, and stones are also encouraged to supplement the primary groundcover and, thereby, reduce maintenance and irrigation demand.
13. Install climate sensitive irrigation systems and other water-efficient bubblers and drip irrigation systems.
14. Coordinate with the City on use and treatment of recycled water for the irrigation of common area landscaping and public parks.
15. Petrochemical fertilizers or pesticides are discouraged in the routine operation and maintenance of plan area landscaping.

C. Landscaping

Refer to Section 3.5.3.F, Driveways, Parking and Services Aisles for parking lot landscape/shade requirements. Private and public landscaping shall be consistent with the master landscape palette for the Research and Technology Park District.



Native Landscaping

1. A variety of shrubs and groundcover species should be used along the base of buildings to soften and anchor the foundation and along pathways to define edges and transitions.
2. Native plant species should be used whenever possible, including native grasses and shrubs.
3. Pollinator friendly planting and maintenance practices shall be integrated into each site's landscaping.
4. Lawns and highly ornamental landscaping that require frequent pruning and maintenance should be used sparingly to accentuate important public nodes, plazas, gathering areas and where desirable for recreation.



Landscaping shall be provided in setback areas adjacent to the public right of way to soften the transition between the sidewalk and built environment.

Yard Setbacks

5. Setbacks areas, including easements for utilities where feasible, shall incorporate landscaping except where screened from the public right-of-way and public or private open space.
6. Vegetative matter shall be designed to cover 75 percent of required landscaped areas. Exceptions may be granted on a case by case basis.

Trees

7. Tree planter dimensions shall be no less than 6' x 6', and should correlate with the ultimate size of the tree species.
8. Soil analysis shall be performed after site grading and paving is complete and any amendments incorporated per recommendation of a licensed landscape architect.
9. Where appropriate and feasible, Valley Oak trees (*Quercus lobata*) shall be incorporated into open space areas, reflective of the City's historic natural landscape.



Where appropriate landscape design should incorporate Valley Oak trees and/or other native trees reflective of Woodland's historic and natural landscape.

Container Size

10. Trees shall be a minimum of 15-gallon size and shrubs a minimum of 5-gallon size at the time of planting.

D. Screening, Walls and Fencing

Outdoor Storage

Outdoor storage of materials shall be attractively screened from public view.



1. Outdoor storage of goods and materials shall generally be screened from public view by way of landscaping, berms, decorative fencing and/or masonry walls, which are architecturally compatible and complementary to the overall project.
2. Where applicable, screen planting shall be designed to achieve a 75 percent summer opacity and 60 percent winter opacity within 3 years of planting.

Mechanical Equipment

3. All utility and mechanical equipment and service areas shall be located generally away from or adequately screened from public view.
4. Backflow prevention devices shall not be located within the public right of way or along primary pedestrian streets. All domestic, fire, and landscape backflow prevention devices shall be screened with landscaping or low fences/walls.

Limited Use of Walls and Fencing

5. Walls and fencing should be limited to specific screening and/or security purposes and should generally not be used for separation of uses/parcels within the North and South Campus.
6. Low decorative walls or fencing may be used in limited capacity to delineate shared or private open space and outdoor dining/patio areas; but should not exceed 36 inches in height within the front setback area or adjacent to public right-of-way.
7. Fencing located outside of the front setback for screening and/or security purposes shall not exceed eight feet in height. All fences over six feet in height require a building permit.

Design

8. Long expanses of fencing or walls visible to the public shall incorporate enhanced detailing, decorative pilasters and/or the height and wall plane shall be varied to minimize a long and monotonous appearance.
9. Where feasible, fencing and walls should be combined with landscaping to soften the overall appearance and discourage graffiti.
10. All wood fences shall be treated with a semi-transparent, pigmented stain and sealant.
11. Standard chain link fencing as well as barbed/concertina wire or similar boundary security materials are not appropriate.



E. Trash and Recycling

Enclosures

1. Trash and recycling enclosures shall consist of the same or similar design and materials as the overall project and shall adhere to City trash and recycle enclosure specifications.
2. Enclosures shall be located to the side or rear of the building, generally away from public view and separated from adjacent parking stalls by a minimum 3-foot-wide planter.
3. Trellis structures and vines are encouraged as a means of screening the sides and rear of trash enclosures.
4. The area around trash enclosures shall be kept free of trash and debris.

F. Driveways

Driveway Location

1. Driveway placement should avoid disrupting the streetwall and pedestrian environment and shall be located behind or to the side of buildings whenever possible.
2. Lots with frontage on arterial streets, including along CR 25A, shall locate driveway access on internal side streets. If the only frontage is on a major access street, consolidated access at a single driveway should be provided, if feasible.

Driveway Spacing

3. The minimum spacing or separation requirements for driveways on local or collector streets shall be as follows, unless otherwise approved by the City's Traffic Engineer:
Local Streets: 100 feet between driveways; 150 feet from an intersection.
Collector Streets: 150 feet between driveways; 200 feet from an intersection.
4. Where driveway location standards can't be met for a parcel, the City may require access to that parcel achieved via cross-access over an adjacent parcel.

Driveway Consolidation

5. The consolidation of driveways within adjacent parcels shall occur whenever possible.

Driveway Design

6. Driveways shall be a minimum width of 25 feet and a maximum width of 40 feet at the back of the sidewalk.
7. Driveway width and placement shall accommodate truck turning movement and clearing without blocking roadways.
8. Access driveways shall provide adequate length to accommodate off-street vehicle stacking needs during times of peak use.
9. Driveways should incorporate material changes to alert pedestrians of potential conflict.

G. Vehicle and Bicycle Parking

Off-street Parking

1. Off-street parking shall be determined by the business owner/applicant based on information provided by owner/applicant in a parking use and needs statement, subject to approval by the Community Development Department.



Parking lots shall consist of landscaping, tree canopy and clearly defined pedestrian walkways to adjoining buildings.

2. The maximum number of off-street parking spaces shall not exceed the following amounts, unless otherwise approved by the Community Development Director:
Hotel: 1 spaces per room, plus 1 space for the manager
Commercial, Business Support Retail: 1 space per 300 gross square feet
Office, Flex, Research and Development: 1 space per 300 gross square feet
Light Industrial: 1 space per 500 gross square feet
Manufacturing, Storage, Warehouse: 1 space per 2,000 gross square feet
Restaurants, cafes, micro-brewery: 1 space per 4 seats

Electric Vehicle Charging

3. All commercial parking lots and/or structures shall provide electric vehicle charging pursuant to the California Green Building Standards Code, Tier 1 requirements.

Parking Lot Landscaping and Shading

4. Proper tree variety and placement shall be designed to result in 50 percent parking lot shading within 15 years of installation. Primary entrance roads and driveways not used as back-up areas are exempt from this requirement.
5. Solar and shade covers are permissible, but should not significantly reduce the number of trees otherwise required to provide 50 percent lot shading.
6. Surface parking lots visible from the public right-of-way shall incorporate landscape screening, trellising, low walls/fences at least 36 inches in height to continue a "green wall" and minimize interruption to the streetwall.
7. At least 6 feet of landscaping should separate surface parking from any adjacent sidewalk or public right-of-way.
8. Shaded sidewalks or other pedestrian walkways shall be clearly delineated by way of landscaping and enhanced paving or design and should connect parking areas directly to building entries.

On Street Parking

9. On-street parking spaces adjacent to the property street frontage may be considered and counted when determining parking needs.



Shared or District Parking Facilities

10. Multiple buildings within the same cluster or block should be designed to share parking and off-street loading facilities and service areas, to the extent possible.
11. Future parking needs may be phased by allocating space for such needs, but allow for other uses until the parking use is required. Landscaped areas that can be converted to expanded parking or building coverage are preferred in lieu of constructing oversized parking lots.

Reduced Parking Demand

12. Projects shall demonstrate that parking reduction strategies have been incorporated to reduce on-site parking demand through Transportation Demand Management strategies such as but not limited to the following:
 Parking cash-out for employees; Subsidized transit passes or car sharing programs; dedicated parking spaces near building entrances for rideshare and carpools; guaranteed ride home program; alternative work week and flex-time schedules; telecommuting or work-at-home programs; dedicated employee housing; compliance with City VMT/TDM ordinance, as available; participation in City VMT fee program, as available.

Bike Parking Facilities

13. Bicycle racks shall be placed near building entrances, in safe, well-lit locations, and permanently anchored to a hard, dust free surface, preferably asphalt or concrete slab, and installed in a manner which allows adequate space for access to the bicycle and locking device.
14. Covered and secured bike parking shall be incorporated into each project for use by employees and visitors. The use of bike lockers is encouraged where feasible.
15. Facilities such as locker rooms, showers (2 minimum) or similar facilities are required in all general industrial, office and research technology buildings over 50,000 s.f. and encouraged in all other commercial developments.
16. The number of bike parking spaces provided shall be a minimum of 10 percent of the total number of vehicular spaces provided or credited to the project (e.g. 200 vehicle parking spaces provided multiplied by 10 percent = 20 bike parking spaces required).



Bike lockers are encouraged throughout the North and South Campus for use by employees and visitors. Lockers should be located in lighted and highly visible locations.

H. Loading

Service Aisles and Loading Space

1. Loading and service aisles shall be constructed of durable materials and designed to support safe and efficient access for delivery or service trucks.

2. Where loading and service aisles may function as secondary or informal access ways, consideration should be given to design as well as function.
3. Where necessary, screening may be required to reduce noise and visual impacts associated with service aisles.
4. Uses with operations requiring a heavy volume of truck traffic shall have separate truck parking areas that are clearly delineated.
5. Designated pull-out or drop-off locations should be provided such that driveway or access to parking is not impeded.

Loading Area Design Standards

6. Loading bays shall be adequately sized and spaced to provide adequate space for vehicles, whether rear loading or side loading at any loading dock, door, or area, without encroaching into required parking spaces or driveways.
7. Loading bays shall not be less than 10 feet in width, 25 feet in length, and 14 feet in height. For semi-trucks, there shall be at least 85 feet from the edge of the loading dock to the far edge of the maneuvering area. Extended trailers and larger docks may require up to 130 feet.
8. One truck loading and unloading space for buildings with a gross floor area of 7,500 square feet or greater should be provided, plus one additional space for each additional 20,000 square feet or fraction thereof unless otherwise approved by the Community Development Department.
9. Flex office and office condominiums having need for only occasional shipping or receiving of goods may use a portion of the driveway or parking area for temporary loading and unloading.

I. Signage and Wayfinding

Setback, Height and Area Requirements

1. Design Review approval is required for all new signage. Sign setback, height and area requirements are subject to Table 3.4.

Zone	Max Sign Height		Min Setback from Property Line	Total Sign Area Per Business	Max Total Sign Area Per Business
	Wall Mounted	Ground Mounted	Ground Mounted	All Permanent Signs	All Permanent Signs
RTP, RTP/RFO	40ft	12ft	10ft	The length of building frontage x 2	300 s.f.
RTP/TO, VCMU	30ft	8ft	3ft	The length of building frontage x 2	175 s.f.



RTP/CCO, HDR/CCO	20ft	12ft	5ft	The length of building frontage x1.5	175 s.f.
HC	30ft, shall not exceed height of hotel	12ft	10ft	The length of building frontage multiplied by 2	200 s.f.
VCOS, OS/GB	Commercial uses permitted with the VCOS (The Yard) shall be subject to the standards and guidelines applicable to the VCMU zone; otherwise signage within the VCOS, OS and GB zones shall be consistent with Park and Greenbelt Master Plan.				



Pedestrian oriented blade signs are encouraged in the Community Commercial Overlay Zones.

Location

2. Signs should be considered during building design and properly scaled and located so as not to dominate or interfere with architectural detailing and overall building façade.
3. In general, signage should not be placed more than 20 feet above the sidewalk or be located above the building streetwall height if upper stories are stepped back.
4. Multi-tenant signage should be coordinated and cohesive in appearance including consistent sizing and lighting requirements.
5. Customized projecting, blade signs or under canopy signs are encouraged within the Community Commercial Zone or where commercial uses are incorporated within mixed use projects. Blades signs must provide at least 8 feet of vertical clearance as measured from the sidewalk to the base of the sign.
6. Signs and structural features shall be subject to cross visibility requirements as outlined in the Woodland Municipal Code and subject to review and approval by the City Traffic Engineer.

Text

5. Signs may only contain the name and logo or symbol of the business and its street address. Trade slogans are not permitted.
6. Logos shall not exceed 4 square feet in size except if it is determined by City Planning staff that building scale or other factors, such as enhanced design and quality, warrant increased logo square footage.

Materials and Design

7. Signs shall be constructed using durable, fade and weather resistant materials such as stone, tile, cast concrete, or other similarly durable masonry, metal or wood materials that complement the architectural design and style of the building.
8. Unique and creative sign design is encouraged to enhance the unique identity of the Plan area.
9. Faded or damaged signs shall be replaced or repaired in a timely manner.
10. Cabinet or box signs, roof mounted signs, window signs above the second floor and pole mounted signs are not permitted.
11. Feather signs, flags, banners and other attention getting signs or devices are prohibited except during grand openings, in which case they must be removed within 30 days.



Unique and creative exterior signage is encouraged within the North and South Campus.

Illumination

12. Signs shall be spot illuminated from the front or consist of letters, numbers, or graphics that are halo backlit. Internally illuminated pan channels letters are not appropriate except within the Community Commercial and Highway Commercial zones. Neon is permitted when integrated into custom designs.
13. Flashing, traveling, animated, or intermittent illumination is prohibited.
14. Exposed conduit, tubing and raceways are prohibited. Transformers and other equipment shall be fully screened/concealed.

Monument Signs

15. Where not governed by a comprehensive sign plan, one monument sign shall be allowed for each street frontage of a lot.
16. Monument signs shall include a decorative or enhanced base and shall be up-lit except for cut-out letters or numbers which may be internally illuminated.

Comprehensive Sign Plan

17. A Comprehensive Sign Plan is encouraged for multi-tenant buildings and for North and South Campus blocks where multiple buildings create a campus like setting.
18. A Comprehensive Sign Plan should consider the need for wayfinding where multiple buildings surround a central common area or where uses may result in multi-location trips for Technology Park visitors.
19. A Comprehensive Sign Plan may be approved by the Community Development Department and should include guidelines pertaining to overall size and square footage



of individual tenant and building signs, lettering heights, materials, illumination, monument placement, size and design, and ancillary pedestrian directional or informational signage such as building entrance directories and other wayfinding methods.

Highway 113 Oriented Signage

20. Businesses with building frontage along HWY 113 shall be permitted additional sign area (up to 1.5 times the building's linear freeway frontage) for on-building signage that faces the freeway.
21. Standalone/freestanding monument signs are generally not permitted along SR 113 unless part of a Comprehensive Sign Plan where multiple businesses are represented and where the monument sign(s) are of exceptional design and quality, with material finishes that represent the unique character of the Plan Area. Additional height up to 50 feet may be permitted through Comprehensive Sign Plan review and approval.

J. Public Art

Obligation

1. Public art shall be incorporated within new development projects, including private projects, consistent with the Woodland Municipal Code (WMC), Public Art Ordinance (WMC Section 17.104.200).

Installation

2. The placement and installation of Public Art is encouraged within shared open space areas such as outdoor patios, plazas and courtyards and within view of a large number of people such as along sidewalks, at intersections, within roundabouts, medians or The Yard, and at primary Plan area gateway locations along CR 25A and Road B.

Artwork

3. Public Art should be unique and of high quality design and execution; however, it is encouraged that artwork located within the public realm be of a unified theme reflective of the Research and Technology Park's focus on agricultural and technological innovation.

3.5.4 Special Character Guidelines- Research Technology Park (RTP) Zone

A unique focus of the RTP Zone is the campus-like setting; a place of collaboration and idea exchange within an inspired and forward thinking environment. The following Special Character Guidelines are intended to assist in achieving that vision.

A. Site Design



Courtyards and outdoor seating areas shall be incorporated as a component of site design to help create a campus-like setting in the RTP Zone.

Block Pattern

1. Block lengths will depend on parcelization, however overall site planning shall consider broader connectivity that promotes pedestrian/bike activity by way of paseos, paths and connecting walkways.

Campus-Like Setting

2. Campus building placement/configuration should be promoted whenever possible, creating opportunities for interior courtyards, plazas, seating areas, and other public or semi-public open spaces.

3. Highly visible and well designed secondary building entrances located internally to the site should face onto common open space areas.
4. Paseos and other internal and external walkways shall connect employees and visitors to key pedestrian pathways by and between buildings, through to public right-of-ways, greenbelts and/or the broader bike/pedestrian trail network.

Pedestrian Corridors and Connection

5. Internal pedestrian walkways or multi-use paths shall be incorporated within the North Campus where appropriate. Landscaping shall be provided along a majority of the paths on one or both sides of the walkway. Shaded resting/seating areas shall be appropriately located in visible and lighted locations.
6. Designated pedestrian paseos should be identified to link the North Campus areas from north to south and to Road B, allowing employees and visitors the ability to connect with various businesses/buildings.
7. Pedestrian scale light poles of not more than 16 feet in height or lower level bollard style lighting, or a combination of both, shall be provided at all outdoor walkways, parking, loading and service areas, ramps/stairs, and building entrances.
8. A minimum of 1-foot candle of lighting shall be provided; "hot spots" of 10 foot candles or greater shall not be permitted.



Walkways shall be illuminated with pedestrian scale lighting such as low level bollard lighting.

Common Open Space

9. New development shall provide common, usable open space which may include a combination of shared paseos/pathways as well as courtyards/patios or private greenspace. Amount of common and usable open spaces shall be equivalent to at least



5% of the gross floor area of the building.

10. Common open space should be designed as a component of new development in a manner that can be shared among multiple tenants or employees.
11. Private open space, designed for the exclusive use of building occupants and employees should also be considered and may be separated from publicly accessible spaces as balconies, terraces, or patios by elevation, landscaping, or other means.
12. Shared open space, framed by active building facades, should generally orient toward the public right-of-way or internal pedestrian paseos/pathways. Building windows, balconies or entrances should face the open space for added security and visibility.
13. The use of textured or enhanced paving, incorporation of decorative landscaping and shade elements, and movable tables and chairs that provide an adaptable, attractive and inviting environment are encouraged within shared courtyards, patios and private greenspace.

Adjoining Parcels

14. Where possible, setbacks between adjoining parcels should be treated as a unified public area, designed and landscaped to contribute to the broader open space and pedestrian circulation framework.

B. Architecture and Massing

Ground Floor Articulation and Activity

1. The ground floor of all buildings, particularly those façades facing the public right-of-way, should support activity on the street and bring the life of the building into the public realm through the use of frequent and well defined building entryways, façade transparency at the street level, and through architectural detailing such as awnings, canopies, recesses, and enhanced materials and surface changes that provide an interesting and pleasant employee/public experience.
2. Active uses should be located on the ground floor along the building frontage, visible to the public realm/public right-of-way and may include, but are not limited to community rooms and kitchens, employee support areas, recreational facilities, exhibition space, and lobbies.
3. Articulated wall planes at the ground floor along the public right-of-way or where substantially visible to the public should be offset by at least 2 to 4 feet and may be combined with color or material changes to provide substantial shadow lines and visual interest and allow for enhanced landscaping along the base of the building.

Active Corners

4. Buildings located at corners, intersections or roundabouts should reinforce the streetwall through unique architectural design elements or enhanced façade treatment, fenestration, variation in wall plane, tower elements and interesting signage.
5. Corner buildings should be minimally recessed if at all from the property line/public right-of-way except to allow for enhanced landscaping.

3.5.5 Special Character Guidelines- Research Technology Park/Transitional Overlay Zone (RTP/TO)

The following guidelines provide additional recommendations and standards that ensure the RTP/TO zone will provide an appropriate level of intensity transition between the RTP Zone and the Village Center District to the east. Particular emphasis is placed on the interface between the RTP and the public realm along Road B to ensure building types and uses in the RTP/TO actively contribute to a pleasant, walkable, pedestrian environment. Standards and guidelines in this section shall supersede and take precedence over those in Section 3.5.3, General DS/DG.

A. Site Design

Well Defined Streetwall

1. Building placement along Road B should provide clearly defined edges to the public realm and activate the street.
2. For parcels with frontage along Road B within the RTP/TO, seventy percent (70%) of the property shall be developed at the minimum setback line (development may be phased over time). Flexibility to increase the minimum setback may be granted to allow for well-designed outdoor space that includes vertical and horizontal elements and other circumstances where additional building setback is necessary.
3. Buildings over three stories should stepback or provide a varied wall plane on street facing upper floors (above the third floor) to maintain a human scale streetwall.

Green Wall

4. Landscaping, including hardscape elements, decorative planters, and trellis structures with vines, should serve as a continuation and enhancement of the streetwall, contributing to an attractive public realm which extends and complements the architectural style of the built environment.

Pedestrian Experience

5. Primary building entrances should face on to Road B.
6. Façade treatments, when adjacent to Road B and to common open space areas should support a comfortable pedestrian experience by incorporating architectural features that provide shade, such as canopies, awnings, overhangs, projections, as well as visual interest through the use of articulated bays, material / surface change and textures and distinct entryway details.
7. Canopies and awnings should provide a minimum 8-foot clearance above finished grade and may extend over the public right-of-way.

B. Driveways, Parking and Service Aisles

Driveway Location

1. Driveway placement shall avoid disrupting the streetwall and pedestrian environment and shall be located behind or to the side of buildings whenever possible.



2. Not more than one full-access entrance/drive into the RTP/TO zone shall be permitted along Road B between Marston and Road E (see also Circulation Chapter).
3. It is encouraged that the number of limited access driveways (right-in/right-out) along Road B, along the park (The Yard) frontage be limited to avoid disrupting the streetwall and pedestrian environment.
4. Shared access shall be pursued between adjoining parcels/uses.

Parking Lots

5. On-site parking shall be located behind buildings and away from Road B. Not more than 20 percent of a parcel's frontage on Road B may be used for parking, including aisles and backup areas.

3.5.6 Special Character Guidelines-Research Technology Campus/Community Commercial (RTP/CCO) Zone

The following Special Character guidelines address particular aspects of commercial use and building design within the RTP/CCO zone, located at the intersection of CR 25A and Road B, serving as a "gateway" to the Plan Area. The RTP/CCO guidelines have a specific focus on the prominent gateway location and enhancement of the identity of the Plan Area. The RTP/CCO Zone allows both commercial and residential uses within a desirable horizontal or vertical mixed-use development project. Standards and guidelines in this section shall supersede and take precedence over those in Section 3.5.3, General DS/DG.

A. Site Design

Well Defined Streetwall

1. Building placement along Road B should provide clearly defined edges to the public realm and activate the street.
2. For parcels with frontage along Road B within the RTP/CCO Zone, sixty percent (60%) of the property shall be developed at the minimum setback line (development may be phased over time). Flexibility to increase the minimum setback may be granted to allow for well-designed outdoor space that includes vertical and horizontal elements and other circumstances where additional building setback is necessary.
3. "Drive-thru" uses shall not be permitted along Road B or CR 25A. Exceptions may be granted for small scale drive-thru facilities such as a coffee kiosk as an ancillary use within a larger commercial center. Such uses should be located internal to the site, away from Road B.
4. The number of limited access (right-in/right-out) driveways shall be restricted from Road B between CR 25A and Road C to minimize disruption to the pedestrian environment.
5. On-site parking shall not be permitted along the Road B frontage.

B. Architecture and Massing

Gateway (Primary)

1. Buildings located at or the near the corner of CR 25A and Road B shall be designed in a

manner which provides a sense of arrival and should reflect the distinctive character of the Plan area as an agricultural and technological center for innovation.

2. Enhanced landscaping shall be provided along CR 25A between Highway 113 and Road B with an emphasis at the intersection of CR 25A and Road B to accentuate this location as the primary Plan Area entryway.

Road B Frontage

3. Buildings with frontage along Road B shall be designed in a manner so as to not “back-on” to Road B. Windows, entryways, patios and other design features shall be located along the Road B frontage to give the appearance of a primary building frontage and/or shall activate the streetwall.
4. Utility doors and other back-of-house items shall be avoided along Road B and CR 25A.

Ground Floor Articulation and Activity

5. The ground floor of all buildings, particularly those façades facing the public right-of-way, should support activity on the street and bring the life of the building into the public realm through the use of frequent and well defined building entryways, façade transparency at the street level, and through architectural detailing such as awnings, canopies, recesses, and enhanced materials and surface changes that provide an interesting and pleasant public experience.
6. Active uses should be located on the ground floor along the building frontage, visible to the public realm/public right-of-way and may include, but are not limited to retail storefronts, cafes/restaurants, community rooms and kitchens, employee support areas, recreational facilities, exhibition space, and lobbies.
7. Articulated wall planes at the ground floor along the public right-of-way or where substantially visible to the public should be offset by at least 2 to 4 feet and may be combined with color or material changes to provide substantial shadow lines and visual interest and allow for enhanced landscaping along the base of the building.

Active Corners

8. Buildings located at corners, intersections or roundabouts should reinforce the streetwall through unique architectural design elements or enhanced façade treatment, fenestration, variation in wall plane, tower elements, outdoor seating/patios and interesting signage.

C. Driveways, Parking and Service Aisles

Driveway Location

1. Driveway placement shall avoid disrupting the streetwall and pedestrian environment and shall be located behind or to the side of buildings whenever possible.

Parking Lots

2. On-site parking shall be located behind buildings and away from CR 25A and Road B.



3.5.7 Special Character Guidelines-Research Technology Park/Research Flex Overlay (RTP/RFO) Zone

The RTP/RFO zone, located in the South Campus, will serve as an extension of the North Campus, while providing opportunities for research and light manufacturing uses, particularly ag tech related, with immediate and convenient access to prime agricultural land for field research and testing. Development guidelines within this zone provide added flexibility for specialized building types such as greenhouses or other research and testing facilities. Standards and guidelines in this section shall supersede and take precedence over those in Section 3.5.3, General DS/DG.

A. Site Design

Block Pattern

1. Block lengths should be no greater than 600 feet. If longer block lengths are required, mid-block paseos or pedestrian paths, connecting walkways, bicycle facilities, and parks or open space should be provided.

Future Use Expansion

2. Consideration should be given to lot sizes that will allow for future expansion, such as for greenhouse or expanded research facilities.

Campus Setting

3. Where higher employment uses are proposed, a campus style environment should be promoted, creating opportunities for interior courtyards, plazas, seating areas, and other public or semi-public open spaces.
4. Highly visible and well designed, secondary building entrances located internally to the site should face onto common open space areas and parking lots.
5. Paseos and other internal and external walkways shall connect employees and visitors to key pedestrian pathways by and between buildings, through to public right-of-ways, greenbelts and/or the broader bike/pedestrian trail network.
6. Pedestrian scale light poles of not more than 16 feet in height or lower level bollard style lighting, or a combination of both, shall be provided at all outdoor walkways, parking, loading and service areas, ramps/stairs, and building entrances.
7. A minimum of 1-foot candle of lighting shall be provided; "hot spots" of 10 foot candles or greater shall not be permitted.

Common Open Space

8. New development shall provide common, usable open space which may include a combination of shared paseos/pathways as well as courtyards/patios or private greenspace. Amount of common and usable open spaces shall be equivalent to at least 5 percent of the gross floor area of the building.
9. The use of textured or enhanced paving, incorporation of decorative landscaping and shade elements, and movable tables and chairs that provide an adaptable, attractive and inviting environment are encouraged within shared courtyards, patios and private greenspace.



Required open space may be developed as a courtyard or patio and should include landscaping, lighting and outdoor furniture for the enjoyment of employees and visitors.

B. Architecture and Massing

Unique Building Types

1. Unique or nontraditional building types such as greenhouses are permitted within the RTP/RFO Zone but shall be designed with high quality materials and in a manner that does not detract from the overall aesthetic quality of the Plan Area.
2. Where appropriate, landscaping or screening may be required to soften the appearance of a non-traditional building types.

Gateway (Secondary)

3. Buildings located at or the near the corner of CR 25A and Road B shall be designed in a manner which provides a sense of arrival and should reflect the distinctive character of the Plan area as an agricultural and technological center for innovation.
4. Enhanced landscaping shall be provided along CR 25A between Highway 113 and Road B with an emphasis at the intersection of CR 25A and Road B to accentuate this location as a critical gateway to the Plan Area's South Campus.

C. Ag Interface

1. A minimum 150-foot buffer is encouraged, where feasible, along the southern edge of the Plan Area, adjacent to agricultural lands along the urban limit line.
2. Uses within the buffer may include parking, streets, bike / pedestrian multi-use trails, shipping/receiving yards, stormwater management uses/facilities, or uses. Additionally, uses consistent and compatible with agricultural uses, agricultural field research or similar (i.e. greenhouses, field research offices, community gardens or agricultural uses/structures) are permitted within the ag buffer.



3.5.8 Special Character Guidelines-Highway Commercial Zone (HC)

The following special character guidelines are intended to promote an attractive Plan Area gateway that reflects the character of the Research and Technology Campus, while serving both visitors to the Plan Area as well as the traveling public in a convenient and efficient manner. Standards and guidelines in this section shall supersede and take precedence over those in Section 3.5.3, General DS/DG.

A. Site Design

Pedestrian Connection

1. Site design shall consider pedestrian connectivity to adjacent and nearby commercial uses, businesses, bike/pedestrian paths, and mobility hub(s). Pedestrian walkways should be clearly delineated by overhead trellising, shade trees, enhanced paving, landscaped edges or other identifying characteristics. Pedestrian walkways shall be well lit and visible.

B. Architecture and Massing

Architectural Style and Scale

1. Development within the Highway Commercial Zone shall reflect the “ag-tech” character of the Plan Area.
2. Contemporary design styles are encouraged.
3. Hotel(s) shall provide a well-defined, covered entryway for loading and unloading.

Gateway

4. Projects located at the corners of CR 25A and Roads A and B shall include enhanced corner design elements and landscaping to provide a sense of arrival.
5. Buildings with frontage along CR 25A, Road A or Road B shall be designed in a manner so as not to “back-on” to those roadways. Windows, entryways, patios and other design features shall be located along the CR 25A and Road A/B frontages to give the appearance of a primary building frontage.
6. Utility doors and other back-of-house items shall be avoided along CR 25A and Road A/B or appropriately screened from public view.

C. Driveways, Parking and Service Aisles

Driveways

1. Limit driveways to a single right-in right-out on CR 25A.

Drive-Thru Facilities

2. Highway Commercial uses that include drive-thru facilities should locate the drive-thru aisle away from CR 25A and Road A where feasible.
3. Not more than one drive-thru facility may be permitted on a single parcel.
4. Drive-thru aisles shall incorporate covered overhangs and/or trellises and be landscaped on both sides of the aisle. Adjacent planter areas/medians shall be at least 5 feet wide and

shall include trees or other vertical elements such as landscape screens/walls.

5. Pedestrian pathways on parcels with drive-thru facilities shall be clearly delineated by stamped/decorative, color paving.
6. Outdoor/patio dining areas shall be provided with all drive-thru coffee shops / restaurants.

Gas Station

7. A single service/gas station may be permitted within the Highway Commercial Zone.
8. Fuel pump canopy design shall have similar roof form and design to primary building and be of exemplary architectural design.
9. A minimum of 10' wide landscape area shall be provided along all street frontages.
10. A single monument sign with fuel price may be permitted and shall incorporate architectural treatments consistent with the main building.



PART 2: DESIGN STANDARDS AND DESIGN GUIDELINES FOR THE VILLAGE CENTER DISTRICT

The Village Center District includes Village Center Mixed Use (VCMU) Zone and the Village Center Open Space (VCOS) Zone, The Yard, an 11-acre linear park. Residential Uses in the Village Center District are Village Center Medium Density (VCMDR) and Village Center Low Density (VCLDR) Zones. In this District, the DS/DG provide the comprehensive DS/DG for each zone and no General DG/DG's are provided. However, some standards refer to DS/DG in other sections. The VCMDR and VCLDR zones tier off of the General Residential District DS/DG found in Section 3.5.12.

3.5.9 DS/DG for the Village Center Mixed Use (VCMU) Zone

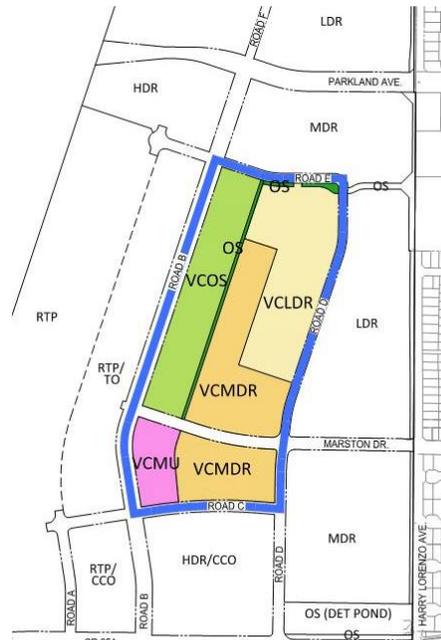
A. Site Design

Well Defined Streetwall

1. Building placement along Road B and Marston Drive should provide clearly defined edges to the public realm and activate the street.
2. For parcels with frontage along Road B and Marston Drive, seventy percent (70%) of the property shall be developed at the minimum setback line (development may be phased over time). Exceptions may be granted to allow for articulated entryways and well-designed outdoor space that includes vertical and horizontal elements that simulate and maintain the appearance of a streetwall.
3. Where portions of a building other than entryways are setback from the sidewalk, the area should be treated as a courtyard or patio.
4. Buildings over three stories should stepback or provide a varied wall plane on street facing upper floors (above the third floor).
5. On-site parking, if provided, shall be located behind the building and away from Road B and Marston Drive.
6. "Tuck under" parking is encouraged, where feasible, as an alternative to surface parking.

Active Corners

7. Buildings located at Road B and Marston Drive should incorporate unique architectural design elements or enhanced façade treatment, fenestration, variation in wall plane, tower elements and interesting signage.



Pedestrian Experience

8. Primary entrances and/or outdoor use areas such as patios shall be located at intervals of no more than 50 feet and should face the public right-of-way or public realm which may include an active alley if that is the primary entrance.
9. Primary business entrances shall not face parking lots.
10. Façade treatments, when adjacent to Road B and Marston Drive should support a comfortable pedestrian experience by incorporating architectural features that provide shade, such as canopies, awnings, overhangs, projections, as well as visual interest through the use of articulated bays, material and surface change and textures, and distinct door and window details.
11. Canopies and awnings should provide a minimum 8-foot clearance above finished grade and may extend over the sidewalk/public right of way.
12. Avoid locating residential uses on the ground floor unless a live-work unit. Lobby entries to upper floor units may be provided along the building frontage but should be carefully placed to avoid disrupting the storefront continuity.
13. Pedestrian access should consider and connect thoughtfully to the mobility hub, The Union.



Roll up doors are encouraged within the Village Center to allow for greater interaction and interest between the built environment and the pedestrian right of way.

Public and Private Outdoor Space

14. Outdoor dining is encouraged and may take place within the public right-of-way subject to an encroachment permit issued by the Community Development Department.
15. Environmental conditions such as sun, shade and prevailing winds should be considered when positioning courtyards and outdoor seating areas.
16. Outdoor patio/dining enclosures (fencing or gates) shall be designed with quality materials and should incorporate landscaping where possible to help soften their appearance.
17. Trees, trellises and other shade structures are encouraged within outdoor courtyards and patios.
18. Outdoor display of merchandise which enhances the appearance and pedestrian experience is permitted, but shall not obstruct the accessible path of travel, any building entrance/exit or accessible ramp. A minimum of 5 feet of clear walk space shall be maintained along all pedestrian paths and public sidewalks.



Outdoor display of merchandise is encouraged within the Village Center. However, a minimum 5-foot clear walk space shall be maintained on all pedestrian paths/sidewalks.



Multi-Use Design

19. Alleys and paseos should be fully designed to allow for shared uses including primary or secondary access to businesses, outdoor dining and attractive pedestrian walkways.
20. The use of pavers or stamped/colored concrete is encouraged within alleys to provide a durable and attractive passageway and help define a shared pedestrian realm.

B. Architecture and Massing



Solid wood, standing metal seam, stone, concrete and masonry, including red brick, are appropriate materials in the Village Center.

Architectural Style and Building Materials

1. Building design should reflect a more traditional urban center with predominantly two and three story buildings.
2. Simple architectural design is preferred with elements of contemporary farm style, warehouse / loft style, and/or ag/industrial rustic.
3. Materials and colors should be well thought out to provide a unified and attractive exterior, but should generally be limited to one base material and not more than two accent materials to avoid a busy appearance.
4. High quality and durable materials such as solid wood, standing metal seam, quarry stone, concrete and masonry, shall be used on all buildings, with particular attention paid to finish types and surface textures that enhance visual interest and variety particularly at the pedestrian level. Innovative "green" materials and/or "green walls" are encouraged.
5. Naturally finished metal, and wood are appropriate as accent materials. The use of stucco siding is permitted but should be limited and applied as smooth texture.
6. Matte finishes are preferred; highly reflective surfaces are generally not appropriate and should be used sparingly as accents only.
7. A rich contemporary color scheme is encouraged.
8. Building lighting, including illumination and fixture style, as well as landscape pallet and location should be considered early in the design process. Lighting and landscaping should be well integrated into the overall project site plan and complementary to the building design and proposed architectural style.
9. Design elements such as roll up doors, string lights, upper level decks, balconies and finished roof tops that can support outdoor gathering/dining opportunities are encouraged to promote a lively urban environment.

Ground Level Façade

10. Facades at the ground level, particularly those facing the public right-of-way, should include frequent entrances and display windows, and should be visually distinguishable from upper levels through the use of architectural detail, awnings, canopies, or by recessing upper floors above the second floor.

11. A majority of ground floor storefronts should be transparent and provide opportunity for storefront display area and/or patron seating/dining. Avoid blank walls greater than 12 feet in length.
12. Buildings located at prominent street corners shall include enhanced architectural features at the corner such as towers or variations in building height as well as design primary covered, entrances near or facing the corner.
13. Building windows should be located on both sides of the corner, and architectural features shall be wrapped around all sides of the building where visible to the public.



Enhanced architectural treatment shall be provided at prominent street corners, particularly at the Road B and Marston intersection.

Vertical Mixed Use

14. Upper level residential uses should be compatible with ground floor retail space and may include studio or smaller one- or two-bedroom units. Large three- and four-bedroom apartment units are generally not appropriate.

Windows

Proper window glazing should allow for visibility and connection between indoor uses and the pedestrian right of way.



15. Windows should be inset or recessed from the wall plane for visual interest and shadow line.
16. Proper glazing should enhance visibility and connection between indoor and outdoor spaces, particularly at the pedestrian/ground level.
17. Tinted glazing is not permitted.

Rooflines and Cornices

18. Rooflines should be considered in the overall building design and should provide an attractive termination of the building through the use of a decorative cornice or parapet, continuous banding or trim, decorative gables or other architectural features that help clearly define the building edge.
19. A simple rectangular roof line is preferred. Shed or sloping roof lines may be used as accents. A simple gable roof may be considered with appropriate pitch and use of materials that reflect a contemporary or farm style architecture.
20. If used, decorative parapets should be extended around the side of a building and contain necessary depth to appear as an integral part of the building design and form.



Lighting

21. Building and on-site lighting shall reinforce the architectural style of the development and enhance the overall ambience.
22. Where appropriate and complementary, up-lighting or rooftop lighting is encouraged to accentuate major architectural details, building form and/or landscape elements at night, however, excessive light and glare shall be avoided.
23. Minimum required illumination shall meet Uniform Security Code requirements as set forth in Title 15 of the Woodland Municipal Code.



Lighting shall reinforce the architectural style of the building.

C. Sustainability

Projects shall comply with the Sustainability Guidelines outlined in Section 3.5.12.B.

D. Screening, Walls and Fencing

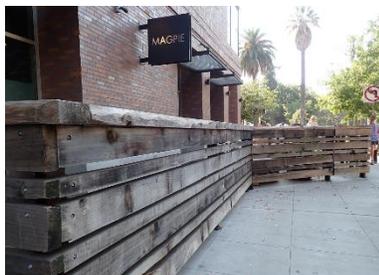
Outdoor Storage

1. Outdoor storage of goods and materials shall be located away from Road B and Marston and shall be screened from public view by way of decorative fencing/trellising, landscaping and/or masonry walls with vines. All requiring screening shall be architecturally compatible and complementary to the overall project.

Mechanical Equipment

2. All utility and mechanical equipment and service areas shall be screened and located away from Road B and Marston Street. Where feasible, mechanical equipment shall be located on top of the building and screened from view with parapet walls.
3. Backflow prevention devices shall not be located within the public right of way or along primary pedestrian streets and be screened with landscaping or low fencing/walls.

Limited Use of Walls and Fencing



Low decorative fencing and walls may be used in limited capacity to delineate shared open spaces and outdoor dining areas.

4. Low decorative walls or fencing may be used in limited capacity to delineate shared or private open space and outdoor dining/patio areas; but should not exceed 36 inches in height within the front setback area or adjacent to public right-of-way.

5. Fencing located outside of the front setback for screening and/or security purposes shall not exceed eight feet in height. All fences over six feet in height require a building permit.)

6. Standard chain link fencing as well as barbed/concertina wire or similar boundary security materials are prohibited within the Village Center District.

E. Trash and Recycling

Enclosures

1. Trash and recycling enclosures shall consist of the same or similar design and materials as the overall project and shall adhere to City trash and recycle enclosure specifications.
2. Enclosures shall be located to the side or rear of the building, away from public view and should be separated from adjacent parking stalls by a minimum 3-foot-wide landscaped planter where applicable.
3. Trellis structures and vines are encouraged as a means of screening the sides and rear of trash enclosures.
4. The area around trash enclosures shall be kept free of trash and debris.

F. Commercial and Residential Compatibility

Commercial / Residential Interface

1. Where commercial and residential uses are located on the same block or combined as part of a mixed-use project, efforts should be taken to minimize potential conflicts between the two uses including locating commercial uses that may generate loud noise, fumes, light, etc., along the street frontage, and residential uses on upper floors away from more intense commercial activity. Rooftops should be considered for commercial exhaust vents and generators.
2. A buffer of lower intensity commercial uses such as office or storage space located between residential units and more intense commercial uses can help minimize potential conflicts between the two.

G. Driveways, Parking and Service Aisles

Driveway Location

1. Driveway placement shall avoid disrupting the streetwall and pedestrian environment and shall be located behind or to the side of buildings. On-site parking, when necessary, shall be accessed from the rear of the property.
2. Minimize the number of driveways to avoid loss of on-street parking spaces.

Driveway Design

3. Driveways should incorporate material changes to alert pedestrians of potential conflict and shall provide good visibility of pedestrians from adjacent roadways and exit points.

Required Parking

4. On-site parking lots/areas shall be located behind buildings, away from Road B and Marston Drive.
5. A common shared parking area is encouraged for employee and customer use for businesses within the Village Center. Undeveloped lots within or adjacent to the Village Center may be used as temporary parking areas in the early phases of the project build



out prior to development and service to the Mobility Hub.

6. The maximum number of off-street parking spaces shall not exceed the following amounts, unless otherwise approved by the Community Development Director:
Commercial, Retail, Service: 1 space per 500 gross square feet
Office, Flex, Research and Development: 1 space per 500 gross square feet
Daycare: 1 space per 1,000 gross square feet
Restaurants, cafes, micro-brewery: 1 space per 4 seats
Residential: 1 space per studio/1 bedroom unit. 2 spaces per 2 bedroom units or greater.
7. On-site parking, if provided, shall be located behind the building. "Tuck under" parking is encouraged as an alternative to surface parking.

Electric Vehicle Charging

8. All commercial parking lots and/or structures shall provide electric vehicle charging pursuant to the California Green Building Standards Code, Tier 1 requirements.

Parking Lot Landscaping and Shading

9. Proper tree variety and placement shall be designed to result in 50 percent parking lot shading within 15 years of installation. Primary entrance roads and driveways not used as back-up areas are exempt from this requirement.
10. Solar and shade covers are permissible, but should not significantly reduce the number of trees otherwise required to provide 50 percent lot shading.
11. Surface parking lots visible from the public right-of-way shall incorporate landscape screening, trellising, low walls/fences at least 36 inches in height to continue a "green wall" and minimize interruption to the streetwall.
12. At least 6 feet of landscaping should separate surface parking from any adjacent sidewalk or public right-of-way.
13. Shaded sidewalks or other pedestrian walkways shall be clearly delineated by way of landscaping and enhanced paving or design and should connect parking areas directly to building entries.

Bike Parking Facilities

14. Bicycle racks shall be placed near building entrances, in safe, well-lit locations, and permanently anchored to a hard, dust free surface, preferably asphalt or concrete slab, and installed in a manner which allows adequate space for access to the bicycle and locking device, and avoid pedestrian conflict.
15. Covered and secured bike parking shall be incorporated into each project for use by residents and employees. The use of bike lockers is encouraged where feasible.



Innovative or artistic bike racks are encouraged within the Village Center provided they remain functional and secure for the user.

Lighting Standards

16. Minimum required illumination shall meet Uniform Security Code requirements as set forth in Title 15 of the Woodland Municipal Code.

H. Signage

Projects within the Village Center District shall comply with the Signage and Wayfinding Guidelines outlined in Section 3.5.3.G (General DS/DG for Research and Technology Park Zones), Additional guidance for the VCMU zone is provided below.



A-Frame Signs

1. In addition to the Signage and Wayfinding guidelines provided for in Section 3.5.3.G, high quality and attractive A-frame signs (4 feet tall by 2 feet wide max) may be located in the public right-of-way in front of small retail, restaurant and café uses when properly weighted or secured to prevent tipping or moving.
2. A-frame signs and similar pedestrian oriented temporary signs may not inhibit accessible travel within the right-of-way and must be brought inside after business hours.

Prohibited Signs

3. Traditional, internally illuminated pan-channel letters and cabinet signs are prohibited within the Village Center. Signage within the Village Center shall be of high quality and custom design and shall contribute to the urban, energized environment envisioned for the District.



I. Public Art

Obligation

1. Public art shall be incorporated within new development projects, including private projects, consistent with the Woodland Municipal Code (WMC), Public Art Ordinance (WMC Section 17.104.200).

Installation

2. The placement and installation of Public Art consistent with the Woodland Public Art Ordinance is encouraged within shared open space areas such as outdoor patios, plazas and courtyards and within view of a large number of people such as along sidewalks, at intersections, within roundabouts and medians, within the VCOS zone (The Yard) and at primary Plan Area gateway locations along CR 25A and Road B.

Artwork

3. Public Art should be unique and of high quality design; however, it is encouraged that artwork located within the public realm be of a unified theme reflective of the Research and Technology Park's focus on agricultural and technological innovation.

3.5.10 DS/DG for Village Center Open Space (VCOS) Zone

A. Site Design

The Yard will serve as a central gathering place for Plan Area employees, visitors and residents. Formal and informal spaces within the park will allow for a variety of uses and activities.



1. The Yard is intended to serve as a both passive and active recreational linear park designed with formal and informal spaces that provide the flexibility to accommodate various activities and user groups.

2. Public plazas within the park shall be designed to accommodate social and employee gatherings, special events, farmers markets, food trucks, mobile retail vendors and other pop-up uses, should consider design features such as an elevated stage for musical/theatrical performances.

Park frontage along Road B shall consider efficient means to accommodate food trucks and other appropriate pop-up uses so as not to

obstruct traffic or pedestrian walkways along the western edge of the park.

3. Certain areas of the park should remain open and unobstructed by trees and/or berms to allow for informal pick-up games of soccer, volleyball, disk golf or similar uses. Shade trees should be provided at the periphery of open space areas.

B. Commercial/Retail Use

1. One and a half (1-1/2) acres of commercial/retail development is permitted within The Yard consistent with a master park design plan.
2. Architectural design and site development guidelines/standards outlined in Section 3.5.9 A and B shall generally be applied to commercial/retail buildings or structures within the park, as applicable. Appropriate uses within the park generally include coffee shops/kiosks, farmers market, container style micro-brewery/beer garden, restaurants with patio dining, artist studio, fitness studios, small retail shops (e.g. small nursery, florist, or clothing boutique) willing to showcase goods outdoors.
3. Both hard and soft-scape courtyards, plazas and patios are encouraged as connections to and transitional space between commercial/retail uses and recreational open space in the park.
4. A centrally located, covered plaza is encouraged within close proximity to the Village Center commercial area, which may accommodate bike parking as well as open-air kiosks or seasonal market stands.



A covered pavilion within The Yard will allow for year round open air activities including farmer's markets and craft fairs, as well as covered bike parking.

C. Amenities



Covered seating areas including fixed and movable furniture is encouraged within The Yard.

1. The Yard should include furniture, both fixed and movable, for picnics, rest areas and social gatherings. Both built and natural elements should provide sufficient shade during summer months.
2. Pedestrian and bike pathways shall be provided throughout the park, including soft surfaces such as decomposed granite for jogging, and shall connect to off-site bike and pedestrian networks.
3. Public restroom shall be provided.
4. Bike and scooter parking shall be provided throughout the park in safe and highly visible locations, near activity nodes for the use and convenience of cyclists.



D. Lighting

1. Park lighting shall be provided to ensure safety and security and to allow for continued park use and activity after dark. All pedestrian/bike pathways shall be illuminated by way of pedestrian scale overhead lighting or lower level bollard style lighting.
2. Minimum required illumination shall meet Uniform Security Code requirements as set forth in Title 15 of the Woodland Municipal Code.

E. Landscape

3. Landscaping within The Yard shall feature native ground covers and trees, including Valley Oaks, and pollinator friendly plant species.

3.5.11 DS/DG for Village Center Low and Medium Density Residential Zones (VCLDR, VCMDR)

Projects within the VCLDR and VCMDR zones shall comply with the General Residential District Guidelines outlined in Section 3.5.12, found in Part 3 of this document. The guidelines in this section supplement the general residential guidelines provided in Section 3.5.12 in order to enhance the more urban-like character of the Village Center District, while providing a seamless transition to the adjacent residential neighborhoods in the Villages District and to ensure appropriate and positive transitions.

A. Neighborhood Design

Building Orientation, Setbacks and Park Interface

1. Primary entrances to those residential units located along the eastern boundary of and immediately adjacent to The Yard shall be designed to front or appear to front on the park.
2. Primary doorways, stoops, patios/porches and similar design features shall be included to emphasize a strong relationship to the park. Units may include double frontages or provide secondary access from a rear alley.
3. Residential units fronting the park shall be a minimum of two stories in height and shall be setback a minimum of 10 feet, but not more than 20 feet from the Promenade on eastern boundary/right-of-way. Setback areas may include a low fence (36" if solid or 42" if open) or a decorative low wall to delineate private residential property.
4. Public access to The Yard from the VCLDR and VCMDR neighborhoods shall be provided by mid-block paseos, a minimum of 20 feet wide with a minimum 8-foot-wide accessible sidewalk or pathway, every approximately 300 feet between residential units fronting the park.

B. Architecture and Massing

Building Form and Massing

1. Residential units fronting The Yard are encouraged to incorporate design elements such as balconies and roof-top patios/gardens to further activate the park, to allow units to take advantage of park views and to enhance visibility and "eyes on the park" for safety.

2. VCMDR multi-family units fronting The Yard shall convey, through modulation or offset planes, individual residential units. Urban-style row- or townhouses with walk up front stoops that help elevate the units above the ground level are encouraged.
3. Vertical articulation at the front elevation is encouraged to minimize dominant unbroken exterior wall planes.
4. Small lot and higher density residential units shall provide distinguishable and articulated entryways including recessed doorways, overhead projection/awnings, elevated stoops and/or unique paving to differentiate private space from the public sidewalk/pathway.
5. Where live-work or mixed-use retail/residential projects are located adjacent to the park, the ground floor retail/commercial use shall be visually separated and identifiable from upper residential units by way of differentiated façade treatment, articulated wall plane, awnings or other design elements.



Articulated and/or elevated entryways provide a clear and distinct differentiation of public and private space.

Medium- to Low-Density Residential Transition



Proper scale and massing will allow for a seamless transition between residential zones of varying densities.

6. The design and massing of medium- and low-density residential development within the Village Center District should contribute to a unique and unified urban district.
7. Transitional areas should be seamless between the two residential zones through appropriate transition of scale and massing. The height differential between any two adjacent parcels shall be no more than two stories or 25 feet, whichever is less, within 25 feet of the parcel line.

8. Innovative “outdoor rooms” which foster social and neighborhood interaction and include a mix of hard and soft-scape for outdoor seating, fire rings, and patios rather than traditional front yards are encouraged within the district.



Design

- 9. Medium- and low-density residential units within the Village Center District shall incorporate contemporary and/or modern design elements such as flat parapets, metal awnings and clean lines. Traditional suburban architecture and building forms should be avoided.
- 10. Appropriate building materials include, but are not limited to, wood, glass, brick, metal, cement board siding, stucco or tile. Finishes shall be smooth or near smooth.



Modern design elements are encouraged within the Village Center Residential Zones including flat parapets, smooth finish stucco and metal awnings.

Garages and Driveways

- 11. Garages and driveways are not permitted along The Yard frontage. Garages, driveways and parking for those residential units with park frontage shall be located behind the unit



Alley-load design is encouraged within the Village Center Residential Zones to minimize vehicle prominence and enhance the pedestrian realm. Alley design should include landscaping and decorative fencing, taking into account waste bins and other elements that may require screening or storage.

away from the park.

- 12. Alley-load and non-traditional subdivision layouts that minimize automobile prominence and create a more urban and active environment are encouraged within the Village Center Low- and Medium Density residential zones.

- 13. Deviations from setback requirements will be considered on a case-by-case basis to allow for innovative subdivision layout that meet these objectives based on a finding that adequate unit separation for light, air and ventilation and private open space is still maintained as determined by the Community Development Director. Where projects are located adjacent to existing residential development, setback standards shall be maintained except as provided for in Chapter 7, Section 7.4.1.2, Minor/Administrative Modifications.

PART 3: DESIGN STANDARDS AND DESIGN GUIDELINES FOR THE VILLAGES DISTRICT

The following guidelines provide site planning and design direction for all land use zones within The Villages District. These zones include LDR, MDR, HDR and HCR/CCO. Section 3.5.11 provides standards and guidelines that apply broadly to all residential zones. In addition, 3 sections of Special Character Guidelines are provided for zones of HDR, HDR/CCO, for MDR and LDR sites located in East Villages and for LDR sites located in the North Village. The “Special Character” guidelines are intended to enhance or in some cases supersede the broader Residential District guidelines. If conflicts arise between the General Guidelines and the Special Character Guidelines, the Special Character Guidelines shall take precedence.



3.5.12 General DS/DG for all Residential District Zones (LDR, MDR, HDR, HDR/CCO)

A. Neighborhood/Site Design Building Orientation and Setbacks



Subdivisions designed around and fronting shared open space are encouraged within the Village District.

1. Homes, including multifamily projects, shall be oriented to the street, open space, or common areas. All street-facing dwelling units must have the primary entrance facing or within 30 degrees of a public street or internal walkway.
2. Within Medium and High Density Residential zones, buildings should be located at or near the required minimum setback to enhance the streetwall and activate the public realm.
3. Front yard setbacks along a street may vary by up to 25 percent, subject to the setback requirements for residential uses, to create an interesting and attractive street edge and

support visual continuity on the block.

4. Active living spaces including community spaces, recreation rooms and gyms should be oriented to the front of the home/unit and at the ground level in multifamily projects.



5. Enhanced entryway features such as porches, stoops, balconies, decorative landscaping/paving at entryways, windows, and other architectural elements shall be incorporated to enhance the pedestrian-scale and character of the street.
6. For multifamily housing development, the minimum building separation shall be maintained to provide sufficient privacy between units, private outdoor open space and balconies. Windows, balconies, patios, and courtyards should be oriented to protect the privacy of users and reduce unwanted noise between units.



Elevated front stoops help distinguish between the public and private realm.

Lot and Building Variations

7. For detached single family lots in MDR and LDR zones, building elevations shall be varied to create interest along the street and include:
 - A mix of single, two and three-story units along a block, where appropriate;
 - Variation in the homes along the same street with different building heights, setbacks, massing, and roof shapes;
 - Avoidance of repetition of identical floor plans or elevations in homes next to each other;
 - Street corners suited for architectural treatments that engage the street, such as windows, wrap around porches, and other ground floor building articulation.
8. For multifamily housing in MDR and HDR zones, building design shall incorporate varied wall planes, scale and massing.
9. No building façade may extend n a continuous plane for more than 15 feet without a window or projection, offset, or recess of the building wall at least one foot in depth.
10. All street-facing facades shall have at least one horizontal or vertical projection or recess at last two feet in depth, for every 25 horizontal feet of wall. Building entrances and front porches may count toward meeting this requirement.



Varied wall planes, scale and massing should be used to break up and add interest to multi-unit residential buildings.

11. Where adjacent to single family or lower-density residential development, building scale and massing should be compatible with the character, rhythm, height, and massing of adjoining single family or lower density development. The height differential between any two adjacent parcels shall be no more than two stories or 25 feet, whichever is less, within 25 feet of the parcel line.
12. In order to achieve architectural diversity within low density (LDR), residential subdivisions, the following minimum range of different floors plans and building elevation shall be provided:

Number of Units	Required Number of Floor Plans	Minimum Required Number of Elevations (per plan)
<25 units	2 plans	4 elevations (2 per plan)
25-50	3 plans	9 elevations (3 per plan)
51-75	3 plans	9 elevations (3 per plan)
76-120	4 plans	12 elevations (3 per plan)
>120	5 plans	15 elevations (3 per plan)

Common and Private Open Space within Multifamily Residential Developments



Multifamily residential units should be designed around common open space incorporating walking paths, garden boxes and seating areas for resident enjoyment.

11. Arrange multi-family residential buildings to provide functional public and private outdoor space for residents.

12. A minimum of 48 square feet of open space shall be provided per unit in high and medium density residential zones where multifamily projects are proposed. Required open space may be private or common or a combination.

13. Common open space areas and facilities should be conveniently and centrally located to serve the majority of units. Amenities such as pools, club houses, or other recreation areas, common patios or greens, seating, picnic spots, tot lots, community garden boxes, and walking paths should be

incorporated, where appropriate, to serve different age groups and/or the anticipated



residents of the development.

14. Private open space, such as front yard area patios, stoops, porches, backyards, and balconies (for upper story units) is encouraged, contiguous to the units it serves.
15. Private open space, where provided, shall be a minimum of 48 square feet per unit and provide functional and comfortable outdoor living, including appropriate depths to accommodate chairs and small tables. Personal storage spaces can be designed as an extension of the private open space, but shall not count toward meeting private open space requirements for each dwelling unit.
16. Private open space should be integrated into the design of building and designed and located to take advantage of the winter sun and/or shade and breezes in the summer months, to the extent possible.

Connectivity

17. For multifamily development projects, an on-site pedestrian circulation system shall connect residents internally within the development to parking areas and open space, as well as to neighborhood sidewalks, paths and transit stops.
18. Adjacent commercial and retail uses shall be easily accessible by way of internal and/or mid-block paseos and pathways that connect residents to such amenities without requiring residents to walk to the perimeter of the block.
19. Where residential block sizes exceed 600 feet in length, a mid-block paseo, a minimum of 20 feet wide with a minimum 8-foot-wide accessible sidewalk or pathway, shall be provided to facilitate better internal connection for area residents.
20. Along pedestrian paths and paseos, provide pedestrian amenities, such as shade trees, seating, lighting, and landscaping, to extend the open space area and support safe pedestrian use.



Pedestrian walkways should connect residents to adjacent common areas, parks, parking areas, or other amenities.

B. Architecture and Massing

Building Materials

1. Building materials, textures, colors and treatments should be varied to avoid a dull monotonous façade, but should correspond to variations in mass and wall plane.
2. Materials should appear substantial and integral to the structure. Materials shall be durable to withstand weathering with age. Exterior finish materials shall have an expected lifespan of no less than 30 years. Non-durable materials, such as plastic, tin, and vinyl shall be avoided, as well as highly reflective materials. Exterior timber shall be protected from decay by stain and sealant.
3. When applicable, masonry must be applied authentically. Masonry shall wrap corners and terminate at inside corners. Other material changes shall occur at an inside corners or other appropriate transition elements, such as a fence line.
4. Stucco finishes shall be smooth or very light sand finish. Stucco foam trim, where used, shall be smooth finish or shall incorporate a natural woodgrain texture where appropriate.
5. Heavy knock down or “lace” stucco is not appropriate.



Exterior building materials should be high quality and correspond to changes in massing and wall plane.

Building Form and Architectural Style

6. Where the distance between multi-family buildings is 15 feet or greater, all elevations shall be articulated with windows, frames, shutters, and/or other architectural treatments consistent with the primary elevation. At corner lots, side yard facades shall maintain the same level of architectural detail as the front/primary façade.
7. Multi-family buildings shall convey, through modulation or offset planes, individual residential uses. Porches, stoops and balconies may be used to orient the building and residential units toward the street.
8. Long multifamily buildings should appear as distinct and articulated smaller buildings that complement the scale and massing of surrounding housing. Create variations in building facades through use of varied massing, shape, materials, and roof forms.
9. Building heights shall transition between larger-scale (medium- and high-density) and smaller-scale (low- and medium-density) projects on adjoining lots or across a shared street. The height differential between any two adjacent parcels shall be no more than



Multifamily buildings can convey individual residential uses through variation in wall plane, massing, materials and design elements such as elevated front entryways.



- two stories or 25 feet, whichever is less, within 25 feet of the parcel line.
10. Shade/shadow impacts shall be minimized by stepping back upper floors above three stories.
 11. Windows shall be located in a manner that avoids direct views into adjacent single family yards to respect and maintain privacy to the extent possible.
 12. Contemporary cottage, urban farmhouse or similar and/or complementary styles are encouraged within the lower density zones. A mix of modern and traditional architectural styles are appropriate for higher density residential development.

Porches, Entries and Courts

13. Entryways should be appropriately sized to the scale and massing of the home or building and should provide roof cover.
14. Porches, verandas, and other architectural elements such as a sidelight window panel should provide a clear sense of entry and design interest to a home.
15. Porches, patios, and forecourts shall have a minimum depth of 6 feet to accommodate circulation and chairs.
16. A pathway separated from the driveway leading from the sidewalk to the entry should be provided.



Doors and Windows

17. Doors and windows should be designed to add visual interest to the building elevation, while complementing the style and architectural features of the home.

18. Windows on street facing units should orient substantially toward the street. Windows should be inset from the wall and designed with high quality trim and details.
19. Exterior doors shall include raised panels, glass, transom windows, or other forms of architectural detail and articulation and should be inset a minimum of 3 inches from the building façade.



Doors and windows should provide high quality detailing and trim that complement the overall architectural style of the home.

Roofline



Varied rooflines add interest to a neighborhood and streetscape.

20. Variety in roof forms, ridge heights, massing, and overhangs will give individual character to homes and also provide an interesting rhythm and continuity along the street.
21. Where consistent with the architectural style of the home, the roof form should be articulated through use of dormers, bays, porch roofs, clerestories, cross gables, and hip forms that break up the roof and provide interest along the street.
22. Wide overhangs are encouraged, to provide shadow and depth to building elevations.
23. Rooftop gardens, decks or patios are encouraged, in multi-family developments but should take into account sun, and/or shade and breezes in the summer months.

Garages and Driveways

24. Garages in residential neighborhoods should be designed to be subordinate to the main living area, yet consistent in design.
25. Homes with garages facing the street are encouraged to have the garage door offset behind the front façade of the living area and covered porch, where provided. Alternative designs may be appropriate where sufficient and enhanced landscape design creates a usable outdoor living room and minimizes the prominence of a garage forward design.
26. Garage doors shall include patterns complementary to the architectural character of the house. Windows should be considered to allow for natural light. Frosted/opaque windows may be used to enhance security. Garage doors shall be recessed a minimum of six inches from the garage door frame.
27. Three car garages must separate and offset the third car garage elevation from the others



by at least two feet in width and depth.

28. Detached garages and carports shall be designed to include a minimum of one of the following from the main building: materials, detailing, roof materials, and colors.
29. Use of alternative pavement surfaces, such as concrete pavers, brick, or stone and alternative driveway treatments, such as ribbon driveways are encouraged. Ribbon driveways are made up of two parallel strips of paving, with a strip of grass or pervious pavers between the paving strip, to minimize the impervious driveway surfaces to the area where the vehicle will be driving or parking.

C. Small Lot Development

Entryways

1. Small lot and higher density subdivisions should provide distinguishable and articulated entryways including recessed doorways, overhead projection/awnings, elevated stoops and/or unique paving to differentiate private space from the public sidewalk/pathway.
2. A separation of at least 8 feet shall be provided between the entryway of one unit and the nearest adjacent building wall to allow for sufficient light and ventilation between units. A minimum of 7 feet of the separated distance should be clear to the sky.
3. Address and unit identification as well as decorative low-level lighting shall be provided at the primary entrance.



Small lot residential development should provide distinguishable entryways through small patio spaces, elevated stoops or alternative paving.

Private Outdoor Space

4. Small lot developments should maximize access to private outdoor space, light and air through the use of roof decks and private balconies/patios.

Private courtyards can be incorporated into small lot development through low walls, appropriate vegetation or other design appropriate space delineators.



D. Sustainable Design

Energy Conservation

1. Energy conservation strategies, including window shading devices, selection of colors to reduce heat gain, energy efficient case windows, cool roofs, high-quality insulation and radiant barriers, solar panels, and other features are encouraged to reduce energy consumption associated with heating and air conditioning during winter and summer months, while also contributing to a local community vernacular.
2. All housing units shall be pre-wired for electric vehicle and solar PV systems.
3. Energy-Star rated residential appliances should be incorporated where appropriate.
4. Home and building design and placement should take advantage of passive solar opportunities.
5. Builders are encouraged to use advanced house framing or optimum value engineering to reduce lumber use and waste and improve energy efficiency in a wood framed house.



Architectural detailing such as window awnings or overhangs can minimize direct sunlight intrusion in summer months and reduce the need for artificial cooling.

Water Conservation



Bioswales can serve as natural stormwater filtration and attractive landscaping within a project.

6. Water reduction strategies including the use of high efficiency, low-flow fixtures and tankless water heaters shall be incorporated.
7. Residential projects shall control and treat storm water runoff onsite to the extent feasible through green roof design, onsite bioswales and biofiltration systems or other measures.
8. Recovery of graywater for on-site uses including landscaping is encouraged
9. Residential projects shall connect to the City's reclaimed water system for landscape irrigation.
10. Install climate sensitive irrigation systems and other water-efficient bubblers and drip irrigation systems.



Sustainable Materials and Construction Practices

11. Sustainable, local and renewable building materials should be used and verified whenever feasible.
12. Projects shall strive to reduce or eliminate waste during design, construction, operation and end of life in order to conserve natural resources.
13. The use of low-carbon cement is encouraged whenever feasible and/or the use of alternative materials.
14. Consider prefabricated building/structural components when feasible.
15. Leftover material stock should be recycled, donated or returned for alternative use at the completion of construction.

Low Water Landscape

16. Landscaping should consist of climate-appropriate plantings, including drought-tolerant and native species suited to the Woodland community.

E. Landscaping

Residential Landscaping

1. Native plant species should be used whenever possible.
2. A variety of shrubs and groundcover species as well as a balance of hardscape and paving shall be used within common open space areas and courtyards within multifamily projects and along pedestrian pathways and paseos.
3. Lawns and highly ornamental landscaping that require frequent pruning and maintenance should be used sparingly.
4. The design and location of trees and landscaping for homes shall consider opportunities for solar access and solar panels, as well as, shading and ventilation on hot summer months.
5. Trees shall be a minimum of 15-gallon size and shrubs a minimum of 5-gallon size at the time of planting.
6. Soil amendments shall be applied to support plant health and longevity.



Native and drought tolerant landscaping is encouraged throughout the Plan Area.

Landscaping Adjacent to SR 113

7. Landscaping along SR 113 shall consist of a mix of trees, low groundcover and vine training on all sound walls or highway adjacent perimeter fencing.

F. Walls and Fencing

Height requirements

1. Interior and Corner Lots. Only fences or walls at or under three feet, six inches (3'-6") in height may be placed along the front yard property line or within the front yard setback

area.

2. Interior Lots. Fences or walls at or under seven feet (7'-0") in height may be placed within the rear yard or interior side yard setback areas.
3. Corner lots. Fences and walls greater than three feet, six inches (3'-6") in height up to a maximum of seven feet (7'-0") in height shall maintain a minimum setback of five feet from the street side property line. Only fences and walls three feet, six inches (3'-6") in height or less may be located within five feet of the street side property line of corner lots.

General

4. All fences over six feet in height require a building permit.
5. For swimming pool fencing, refer to WMC Section 17.104.100(E)
6. Solid masonry or block walls in the community will be limited to those areas requiring sound attenuation to achieve noise standards and screening from unsightly elements, such as trash areas. Sound walls are not expected to be required within the Plan Area, except where necessary along SR 113 where adjacent to residential development.
7. The preferred edge treatment along CR 25A adjacent to residential uses is an open style fence softened with landscaping to provide security and privacy to residences.
8. Where long expanses of wall or fences are determined to be unavoidable, based on a noise analysis, articulation of the wall with offsets or landscaping shall be implemented.
9. Vines shall be planted and trained on all masonry walls.
10. Walls or fences along CR 25A should not exceed a maximum unbroken length of 400 feet, without an opening for pedestrian access.
11. Allowable materials for walls include:
 - Stamped or embossed concrete or concrete block with patterns and colors that reflect Woodland's local context, such as agriculture, agricultural technology, native trees and grasses, and/or historic themes;
 - Natural or cultured stone; and
 - Brick or similar materials with colors that are compatible with building color themes within the RTP zones.
12. Chain link fences and wrought iron or steel fencing with points or spears are prohibited in all residential areas.
13. All wood fencing shall be protected with a colored, semi-transparent water sealant stain.
14. Wood fencing in residential areas shall include a decorative cap where visible from the public right of way (i.e. not dog eared).



Vines and landscaping help soften retaining walls and masonry walls required for sound attenuation.



G. Street Lighting

1. Pedestrian-scaled ornamental street lights should be located along public streets and internal walkways between housing units to provide adequate illumination for safety and navigation. Alternatively, bollard lights may also be provided along internal walkways subject to City approval.
2. Lighting shall be directed and shielded to avoid unintended spill over onto adjacent properties. A minimum of 1-foot candle of lighting shall be provided within parking areas, along pedestrian pathways and at building entrances. "Hot spots" of 10-foot candles or more shall be avoided.
3. Required illumination shall at minimum meet Uniform Security Code requirements as set forth in the Woodland Municipal Code.

H. Signage

General

1. Design Review approval is required for all signage.
2. Signage within residential zoning designations is permitted only where commercial uses are also allowed and for multi-family/apartment developments.

Setback, Height and Area Requirements

3. Sign setback, height and area requirements are subject to the Table 3.5:

	Max Sign Height		Min Setback from Property Line	Total Sign Area Per Business	Total Sign Area Per Business Cannot Exceed
Zone	Wall Mounted Signs	Ground Mounted Signs	Ground Mounted Signs	All Permanent Signs	All Permanent Signs
LDR, MDR, VCLDR, VCMDR	12ft	4ft	3ft	-	20ft
HDR, HDR/CCO	20ft	12ft	5ft	The length of building frontage x 1.5	175sf

Location

4. Signs should be considered during building design and properly scaled and located so as not to dominate or interfere with architectural detailing and overall building façade.
5. Signs should consider pedestrian readability and should help identify the location of primary entrances to the building or community.
6. Permanent rental or real-estate signs are not permitted.
7. Cabinet, box-signs and internally illuminated pan-channel letters are prohibited in

residential zones and on residential projects. Signage should be of high quality design and materials.

8. Multi-tenant signage within the HDR/CCO zone should be coordinated and cohesive in appearance including consistent sizing and lighting requirements.
9. Customized projecting, blade signs or under canopy signs are encouraged within the HDR/CCO zone. Blade signs must provide at least 8 feet of vertical clearance as measured from the sidewalk to the base of the sign.
10. Signs and structural features shall be subject to cross visibility requirements as outlined in the Woodland Municipal Code and subject to review and approval by the City Traffic Engineer.

Text

11. Signs may only contain the name and logo or symbol of the business/residence and its street address. Trade slogans and commercial advertisements are not permitted.
12. Logos shall not exceed 4 square feet in size except if it is determined by City Planning staff that building scale or other factors, such as enhanced design and quality, warrant increased logo square footage.

Materials and Design

13. Signs shall be constructed using durable, fade and weather resistant materials such as stone, tile, cast concrete, or other similarly durable masonry, metal or wood materials consistent with the architectural design and style of the building.
14. Unique and creative sign design is encouraged to enhance the unique identity of the Plan Area and residential zone.
15. Faded or damaged signs shall be replaced or repaired in a timely manner.
16. Cabinet or box signs, roof mounted signs, window signs above the second floor and pole mounted signs are not permitted.
17. Feather signs, flags, banners and other attention getting signs or devices are prohibited except in the HDR/CCO zone during grand openings, in which case they must be removed within 30 days.

Illumination

18. Signs shall be spot illuminated from the front or consist of letters, numbers, or graphics that are halo backlit. Internally illuminated pan channels letters are not appropriate except within the Community Commercial and Highway Commercial zones. Neon is permitted when integrated into custom designs.
19. Flashing, traveling, animated, or intermittent illumination is prohibited.
20. Exposed conduit, tubing and raceways are prohibited. Transformers and other equipment shall be fully screened/concealed.

I. Trash, Recycling and Enclosures

Mechanical Equipment and Trash Enclosures

1. All mechanical equipment, service areas and trash enclosures shall be located away from



or adequately screened from public view by solid fencing, decorative landscaped walls, and/or landscaping. Backflow prevention devices shall not be located within the public right of way or along primary pedestrian streets.

Enclosures - Multifamily

2. Trash and recycling enclosures within multi-family development projects shall consist of the same or similar design and materials as the overall project and shall adhere to City trash and recycle enclosure specifications.
3. Enclosures shall be located to the side or rear of the building, away from public view and should be separated from adjacent parking stalls by a minimum 3-foot-wide landscaped planter where applicable.
4. Trellis structures and vines are encouraged as a means of screening the sides and rear of trash enclosures.
5. The area around trash enclosures shall be kept free of trash and debris.



Trash and recycle enclosures should be designed with similar or complimentary materials as the overall project.

Trash receptacles – Single Family

6. A minimum 3’x10’ concrete pad shall be provided behind the fence line and away from public view for single family trash totes and storage containers. A clear/unobstructed path of travel from the pad to the street shall be provided for ease of disposal.

J. Circulation and Parking

Circulation and Parking



Pedestrian paths should provide ease of access to common space, parking areas or other amenities within or adjacent to the project.

1. Whenever possible, separate access for pedestrians from vehicles and high-use pedestrian walkways and crosswalks with color or special paving treatments. Pedestrian access should be prioritized over vehicular access.
2. Street facing driveways should be limited to 20 feet in width or less.
3. The number of driveways should be minimized to avoid loss of on-street parking spaces.
4. Within multi-family housing projects, pedestrian paths should be clear, understandable and easy to navigate, with emphasis on connecting to common open space and adjacent public streets, sidewalks, and local destinations and to amenities such as commercial services, transit facilities, and the open space greenbelt.
5. Minimize the visual prominence of vehicles from the street

front by siting parking areas to the side or rear of the property and providing smaller parking areas, rather than one large parking lot.

6. Parking areas including pedestrian walkways shall be illuminated for safety and security.

Electric Vehicle Charging

7. All multifamily residential parking lots and/or structures shall provide electric vehicle charging pursuant to the California Green Building Standards Code, Tier 1 requirements.
8. All single family, townhomes and duplexes must be EV capable.

Parking Lot Landscape and Shading

9. On-site parking in medium- and high-density residential developments should be located internal to the site and away from public roadways. Surface parking lots should be avoided in favor of “tuck under” parking were feasible.
10. Proper tree variety and placement shall be designed to result in 50 percent parking lot shading within 15 years of installation. Primary entrance roads and driveways not used as back-up areas are exempt from this requirement.
11. Solar and shade covers are permissible in medium- and high density developments, but should not significantly reduce the number of trees otherwise required to provide 50 percent lot shading.
12. Surface parking lots visible from the public right-of-way shall incorporate landscape screening, trellising, low walls/fences at least 36 inches in height to continue a “green wall” and minimize interruption to the streetwall.
13. At least 6 feet of landscaping should separate surface parking from any adjacent sidewalk or public right-of-way.
14. Shaded sidewalks or other pedestrian walkways shall be clearly delineated by way of landscaping and enhanced paving or design and should connect parking areas directly to building entries.

Bicycle Parking

15. Bike racks and lockers shall be provided and located within 30 feet of multifamily and mixed-use building entrances, in a safe, well-lit area, and permanently anchored to a hard, dust free surface, preferably asphalt or concrete slab, and installed in a manner which allows adequate space for access to the bicycle and locking device, and avoid pedestrian conflict.



Bike parking facilities should be located in a visible and well-lit area and permanently anchored to a dust free surface.



Alley Access

16. Rear loaded garages with alley access is encouraged where feasible.
17. The use of pavers or stamped/colored concrete shall be provided within alleys to provide a durable and attractive passageway and help define a shared pedestrian experience.
18. Alleys shall include a minimum of 1-foot candle of lighting for safety and security. Address signs shall be illuminated.

3.5.13 Special Character Guidelines for High Density Residential Zones (HDR, HDR/CCO)

A. Site Design

Well Defined Streetwall

1. Building placement along Road B should provide clearly defined edges to the public realm and activate the street.
2. For parcels with frontage along Road B within the HDR/CCO Zone, 60 percent (60%) of the property shall be developed at the minimum setback line (development may be phased over time). Exceptions may be granted to allow for well-designed outdoor space that includes vertical and horizontal elements that simulate and maintain the appearance of a streetwall.
3. Within the HDR/CCO zone, a minimum of 60 percent (60%) of the Road B frontage ground floor uses shall consist of commercial/retail uses. This minimum requirement may be reduced or waived by the Community Development Director for an affordable housing project.
4. Buildings over three stories should stepback or provide a varied wall plane on street facing upper floors (above the third floor) to maintain a human scale streetwall.
5. The number of limited access (right-in/right-out) driveways shall be limited to one from Road B between CR 25A and Road C unless otherwise approved by the city, to minimize disruption to the pedestrian environment.
6. On-site parking is not permitted along the Road B frontage.
7. Landscaped and illuminated pedestrian connections shall be provided between the HDR zone and the North Campus Research and Technology Park.

B. Architecture and Massing

Gateway (Primary)

1. Buildings located at or the near the corner of CR 25A and Road B should be designed in a manner which provides a sense of arrival and should reflect the distinctive character of the Plan area as an agricultural and technological center for innovation.
2. Enhanced landscaping shall be provided along CR 25A between Road B and Road D with an emphasis at the intersection of CR 25A and Road B to accentuate this location as the primary Plan Area entryway.

Road B Frontage

3. Residential and/or mixed-use residential projects with frontage along Road B in the North or South Urban Village shall be designed in a manner so as to not “back-on” to Road B. Windows, entryways, patios and/or other design features shall be located along the Road B frontage to give the appearance of a primary building frontage.
4. Utility doors and other back-of-house items shall be avoided along Road B and CR 25A or shall be screened from public view by decorative trellising landscaping.

Ground Floor Articulation and Activity



The ground floor of mixed use buildings along Road B shall include retail and commercial uses to bring activity to the public realm.

5. The ground floor of all buildings, particularly those façades facing the public right-of-way, should support activity on the street and bring the life of the building into the public realm through the use of frequent and well defined building entryways, façade transparency at the street level, and through architectural detailing such as awnings, canopies, recesses, and enhanced materials and surface changes that provide an interesting and pleasant public experience.

6. Active uses should be located on the ground floor along the building frontage, visible to the public realm/public right-of-way and may include, but are not limited to

retail storefronts, cafes/restaurants, community rooms and kitchens, employee support areas, recreational facilities, exhibition space, and lobbies.

7. Articulated wall planes at the ground floor along the public right-of-way or where substantially visible to the public should be offset by at least 2 to 4 feet and may be combined with color or material changes to provide substantial shadow lines and visual interest and allow for enhanced landscaping along the base of the building.

C. Driveways, Parking and Service Aisles

Driveway Location

1. Driveway placement should avoid disrupting the streetwall and pedestrian environment and should be shared, located behind or to the side of buildings whenever possible.

Parking

2. On-site parking shall be located behind buildings and away from CR 25A and Road B.

D. Commercial and Residential Compatibility

Mixed Use Projects

1. Where mixed-use projects include residential units in the HDR/CCO zone, special consideration should be given to the location of noise or odor generating or visually



- unappealing equipment, including vents/fans, refrigeration units and trash enclosures.
2. Proper screening, odor reduction and sound attenuation shall be provided to reduce impacts and screen such equipment from residential units/view.
 3. Commercial uses along Road B should locate at the ground floor and residential units locate above the ground floor to minimize noise impacts to the residential units and to maintain an active gateway area streetscape.
 4. If residential units are located along Road B, a landscape buffer of at least 10 feet shall be provided from back of walk.

3.5.14 Special Character Guidelines for Low Density Residential (LDR-East Village) and Medium Density Residential (MDR) Zones

A. Neighborhood Design

Building Orientation and Setbacks

1. Alley-load and non-traditional subdivision layouts that create a more urban and active environment are encouraged within the East Village Low and Medium Density residential zones.
2. Deviations from setback requirements will be considered on a case-by-case basis to allow for innovative subdivision layout that meet these objectives based on a finding that adequate unit separation for light, air and ventilation and private open space is still maintained as determined by the Community Development Director. Where projects are located adjacent to existing residential development, setback standards shall be maintained except as provided for in Chapter 7, Section 7.4.1.2, Minor/Administrative Modification.



Non-traditional residential design may include alley load and cluster style development. Where appropriate, deviation from traditional setback standards will be considered to support such development.

Adjacency to Harry Lorenzo Greenbelt

3. Residential units within the East Village are encouraged to front onto the Harry Lorenzo greenbelt or are designed in such a manner as to actively engage with the trail system through open style fencing, patio orientation or through the incorporation of accessory dwelling units that front on to the trail.
4. Residential units shall not back on to a greenbelt trail or pedestrian path/paseo and garages/driveways shall not be located adjacent to (or front on to) greenbelt trails/pedestrian paths.
5. Subdivisions between Parkland Avenue and CR 25A within the East Village shall incorporate pedestrian connections via paseos or local roads from the Harry Lorenzo

greenbelt to the Village Center District at least every 300 feet.

Adjacency to Marston Road and Parkland Avenue

6. Low density, detached single-family units are discouraged from fronting on to Marston Road or Parkland Avenue.
7. Rowhouse/townhouse style residential design is encouraged in the Medium Density Residential zone where adjacent to Marston Road. (See Chapter 6, Section 6.5.3, RDZ Density Requirements)
8. Units are encouraged to front Marston Road creating an active, urban streetscape, but may include a secondary alley load entrance away from Marston.
9. No driveways or on-site vehicle parking is permitted along the Marston Road frontage.
10. Walk up stoops or other elements providing a vertical separation between residential units and Marston Road is encouraged.
11. The number of access driveways between Road B and the Harry Lorenzo greenbelt should be limited to avoid disrupting the pedestrian environment.

3.5.15 Special Character Guidelines for Low Density Residential (LDR-North Village) Zone

A. Neighborhood Design

Innovative circulation/infrastructure solutions

1. Innovative neighborhood design concepts are encouraged throughout the Specific Plan's residential districts. In particular, innovative and non-traditional pedestrian paths, landscaping and natural storm drain/water filtration systems that create a distinct, more rural neighborhood character should be considered in the North Village.
2. Curving (non-grid) streets are permitted in the North Village as a means of creating an organic neighborhood design. However, pedestrian connectivity should still be provided throughout the neighborhood and to the Harry Lorenzo greenbelt and to Parkland Avenue.

Adjacency to Hwy 113

3. Residential units that back up to Hwy 113 shall maintain a minimum 30 foot setback from the soundwall/Hwy 113.
4. Single story homes are encouraged where adjacent to Hwy 113.



Adjacency to Harry Lorenzo Greenbelt

- 5. It is encouraged that residential units within the North Village front onto the Harry Lorenzo greenbelt or are designed in such a manner as to actively engage with the trail system through open style fencing, patio orientation or through the incorporation of accessory dwelling units that front on to the trail.
- 6. Residential units shall not back on to a greenbelt trail or pedestrian path/paseo and garages/driveways shall not be located adjacent to (or front on to) greenbelt trails/pedestrian paths.



Accessory Dwelling Units are encouraged along the Harry Lorenzo Ave greenbelt to actively engage the trail system.

Adjacency to Parkland Avenue

- 7. Low density, detached single-family units are discouraged from fronting on to Parkland Avenue.

B. Architecture

Design

Architectural styles within the North Village should help create a distinct neighborhood identity and should generally consist of Urban/Modern Farmhouse, Contemporary Cottage or French Country building forms and elevations.



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4.0 | MOBILITY AND CIRCULATION







4.1 | OVERVIEW

The mobility and circulation framework for the Plan Area is a modified grid, complete street system that accommodates all modes of travel in a walkable, urban environment. A seamless network of dedicated bike/pedestrian facilities promotes convenient access between commercial centers, residential neighborhoods, and open spaces, and allows employees, residents and patrons to choose from a range of transportation options. The shared mobility hub creates a central point of connection, integrating a range of transportation choices to accommodate emerging technologies. A hierarchy of street types ensures proper performance for vehicle and alternative modes while encouraging shared mobility options. Streetscape design standards establish the Plan Area as a distinctive place, creating an attractive and engaging public realm.



A seamless network of dedicated bike/pedestrian facilities promotes convenient access between commercial centers, residential neighborhoods, and open spaces, and allows employees, residents and patrons to choose from a range of transportation options.

This chapter describes the transportation modes, circulation networks and standards for Plan Area roadways, public transit, bicycle and pedestrian facilities. Roadways have been designed to balance the circulation and flow of vehicular traffic with the provision of safe and accessible facilities for walking, biking and public transit. A modified grid street network provides circulation and access within the Plan Area, to Spring Lake and adjacent areas of the city and distributes Plan Area traffic to support a walkable and bike friendly community. Plan Area roadways provide circulation and connectivity and incorporate landscape elements which provide aesthetics, shade and stormwater management. Specific guidance for bike paths, sidewalk improvements, medians and parkways, landscaping and other streetscape elements are defined in this chapter.

4.2 | NEXT GENERATION MOBILITY

The digital revolution is opening up new technologies and transforming the way goods are purchased and delivered and how people chose to get from place to place. For the past 75 years, the design of communities has been driven primarily from the perspective of the automobile user. The emergence of the shared economy and advent of autonomous vehicles are disrupting the traditional way of thinking about personalized transportation. How cities adapt to and plan for future transportation innovations is a topic of much discussion.



The Plan Area is designed to accommodate current systems while anticipating emerging technologies that will offer an increasing range of transportation options for both personal and commercial use. The Village Center will serve as a social and functional hub of the Plan Area where all modes intentionally intersect at a “shared mobility hub”. Supported by the compact and mixed-



The Plan Area road network is designed to accommodate alternative modes of transportation and as well as future technologies.

use nature of the plan, active transportation modes are prioritized throughout the project to promote biking and walking as a preferred mode of choice. Streets and path facilities are designed to provide efficient movement while creating a stronger sense of place, rather than compromising the human experience. Facilities and programs will be implemented to support a shift away from individual vehicle use and toward active and shared mobility options in order to achieve the General Plan goal of 10% reduction in vehicle miles traveled (VMT).

This section summarizes the planned facilities, systems and programs that are contemplated in the Plan Area and will be the focus of the transportation demand management (TDM) program, described in Section 4.2.7. Funding sources and mechanisms for the mobility amenities, programs and services described in the following sections are addressed in the Finance Plan and Development Agreement.

4.2.1 Active Transportation

Supported by a fine grain network of pedestrian / bicycle facilities, the WRTP promotes active and more individualized forms of transportation such as biking, boarding, scootering, and walking. The Plan Area mix of residential, employment, services, recreational uses proximate to each other and

transit services make it convenient to choose alternative transportation choices throughout the day. Active transportation facilities or amenities at the beginning and end of the trip destination are incorporated throughout the Plan Area to further encourage active transportation as a primary means of getting around.



4.2.2 Shared Mobility Hub

Located at the Village Center, a shared mobility hub, *The Union*, will at project build out, serve as the nucleus of the alternative transportation system providing integrated access to intra-city as well as inter-city transit service. The Plan Area's seamless network of roads, bike paths, and sidewalks combined with a variety of transportation services will offer multiple choices to support "last mile" connectivity. The Union, in conjunction with the broader Village Center, will be designed to accommodate a range of potential alternative transportation choices such as:

1. Fixed route bus and micro-transit
2. Car/vanpool/parking
3. Designated car share spaces
4. Electric vehicle charging stations
5. Ride hailing services
6. Bike/scooter share docking stations

The Union will include design features such as architecturally enhanced covered waiting areas, including distinctive seating areas/benches; scooter/bike share facilities and designated parking areas; covered and uncovered bike parking and where feasible, long term/rentable storage

lockers; bike repair station; hydration station/water fountain; wayfinding kiosk; ticketing kiosk as appropriate; enhanced landscaping, shade trees, and landscape features (i.e. rock, decorative fencing, public art) and textured/stamped concrete walkways; and overhead pedestrian scale lighting and bollard lights where appropriate. Services and amenities at the Village Center such as designated car-share/vanpool spaces and EV charging in the parking areas, cafés, a market and co-working spaces will create opportunities to further reduce vehicle miles traveled and GHG emissions, and enhance *The Union* as a place of connection.

Development of *The Union* will occur in phases to correspond with service demand generated as the Plan Area builds out. Meanwhile, an increasing number of UC Davis faculty, staff, and students at UC



The Plan Area road network is designed to accommodate alternative modes of transportation and as well as future technologies.

Davis are choosing to live in Woodland. Campus planners are seeking new ways to promote transit and carpool options for campus-bound commuters and have engaged with the City to explore effective alternatives to single-occupant driving. The Plan Area's strategic location along Highway 113 at the southern-most side of the community, creates a unique opportunity to establish a convenient point of connection for van/carpool and transit services.

Early phases of the Plan Area's shared mobility hub will include park and ride facilities to promote carpooling. An illustrative layout and concept site plan of the shared mobility hub are provided below. As demand grows with the employment and housing development, the full set of transit and shared mobility services will be added at The Union.



Elevated Perspective of the mobility hub "The Union," and Village Center – Artist Rendering

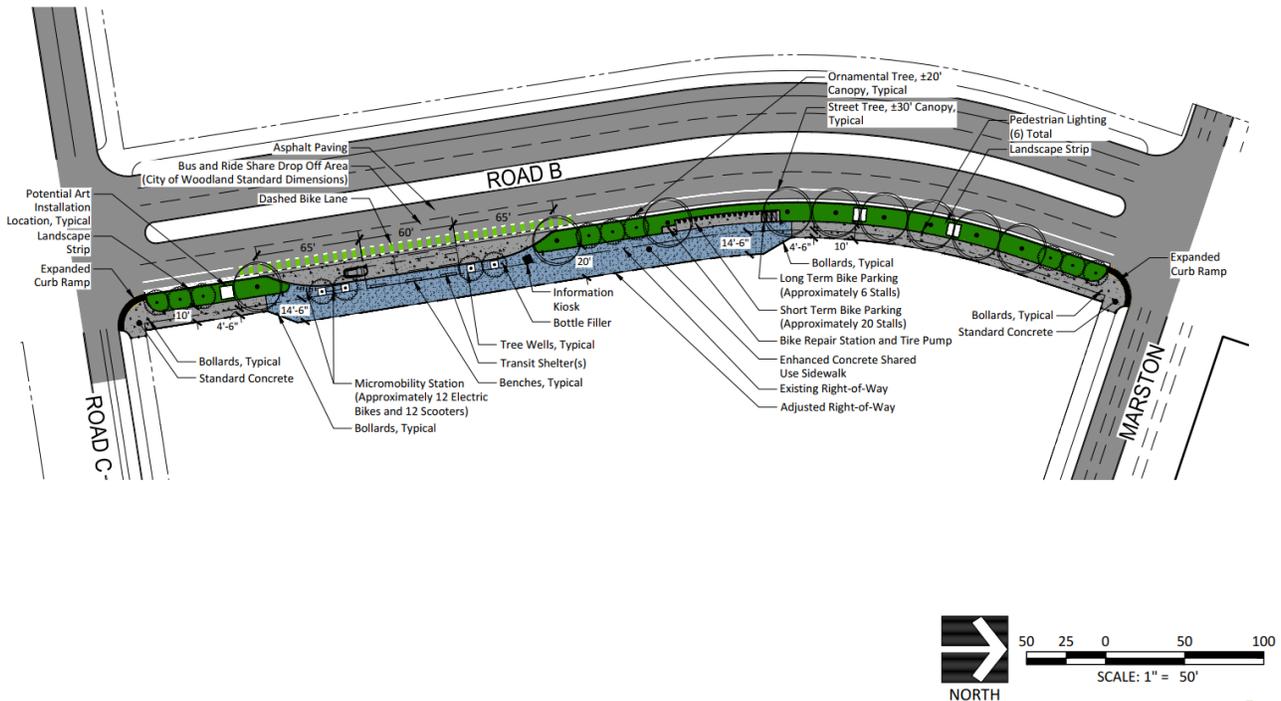


EXHIBIT 4-1: CONCEPT SITE PLAN OF THE MOBILITY HUB “THE UNION.”

4.2.3 Fixed Route Transit Service

Development of The Union will be planned and coordinated with local and regional transit service providers including YCTD and UC Davis Transportation and Parking Services/Unitrans. The shared mobility hub will be the primary point of connection for any fixed route bus service with YCTD’s YoloBus service. Additionally, a new shuttle/vanpool connection providing frequent connection to and from UC Davis and the Davis Amtrak Depot is planned. This service will link into regional transit services connecting into high employment areas such as downtown Sacramento/UC Davis Med Center and the Bay Area. This service will be evaluated as part of the TDM program.

4.2.4 Micro-Transit Service

On-demand shuttle services are supplementing, and in some cases replacing, intra-city fixed route transit service in communities across the country. Woodland and YCTD will be piloting a micro-transit service in 2023 which will inform the future application and expansion of this transportation amenity in the community. The Plan Area is planned and designed to anticipate this service which will not only offer an additional option for “last mile” connection to the shared mobility hub but also provide residents and employees with the flexibility to choose alternative transportation options for primary commute trips and utilize on-demand services for ancillary trips throughout the day.

4.2.5 Vanpool/Ride Share

To supplement transit services, particularly in the earlier phases of the Plan Area build out when ridership numbers have not reached critical service demand levels, park and ride lots will be utilized to support vanpools and ride sharing. Additionally, dedicated parking spaces for car/vanpool and ridesharing will be required for large employment uses. Supplemental funding may be needed initially to subsidize these services and build ridership demand. Funding from UC Davis and the Plan Area's transportation demand management (TDM) program could be key sources of funding to support these services.

4.2.6 Vehicular Travel

The Plan Area accommodates automobiles yet, is intentionally designed to reduce their use through a combination of a modified grid street network and alternative transportation services/amenities that promote alternate mode choices. To further reduce impacts of automobile use, the plan integrates infrastructure and development standards that support zero emission vehicles and car share programs that will reduce reliance on individual vehicle ownership. The following examples of current, and/or future, innovative transportation solutions will be deployed to help reduce vehicular use and GHG emissions.

1. Electric Vehicles. Every parking lot and garage within the Plan Area will be EV ready for the addition of charging facilities. Dedicated EV parking stalls will be placed near primary entrances, key points of destination, and at park and ride/transit stops.
2. Car Sharing Services. On and off-street parking spaces in key locations will be dedicated to car sharing, or similar services, in cooperation with existing and future successful private car sharing vendors such as ZipCar, GIG Car and City CarShare.
3. Ride Hailing Services – Designated curbside zones for ride hailing (i.e. Uber/Lyft) will be strategically located at the Union and other locations within the Plan Area.
4. Smart Parking – Use of intelligent parking management technologies, such as Woodland-based JAPA, may be employed in high demand parking areas to monitor utilization, inform drivers of available parking spaces, and offer adaptive time limit and permitting options.



To reduce impacts of automobile use, the plan integrates infrastructure and development standards that support zero emission vehicles and car share programs that will reduce reliance on individual vehicle ownership.



4.2.7 Transportation Demand Management

Reduction of personal vehicle use and an overall vehicle miles travelled by 10% will be achieved through a variety of project features and services such as the ones described in this Chapter. A Transportation Demand Management (TDM) program for the project will be adopted prior to the first tentative map to guide the build out of the Plan Area and the phased implementation of enhanced mobility facilities and programs.

4.3 | MOBILITY AND CIRCULATION NETWORK OVERVIEW

Proposed land uses, densities and the individual district character described in Chapters 2 and 3 have a close connection to streetscape design standards described in this chapter. Together, land use patterns and streetscape contribute to a sense of place by ensuring that local streets are not over- or under-designed to accommodate both local and through traffic, and parking demand generated by development. Further, streetscape design, including greenbelts and pedestrian paseos, help create a cohesive community identity, enhanced with landscaping, street furniture and lighting, entryways and public art.

The street and path networks for the Plan Area have been carefully designed to support the distribution and function of various uses within the land use districts while also serving as a primary place making feature. Each district is associated with unique types of travel demand which is served by a highly connected hierarchy of streets and pedestrian / bicycle paths. The envisioned circulation networks and street hierarchy is shown on Exhibit 4-3, followed by the street cross section designs. As the Plan Area develops, specific street sections may require modifications in response to unique or unforeseen circumstances. Modifications to the street sections included in Section 4.5 shall be subject to review and approval by the Community Development Director in consultation with the City Traffic Engineer. Providing all modes of mobility equal consideration while emphasizing active and alternative transportation options is a primary goal of the Specific Plan and the 2035 General Plan. Figure 14-16: Table 3-2 from the 2035 General Plan, summarizes the mode priority.

The overall Mobility and Circulation Network and streetscape program consists of the Active Transportation Network and the Roadway Network. The Active Transportation Network includes descriptions of the facilities such as multi use trails, paths, on street bike lanes, shared use marking and pedestrian promenades and sidewalks. The Roadway Networks describes the street hierarchy of Principal Arterial Streets, Minor Arterial Streets, Collector Streets, Local Streets, Alleys as well as Roundabouts / Enhanced Intersections and Streetscape Design Features.

4.4 ACTIVE TRANSPORTATION NETWORK

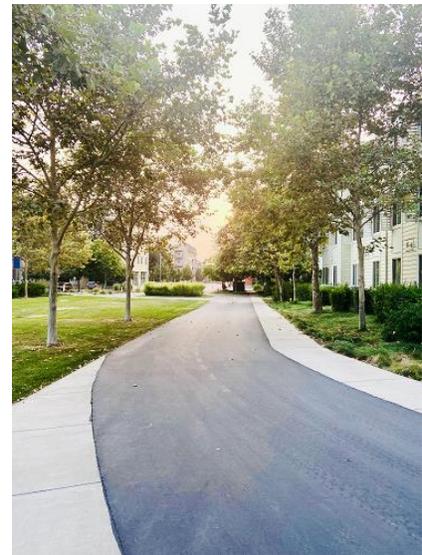


Dedicated on- and off-street bike facilities will allow both transportation and recreational benefits for Plan Area residents and employees.

The Plan Area’s active transportation network is comprised of dedicated on-street and off-street facilities that provide both transportation and recreational benefits, as depicted in Exhibit 4-2, Active Transportation Network. The Plan Area’s interconnected system of sidewalks, bike lanes and multi-use trails/paths are intentionally designed to link seamlessly throughout the Plan Area and to the shared mobility hub promoting more active and healthier transportation choices. Enhanced crossing features and roundabouts will be used where the active transportation network intersects with the primary roadway network.

The backbone of the network is the 4.9 miles of Class 1 facilities distributed throughout the Plan Area, creating a system of greenbelt trails and more formal multi-use paths. A primary feature of the network is the north-south greenbelt spine that serves as a key connector of existing and planned trail networks. Abandoned portions of Harry Lorenzo Avenue will be converted to greenbelt and integrated into a linear greenway connecting existing trails that extend east-west along CR 25A, Marston Ave, Parkland Avenue, and Farmer’s Central Road.

The planned bike and pedestrian overcrossing at the north boundary of the Plan Area will offer safe and convenient access across HWY 113 and connection to the west side of the community via the Woodland Parkway. The future Parkland Avenue overcrossing will provide an additional Class 1 path creating connection between future development of the Specific Plan 1 Area on the west side of HWY 113. The Plan Area is also a key point of connection to the future Woodland-Davis Bikeway / Alternative Transportation Corridor that is envisioned to provide an off-street path between the two cities.



LEGEND

- MULTI USE TRAIL (CLASS I)
(DASHED WHERE FUTURE)
- MULTI USE PATH (CLASS I)
- ON STREET BIKE LANE (CLASS II)
(DASHED WHERE FUTURE)
- SHARED LANE MARKINGS (CLASS III)
-  ROUNDABOUT
-  ENHANCED CROSSING
-  SHARED MOBILITY HUB

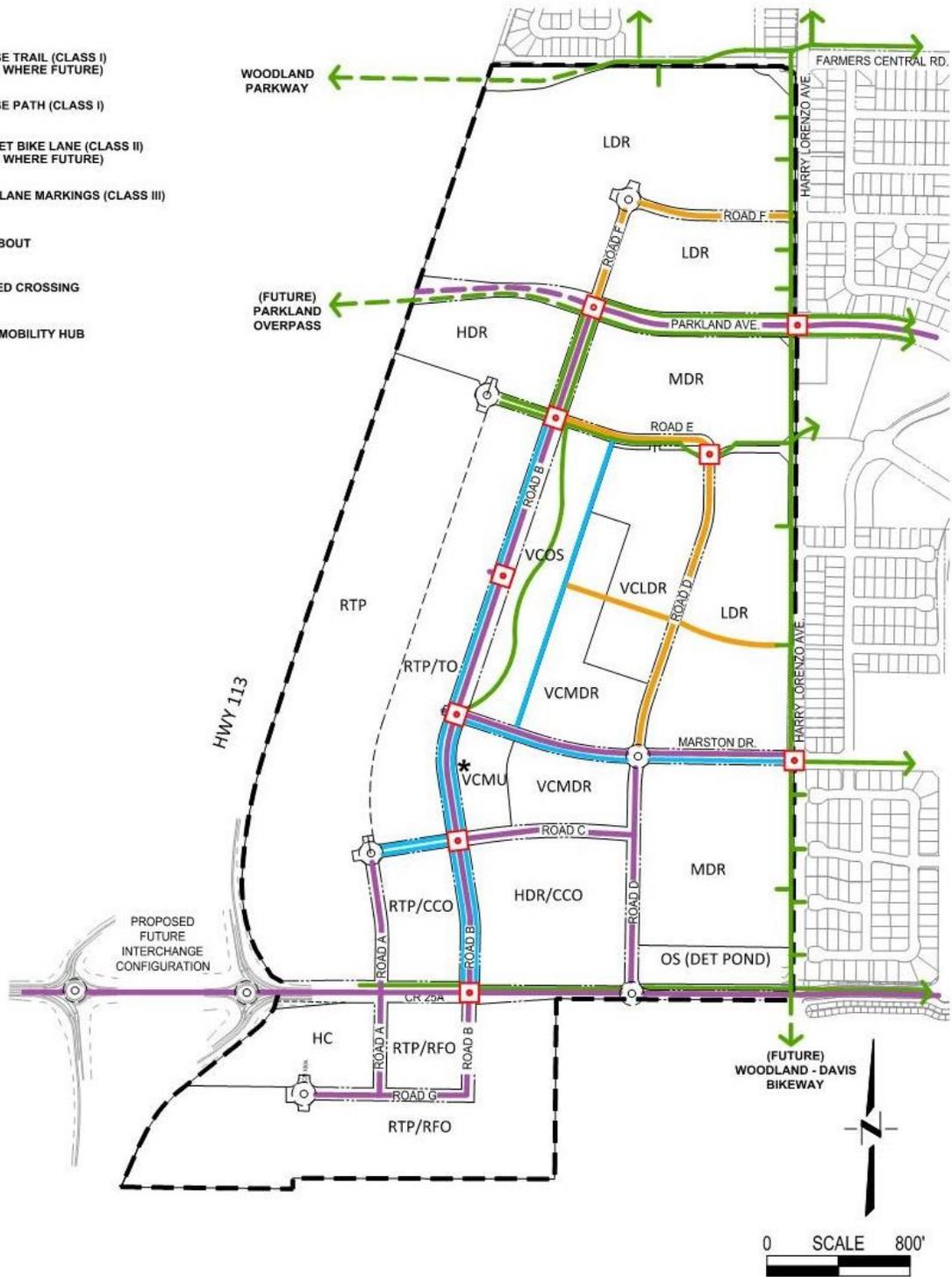


EXHIBIT 4-2: ACTIVE TRANSPORTATION NETWORK

4.4.1 Active Transportation Facility Design

The design of the Plan Area’s active transportation facilities have been carefully considered to relate to the street and land use context, reinforcing biking, walking, and other alternative transportation choices as an integral feature of the Plan. The following design standards and section details describe the characteristics of the network’s hierarchy of trails, paths, lanes and shared lane markings.

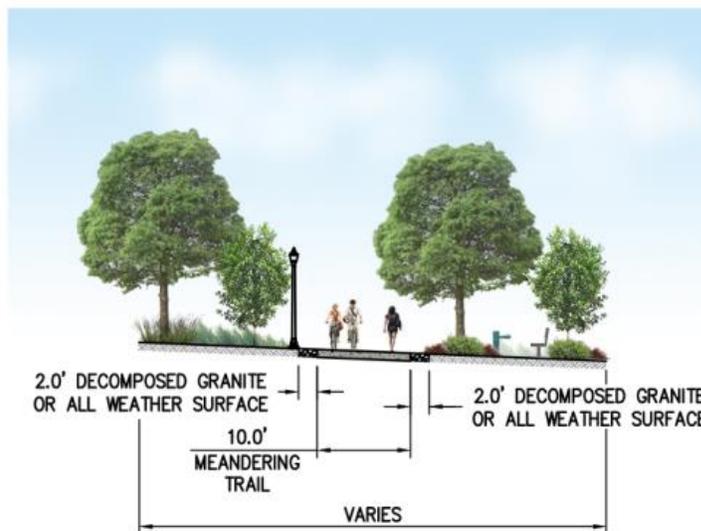


The Plan Area’s active transportation network includes 3.1 miles of multi-use trails and greenbelts, allowing residents and employees the ability to travel throughout the plan area comfortably on a bike, walking, or by way of other non-vehicular modes of transportation.

1. MULTI-USE TRAILS

The Plan Area’s greenbelt and linear greenway network consists of 3.1 miles of class 1 Multi-Use Trails. These facilities are more informal in nature with a 10-foot wide shared use path that gently meanders through landscaped open spaces. The overall width of the greenbelts and greenways shall vary to reinforce the more informal nature of these spaces. The larger greenbelts are at least 50-feet in width and shall be no less than 24-feet at neighborhood connectors or where located along the street right-of-way. The Multi-Use trails include a 2-foot gravel shoulder or low landscape edge with bi-level motion sensing LED path lighting. Trails will be well shaded with trees and have amenities such as exercise stations, benches, drinking fountains and occasional open turf areas to encourage passive and active recreational use.

The overall width of the greenbelts and greenways shall vary to reinforce the more informal nature of these spaces. The larger greenbelts are at least 50-feet in width and shall be no less than 24-feet at neighborhood connectors or where located along the street right-of-way. The Multi-Use trails include a 2-foot gravel shoulder or low landscape edge with bi-level motion sensing LED path lighting. Trails will be well shaded with trees and have amenities such as exercise stations, benches, drinking fountains and occasional open turf areas to encourage passive and active recreational use.



TYPICAL GREENWAY WITH MULTI-USE TRAIL

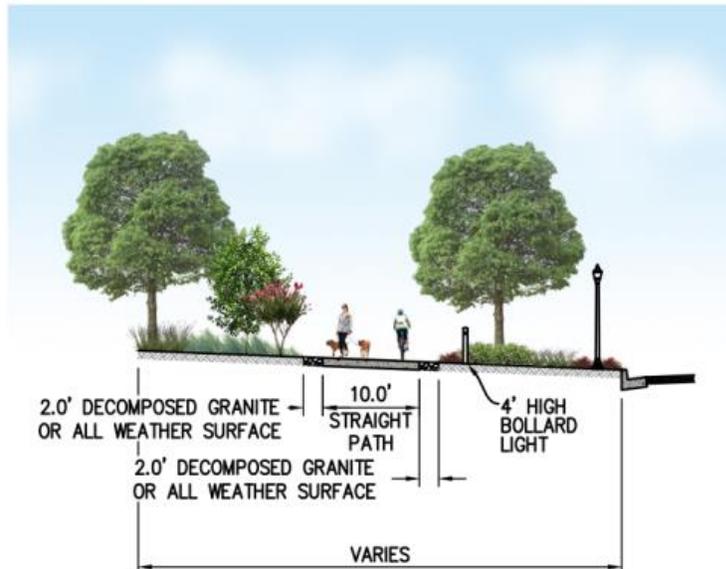
2. MULTI-USE PATHS



A more formal and linear network of multi-use paths through the Plan Area's active urban areas anticipate higher utilization by bikes, scooters and pedestrians.

The Plan Area's greenbelt and linear greenway network transitions to a more formal path facility within the more active urban areas of the plan. The 1.8 miles of Multi-Use Paths are a linear 10-foot wide shared use path that anticipates a higher utilization by all modes including bikes, scooters, pedestrians, and other forms of active transportation. A 2-foot clear recovery zone shall be maintained on both sides of the path. Where adjacent to private use, the recovery zone may be

incorporated within the front setback and may consist of a clear pedestrian zone, free of street furniture or other fixtures, or low landscaping. Lighting will be integrated with the street lighting system and supplemented with pathway or bollard lighting where needed. Bike parking, scooter docs, and transit shelters will be strategically located along the Paths to support utilization of alternative transportation modes.



TYPICAL GREENWAY WITH MULTI-USE PATH

3. THE PROMENADE

The 20-foot wide, tree-lined Multi-Use Path that runs along the east edge of The Yard is called The Promenade. This Path will serve as a connection for bikes and pedestrians between Road E and Marston Drive and as a drivable path for emergency vehicles if needed. Medium and low density residential units in the Village Center will have front entries facing across The Promenade into The Yard to help create an active and more urban open space. The Promenade will have textured concrete pavement, lower level pathway or bollard lighting, and more frequent placement of benches.

4. BIKE LANES

The Active Transportation Network contains 3.5 miles of class II on-street bike lanes. The minimum width of the lanes is 6-feet for designated bike lanes on collector streets. On arterial streets such as Road B, Parkland, and CR 25A where traffic speeds and volumes are higher, bike lanes are 8-feet wide and have a 2-foot buffer on the vehicle lane side. Green lane markings will be used along CR 25A and Road B to further identify the on-street bike facilities.

5. SHARED LANE MARKINGS

Shared lane markings such as sharrows will be used to accentuate the shared nature of these streets. On collector and local streets within the lower density residential neighborhoods that serve as key bike corridors, shared lane markings consistent with the best practices and standards articulated within the MUTCD will be used.



Class II and III bike facilities will be delineated by street markings using best practices from MUTCD.



4.5 ROADWAY NETWORK AND STREET HIERARCHY

The Plan Area’s roadway network is a hierarchy of Principal Arterial, Minor Arterial, Collector, and Local Streets that provide seamless connection into and out of the Plan Area. Roadways are planned to accommodate the specific travel modes prioritized relative to the land uses they serve. Each roadway is also uniquely designed to reinforce the character of each District and support the creation of a distinctive sense of place. The primary roadway network is shown in Exhibit 4-3.

Street Typology | Street typologies expand upon the functional classification and take into account street context, land use context, and travel mode prioritization. This typology ensures that the application of street standards consider a street’s relation to surrounding land uses, appropriate travel speeds, and the need to accommodate multiple travel modes and user abilities.

Street Type		Mode Priority			
		High	→		Low
Principal Arterial	Commercial 				
	Industrial 				
Minor Arterial	Residential 	----- Equal Priority -----			
	Mixed-Use 	----- Equal Priority -----			
	Commercial 	----- Equal Priority -----			
Collector	Industrial 				
	Residential 	----- Equal Priority -----			
	Mixed-Use 	----- Equal Priority -----			
	Commercial 	----- Equal Priority -----			
Local	Industrial 				
	Residential 				
	Mixed-Use 				
	Industrial 				

Equal Priority: Recognize the importance of all transportation modes.

LEGEND

SECTION	RIGHT OF WAY (FT.)	STREET CLASSIFICATION	NO. OF LANES
A-A	134'	ARTERIAL	4
B-B	78'-TBD	ARTERIAL	2
C1-C1	84'	COLLECTOR	2
C2-C2	75'	LOCAL	2
D-D	78'	ARTERIAL	2
E-E	111'	MINOR ARTERIAL	4
F-F	79'	COLLECTOR	2
G-G	113.5'	MINOR ARTERIAL	4
H-H	72'	LOCAL	2
I-I	78'	LOCAL	2
J-J	74'	LOCAL	3
K1-K1	60'	LOCAL	2
K2-K2	60'	LOCAL	2
L-L	71'	LOCAL	2
M-M	70'	LOCAL	2
N-N	116'	MINOR ARTERIAL	4
O-O	128'	MINOR ARTERIAL	4
P-P	120'	MINOR ARTERIAL	4
Q-Q	75'-108'	MINOR ARTERIAL	4
R-R	75'	MINOR ARTERIAL	4
S-S	53'	MINOR ARTERIAL	2
T-T	53'	MINOR ARTERIAL	2

-  SIGNALIZED INTERSECTION
-  ROUNDABOUT

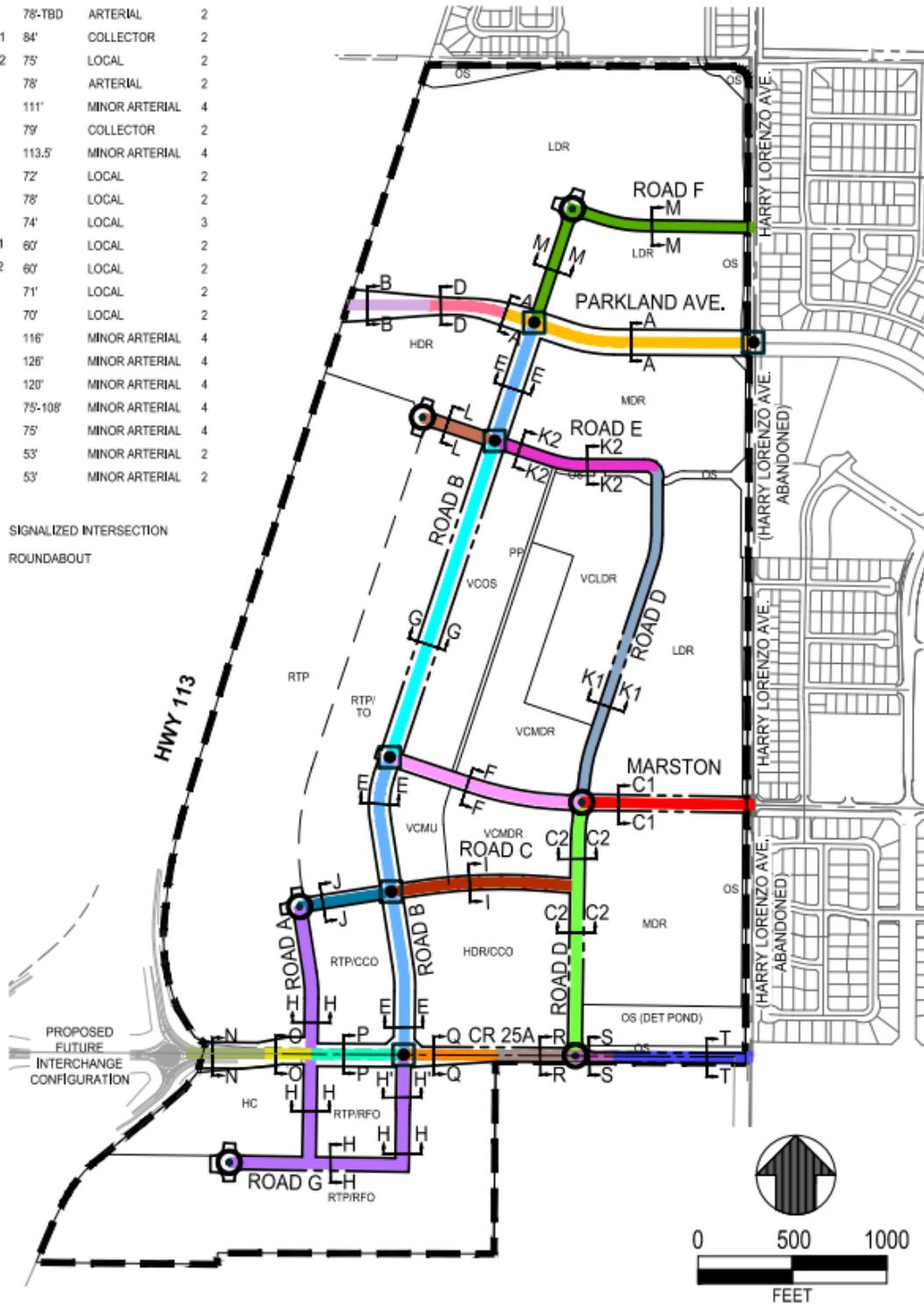


EXHIBIT 4-3: ROAD CIRCULATION DIAGRAM



4.5.1 Principal Arterial Streets

A Principal Arterial Street provides mobility for higher vehicular traffic volumes while maintaining a comfortable bike and pedestrian experience.

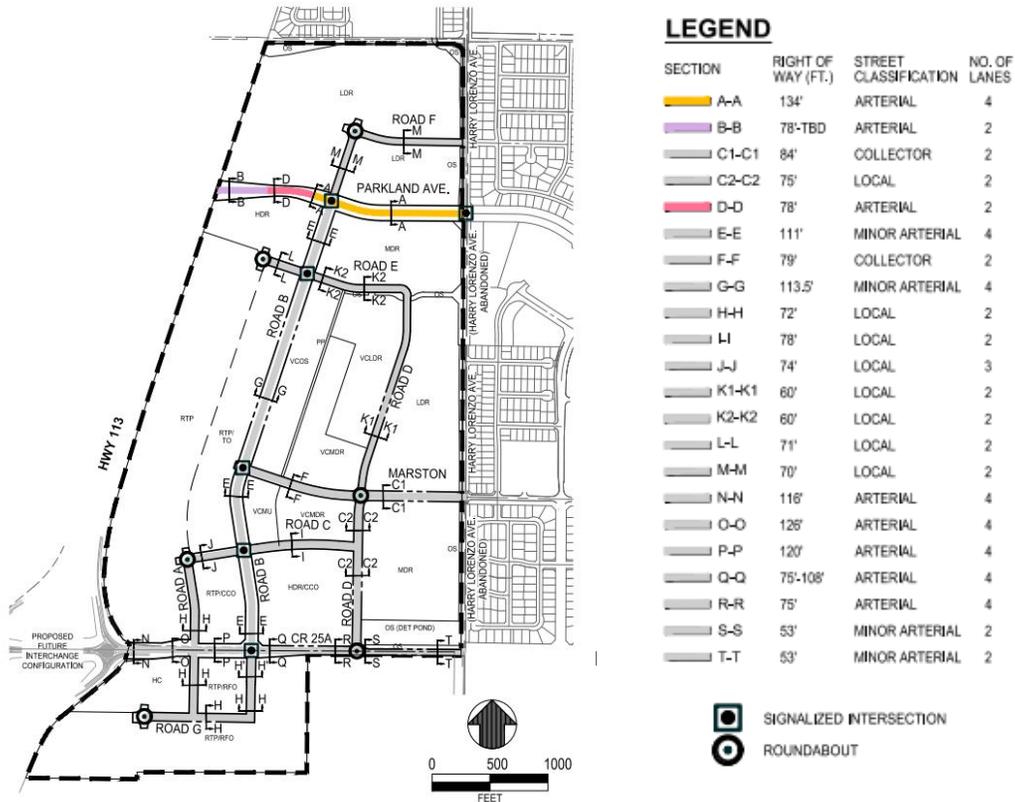


EXHIBIT 4-4: PRINCIPAL ARTERIAL STREETS KEY MAP

1. PARKLAND AVENUE

Parkland Avenue, depicted on the key map above, is the only principal arterial street within the Plan Area. This street segment is the primary connector into and out of the Plan Area from the north, intersects with Pioneer Avenue and Heritage Parkway at the Spring Lake Village Center, the hub of the arterial network in the Southeast Area.

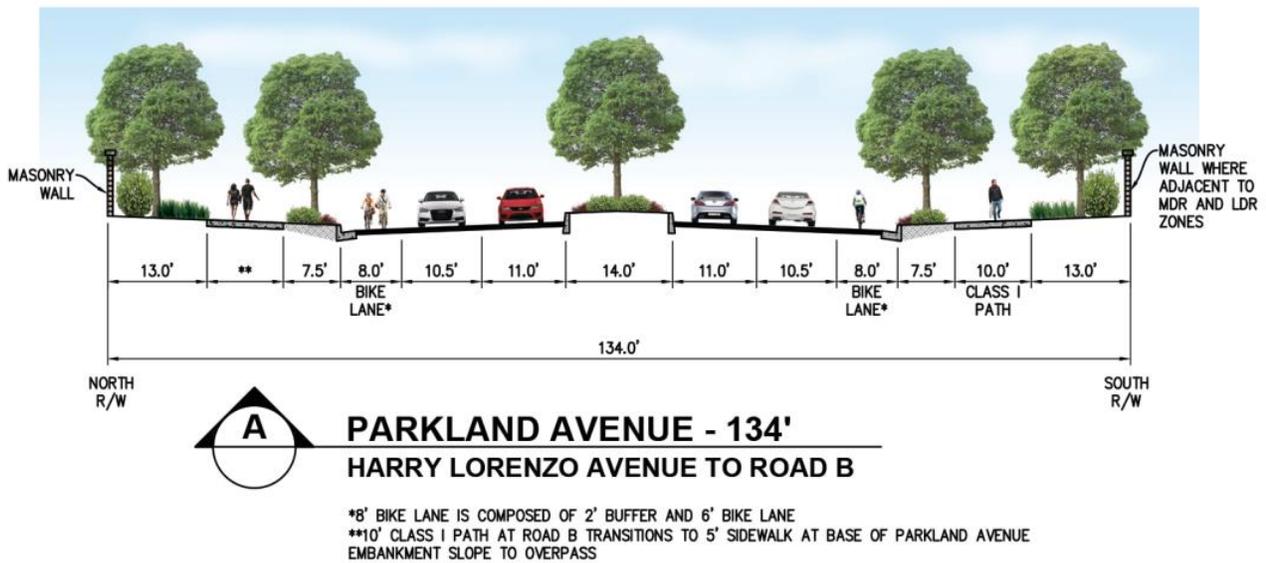
The roadway network will extend Parkland Avenue from Harry Lorenzo Avenue (HLA) to Road B where it will ultimately extend west over Highway 113 (HWY 113). It is anticipated that the segment from Road B to East Street, including the Hwy 113 overpass, will be completed with the build out of General Plan Specific Plan Area 1B, and the necessary right-of-way within the Plan Area will be dedicated as part of the Project infrastructure requirements. Parkland Avenue will continue west from HLA as a 4-lane Principal Arterial with a landscape median, buffered on-street bike lanes, Class 1 bike/pedestrian paths and landscape strips on either side of the paths as illustrated in Section A-A.



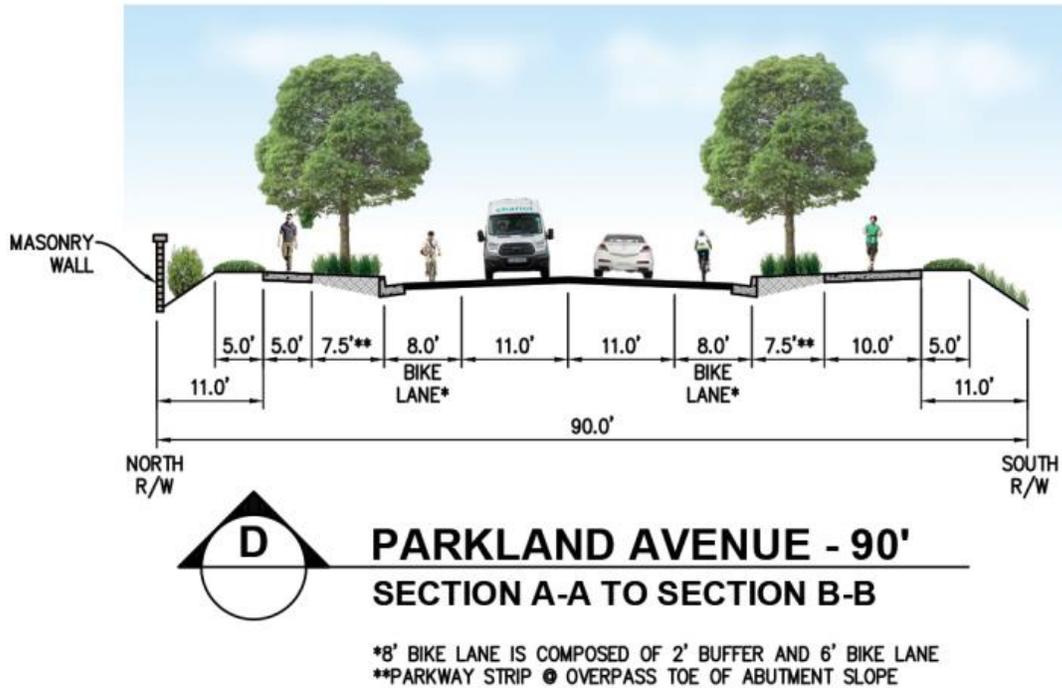
Parkland Avenue will serve a primary connector between the existing Spring Lake neighborhood to the east and the Plan Area.



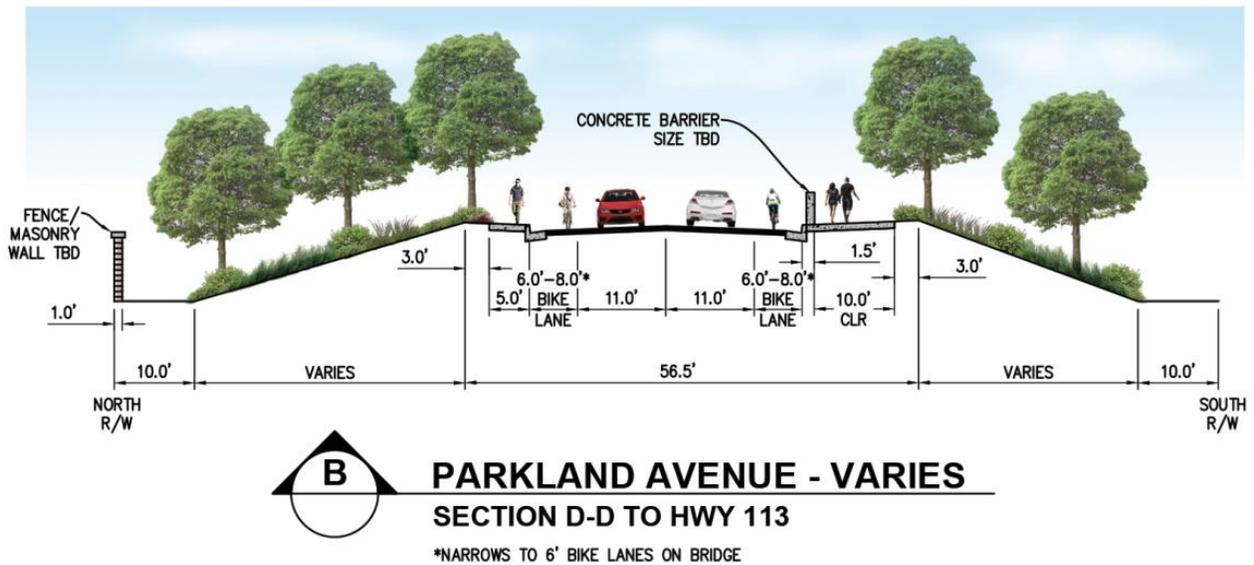
The Parkland Avenue extension will continue with on-street and Class I bike facilities between Harry Lorenzo and the future pedestrian overcrossing.



From Road B to HWY 113, Parkland will transition to an overpass, narrowing to two lanes with on-street bike lanes, planting strips and a 10-foot multi-use Class 1 bike/pedestrian path on the south side of the street. This transition is illustrated from east to west via Street Sections D-D and B-B. The western extent of Parkland Avenue and adjacent right-of-way, is designed in anticipation of future extension across HWY 113 to East Street.



The intersection of HLA and Parkland Avenue will be improved with signalized controls and enhanced bike and pedestrian facilities to provide greater safety and comfort to users of the adjacent north-south greenbelt. The signalized intersection at Road B and Parkland Avenue will be similarly improved with features to enhance safety and comfort for bicycles and pedestrians.



4.5.2 Minor Arterial Streets

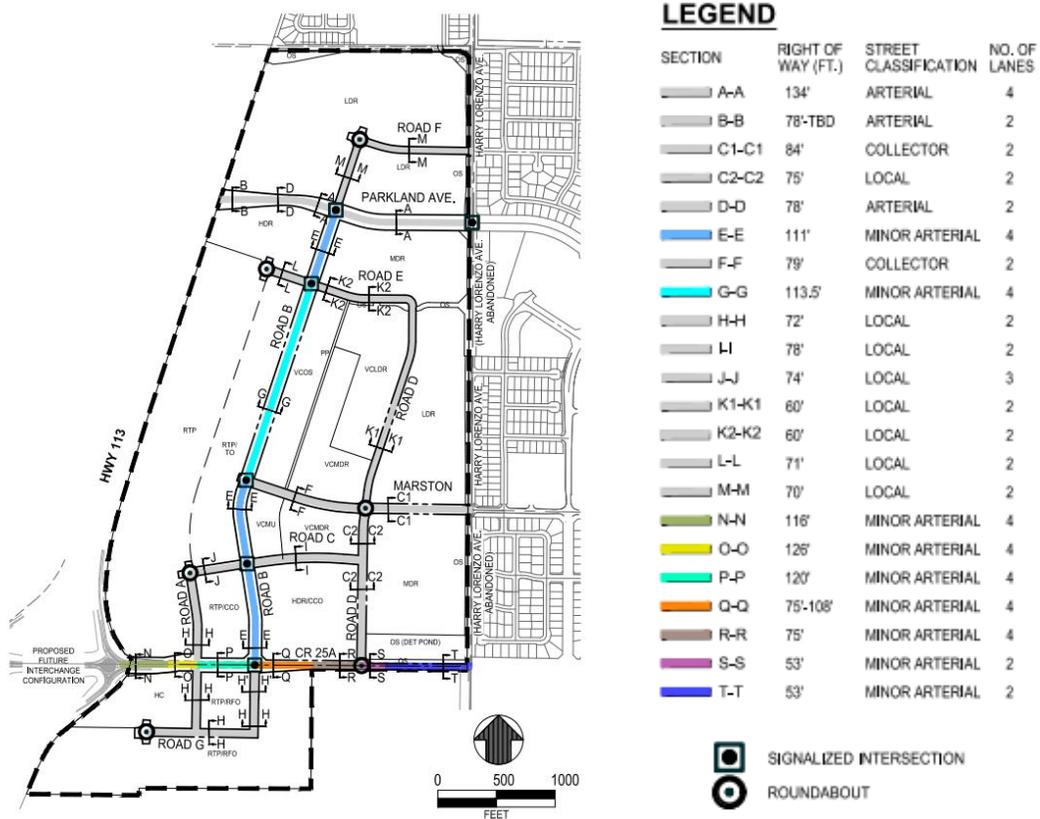


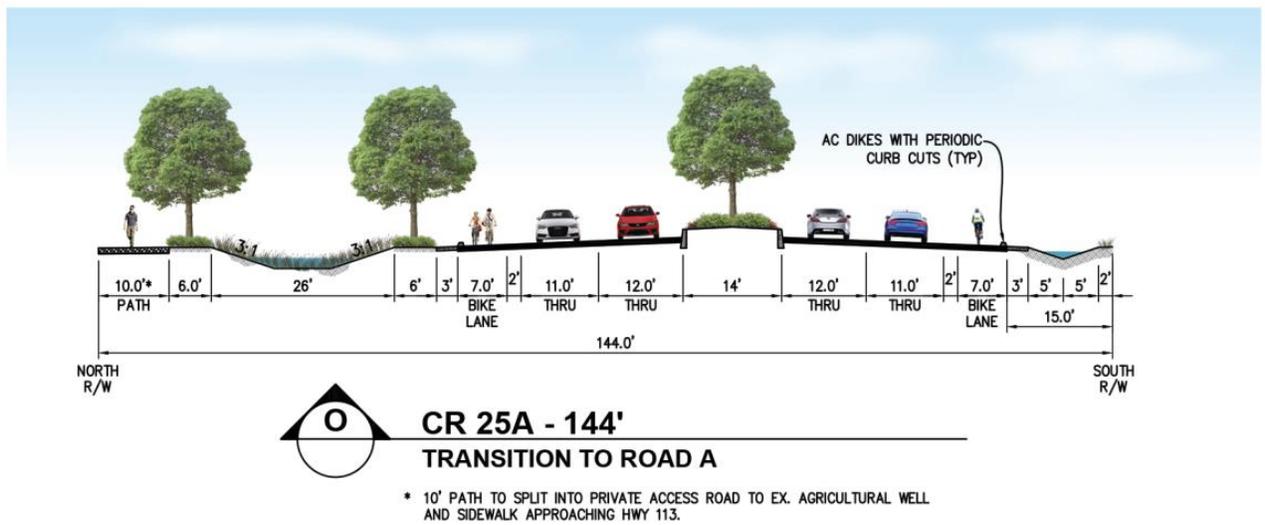
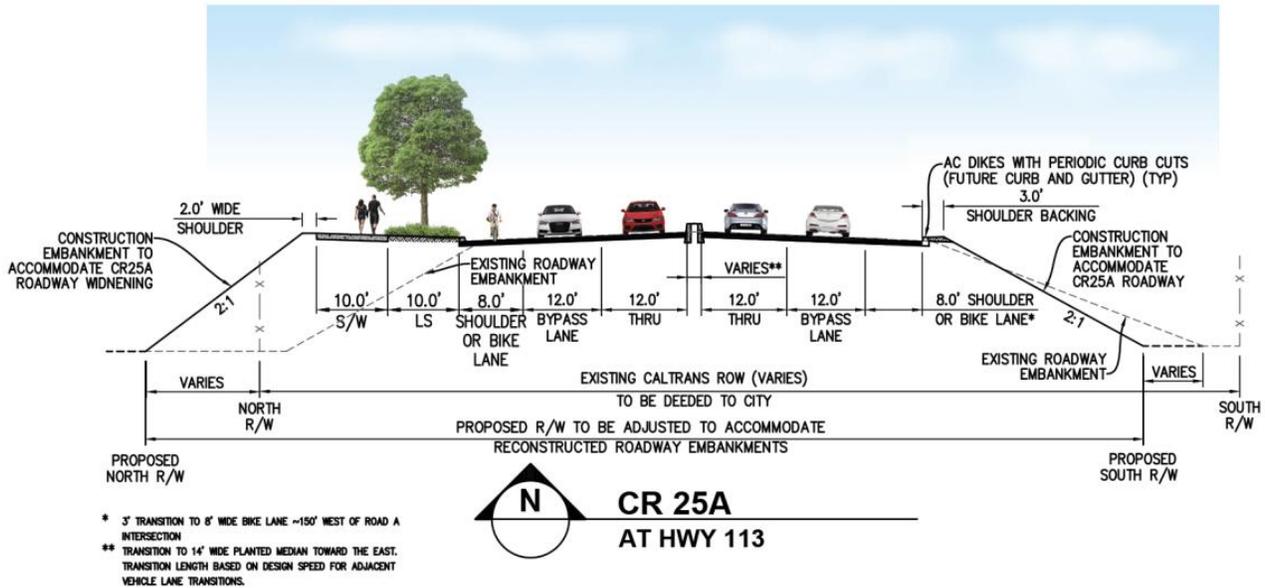
EXHIBIT 4-5: MINOR ARTERIAL STREETS KEY MAP

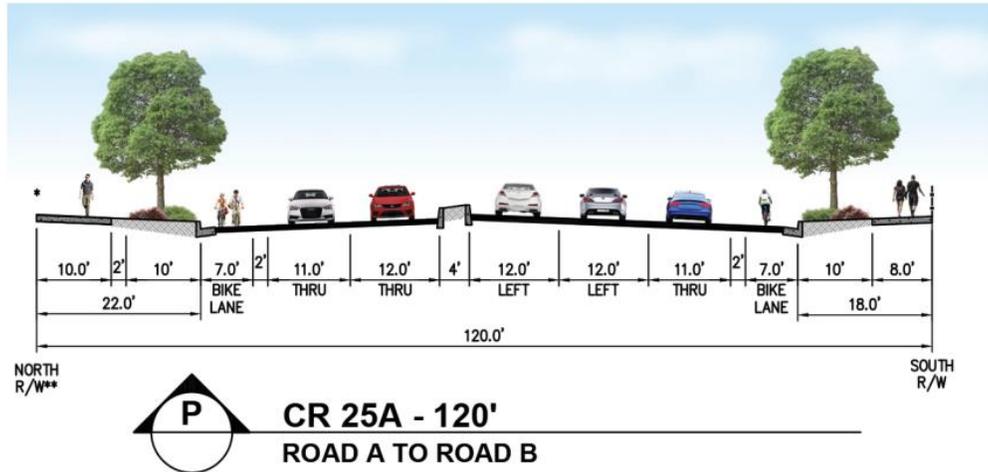
Minor Arterial Streets CR 25A and Road B, as depicted on key map below, provide mobility for higher traffic volumes than local roads, but lower traffic volumes and speeds than Principal Arterial roads. These roadways will serve as critical components of the Alternative Transportation Network, serving as key entryways and placemaking streets as well as the primary transit connections. Access from parcels onto these roadways is limited to reduce points of conflict, smooth the flow of traffic, and enhance urban design.

1. COUNTY ROAD 25A

County Road 25A (CR25A) serves a prominent role as the primary entryway into the Research and Technology Park as well as the southernmost gateway to Woodland. As depicted on Exhibit 4-3, Circulation Diagram, roundabouts are proposed on both the east and west sides of the HWY 113 / CR25A interchange to smooth the flow of traffic and create a more distinctive visual entry experience. A Class 1 Multi-Use Trail will be built on the north side of the roadway extending the existing trail in Spring Lake to the embankment approach to the HWY 113 overpass.

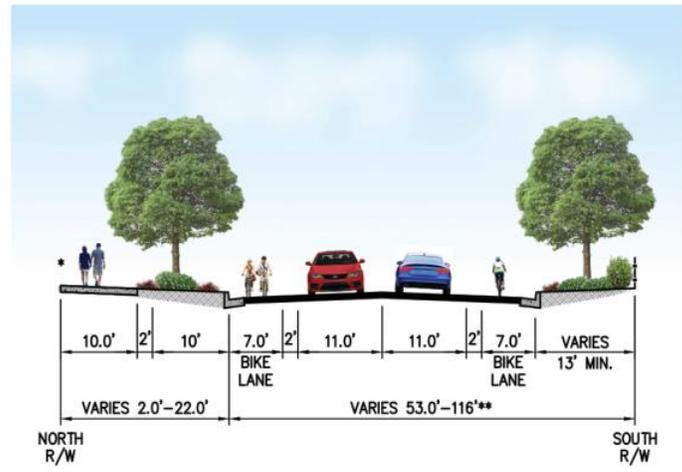
The transition from the interchange to CR25A itself will consist of four lanes of traffic with a 16' wide median as illustrated by Section N-N. Right-of-way for this transition segment is designed to add future pedestrian walkways buffered from the roadway via a tree-lined landscape strip. Between this transition and Road B, CR25A will have four lanes of traffic, turn pockets, buffered on-street bike lanes and a 16' center median island, illustrated by Sections O-O, P-P, and Q-Q. The signalized intersection of Road B and CR 25A is the main entryway into the Plan Area and will included enhanced pavement and monument features to accentuate its role as a prominent entryway.





CR 25A - 120'
ROAD A TO ROAD B

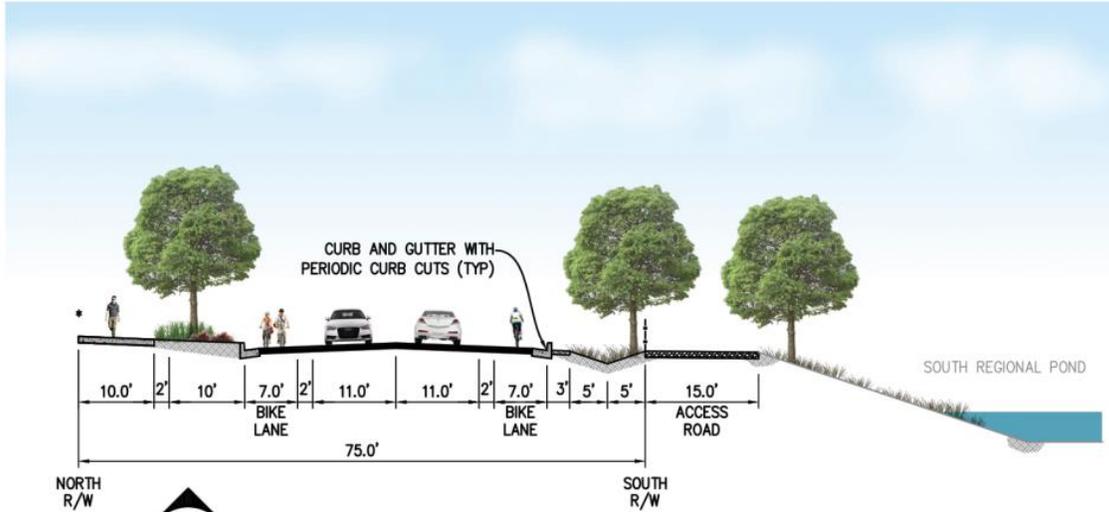
*2' CLEAR RECOVERY ZONE WILL BE REQUIRED WITHIN THE PRIVATE LANDSCAPING NORTH OF PATH
 **NO DIRECT ACCESS TO CR 25A FROM ABUTTING PROPERTY TO THE NORTH OR SOUTH



CR 25A - VARIES
ROAD B TO SOUTH REGIONAL POND - WEST

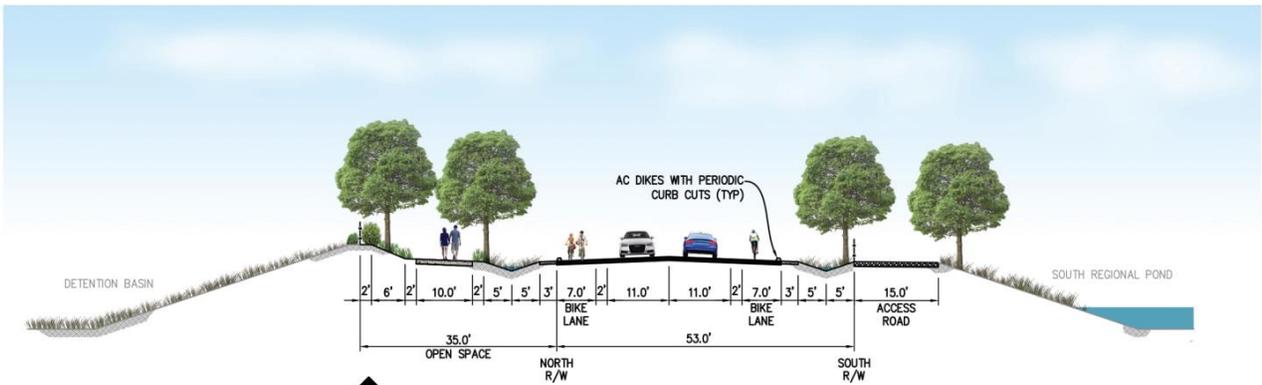
*2' CLEAR RECOVERY ZONE WILL BE REQUIRED WITHIN THE PRIVATE LANDSCAPING NORTH OF PATH
 **ROAD TO INCREASE AT INTERSECTION WITH ROAD B TO ACCOMMODATE THE FOLLOWING LANES: WESTBOUND LEFT TURN, WESTBOUND THROUGH, AND WESTBOUND THROUGH/ RIGHT. TRANSITION LENGTH TO TWO LANES TBD, ESTIMATED TO BE BETWEEN 500-1000'

East of Road B, CR 25A will taper to a two-lane road with roundabout intersection control at Road D, where it will continue east and extend to Parkland Avenue. The two-lane segment within the limits of the Plan Area are illustrated in relation to various adjacent storm water collection facilities, as illustrated by Sections R-R, S-S, and T-T below. The Plan Area infrastructure financing plan will include the completion of CR 25A from Hwy 113 to CR 102.

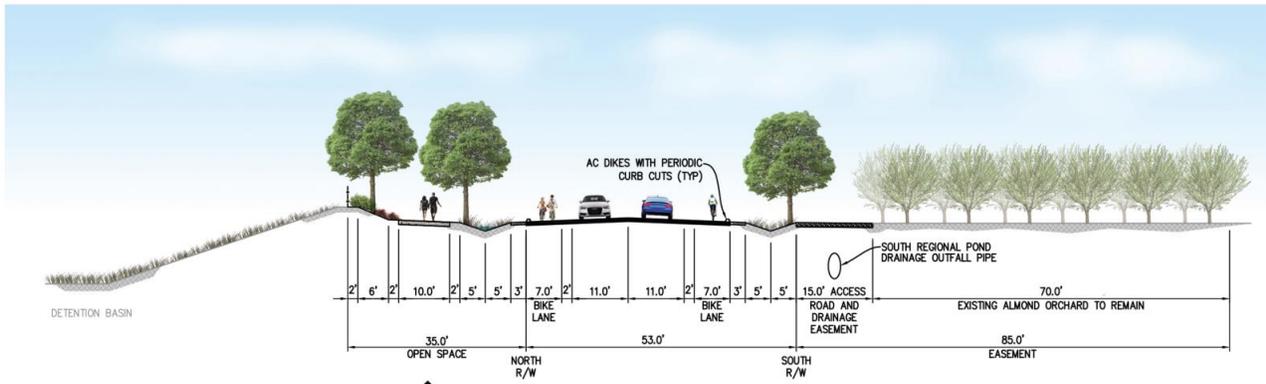


CR 25A - 75'
SOUTH REGIONAL POND - WEST TO ROAD D

*2' CLEAR RECOVERY ZONE WILL BE REQUIRED WITHIN THE PRIVATE LANDSCAPING NORTH OF PATH



CR 25A - 53' (AND 35' OPEN SPACE)
ROAD D TO SOUTH REGIONAL POND - EAST



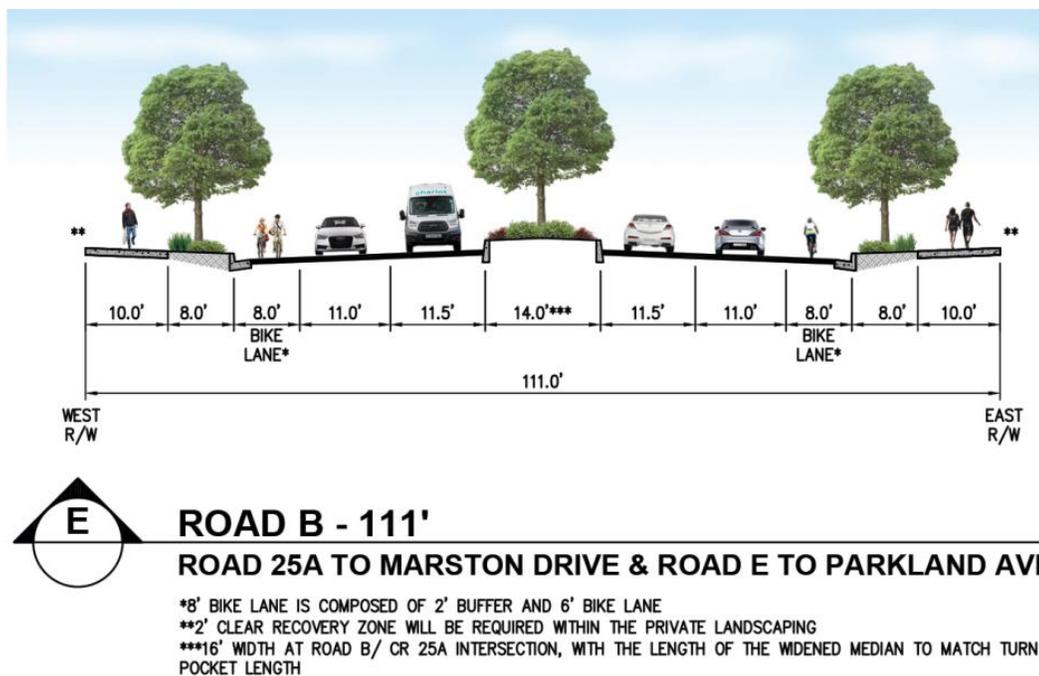
CR 25A - 53' (AND 35' OPEN SPACE)
SOUTH REGIONAL POND - EAST TO HARRY LORENZO AVENUE

2. ROAD B

Road B is a Minor Arterial street that functions as the main spine through the Plan Area linking employment, commercial, residential, and recreational uses. As the most prominent street in the Plan Area, the design and overall experience of Road B for all users is to be distinctive and reinforce, rather than detract from, the sense of place.



All intersections along this roadway are signalized to ensure efficient flow of traffic but also support safe and convenient travel of alternate modes. Enhanced pavement, landscape and aesthetic features will be provided at each of the intersections.



CR 25A to Road C. Beginning at the intersection of CR 25A, the primary entryway to the Plan Area, Road B's character is that of a shared use roadway with two vehicle lanes in each direction, buffered on-street bike lanes, and off-street Class 1 Multi Use Paths. A generous 14-foot wide tree lined median extends the length of Road B complemented by 8-foot wide tree lined landscape parkways on both sides of the street separating the roadway from the multi-use path. Driveway access is limited to no more than one right-in and right-out driveways on each side of the road within the block. Left turn access is not allowed between street intersections. See Section E-E.

Road C to Marston Drive. The character of Road B between Road C and Marston is similar to the segment to the south but transitions to a more urban character on the east side adjacent to the Village Center. See Section E-E. The shared mobility hub, The Union, is integrated into the 8' wide planter strip zone and provides accommodations for bus transit and ride hailing services. The multi-use path changes in pavement design and is signed to slow bicycle speeds as it passes through The Union. The path merges with the on-street bike lane north of The Union where the sidewalk zone transitions to a pedestrian only zone with outdoor dining and other enhanced streetscape features.

The intersection of Road B and Marston Drive is the heart of the Plan Area plan where the Village Center, The Yard, and the North Campus District meet. Special emphasis on the design of this intersection will be made and will include enhanced pavement treatments to highlight the bike and pedestrian crossings, lighting, landscaping, and architectural/public art features. Driveway access along Road B, from Road C to Marston Drive, is limited to a single right-in and right-out driveway in the southbound direction and no driveways are allowed in the northbound direction.

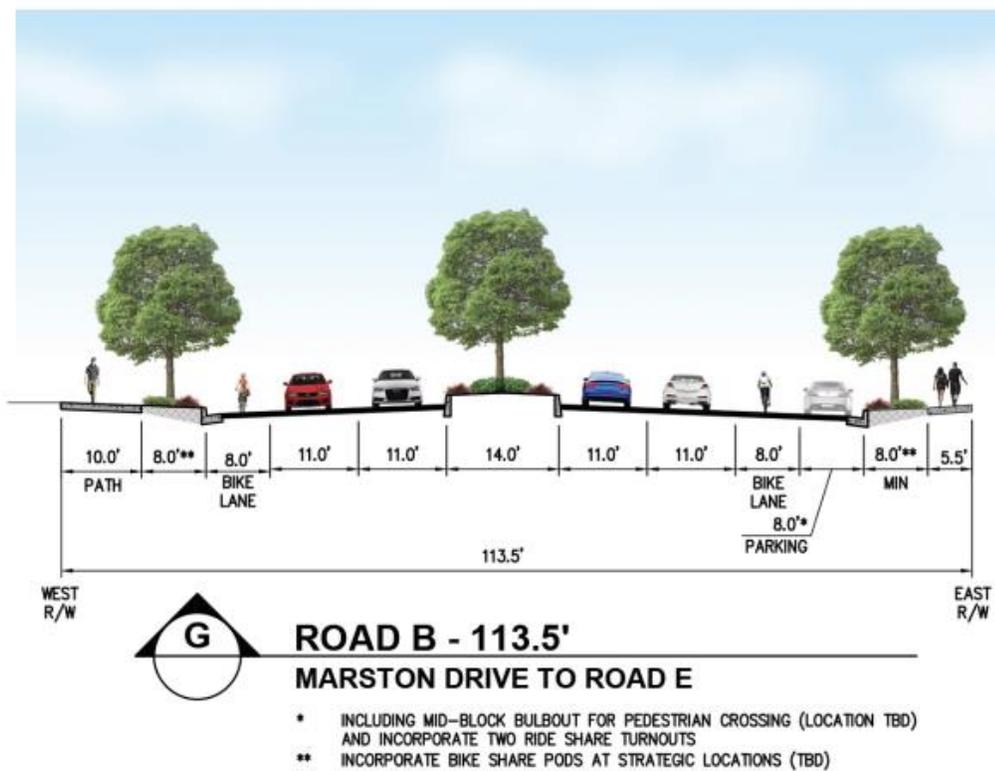
Marston Drive to Road E. This segment of Road B spans the entire west edge of The Yard and the park facing frontage of the North Campus District. The character of this segment of Road B is significant as it functions as a visual extension of The Yard while maintaining its role as the primary north-south road through the Plan Area. The roadway consists of two vehicle lanes in each direction, buffered on-street bike lanes, and on street parking on the east side. An off-street Class 1 Multi Use Path along the frontage of the North Campus District offers convenient bike and pedestrian access for employees. A 5'6" sidewalk provides pedestrian access along the west side of The Yard which is buffered by 24 feet of bike lane,



An enhanced intersection and mid-block crossings along Road B will offer safer pedestrian access to and from The Yard, Village Center and Tech Park campus.

parking and planter strip. Further enhancing the corridor and visually extending the park is a generous 14-foot wide tree lined median. See Section G-G.

An enhanced intersection is planned midway along this segment of Road B to provide access to the North Campus District and offer controlled access across the roadway for bikes and pedestrians. Additional pedestrian and bike only crossings will be integrated between Marston and Road E to offer safe and convenient access between the North Campus District and The Yard. Driveway access for the Plan Area uses adjacent to this segment of Road B are discouraged and shall be consolidated where needed and restricted to right-in and right-out movements only.



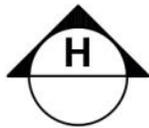
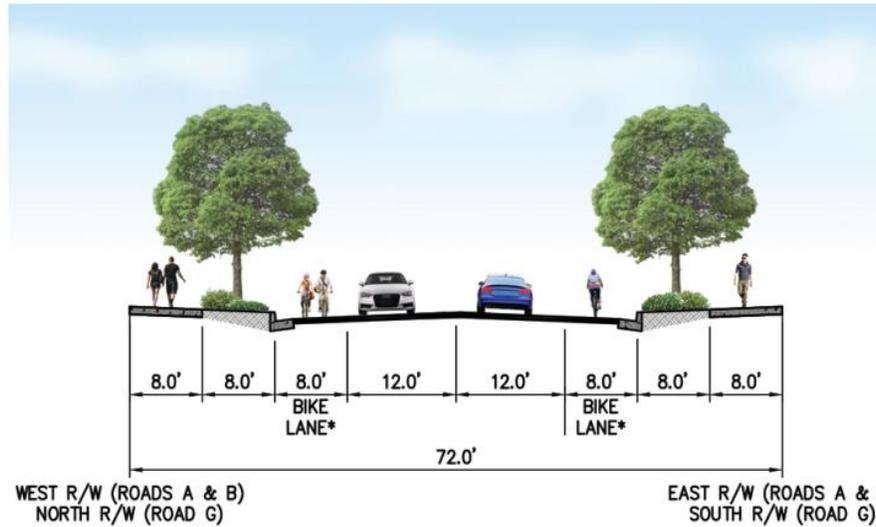
Road E to Parkland Avenue. The segment of Road B from Road E to Parkland Avenue will serve as the primary vehicular access point into the Plan Area from the north. The intersection of Road B and Parkland will be enhanced with pavement markings and gateway features to enhance bike/pedestrian safety and highlight this location as a point of entry to the Plan Area. To minimize conflicts, no driveways are allowed on this road segment.

The roadway consists of a 4-lane road with on-street buffered bike lanes, similar to Road B between CR25A and Marston (See Section E-E on page 4-22). A Class I Multi-Use Trail connects the trails on Parkland Avenue to Class I facilities that provide access to heavily used destinations of the North



Campus District and The Yard. The design of the roadway is enhanced with a 14-foot wide tree lined median complimented by 8-foot wide tree lined landscape parkways on both sides of the street.

CR 25 A to Road G. South of CR 25A, Road B transitions to a Collector Street with two travel lanes and buffered on-street bike lanes. The initial 250 (+/-) feet south of CR25A will include a 4' wide median and 12' northbound turn pocket at the entry. This segment of Road B serves as the primary entry into the South Campus District where it provides access to Highway commercial and lower intensity research park flex uses. See Section H-H.



ROAD A, ROAD B & ROAD G - 72'

SECTION H'- H' (ROAD B, SOUTH OF CR 25A) IS COMPOSED OF THE FEATURES IN SECTION H-H PLUS A 4' WIDE MEDIAN AND A 12' WIDE NORTHBOUND LEFT TURN POCKET FROM CR 25A EXTENDING 250' SOUTH OF THE INTERSECTION - 76'

*** *8' BIKE LANE IS COMPOSED OF 2' BUFFER AND 6' BIKE LANE

4.5.3 Collector Streets

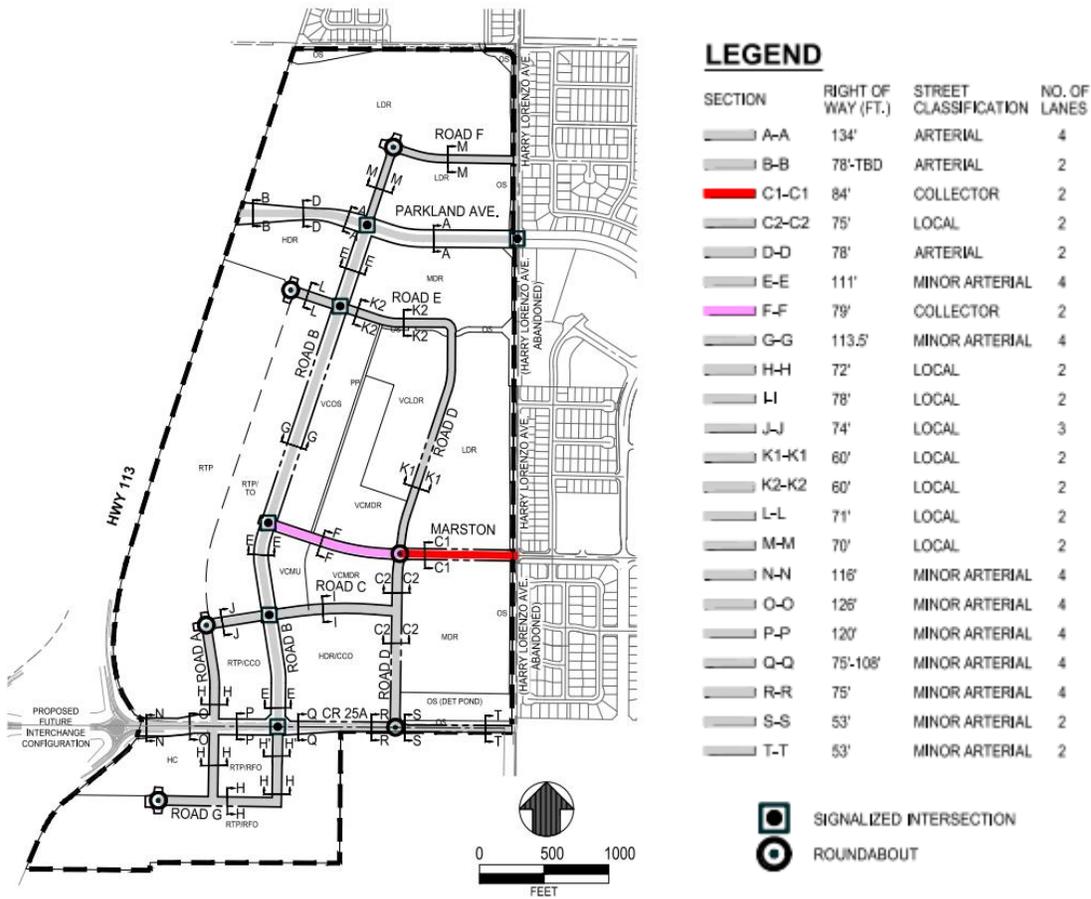


EXHIBIT 4-6: COLLECTOR STREETS KEY MAP

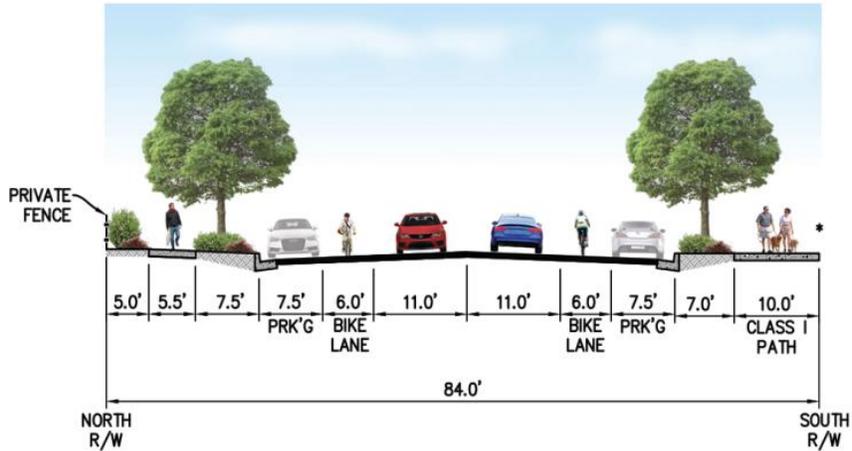
Collector Streets (Marston Drive and Roads A, C, D, E, F, G and B south of CR 25A) provide for relatively short distance travel between and within neighborhoods, and have lower speeds and traffic volumes than arterials. Driveway access to collectors is limited less than on arterials but may still be discouraged on certain segments to limit circulation conflicts. Street design and character of each of the Collector Streets in the Plan Area are described in more detail below.

1. MARSTON DRIVE

Marston Drive connects the Village Center of the Plan Area to Spring Lake neighborhoods and provides convenient access for residents of both areas to employment, retail, services, parks and open spaces. Marston Drive transitions in character from that of a residential street in Spring Lake to an urban mixed-use street at the core of the Plan Area. The segment of Marston Drive from Harry Lorenzo Avenue to Road B includes two travel lanes, parking lanes on each side of the roadway, planting strips, a 10-foot Class 1 Multi-Use trail on the south side and a 5½-foot sidewalk on the

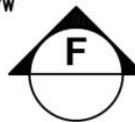
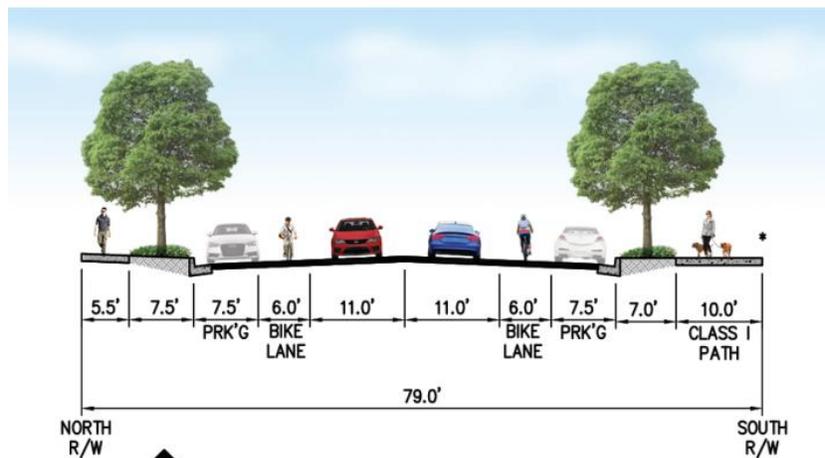


north (Section C1-C1). A roundabout at the intersection of Road D and other traffic calming measures will be utilized to keep speeds low while allowing smooth circulation. Enhanced paving and pedestrian safety features will be installed at the greenbelt crossing at the east edge of the Specific Plan to slow traffic and provide for ease of bike and pedestrian connection across Marston Drive. Driveways for single family will not be permitted and all other driveway access points shall be consolidated to minimize ingress and egress conflicts.



MARSTON DRIVE - 84'
HARRY LORENZO AVENUE TO ROAD D

*2' CLEAR RECOVERY ZONE WILL BE REQUIRED WITHIN THE PRIVATE LANDSCAPING SOUTH OF PATH



MARSTON DRIVE - 79'
ROAD B TO ROAD D

*2' CLEAR RECOVERY ZONE WILL BE REQUIRED WITHIN THE PRIVATE LANDSCAPING SOUTH OF PATH

Marston Drive at Road B. The intersection of Marston Drive and Road B is the central intersection of the Plan Area and is the primary point of access to the core of the North Campus District. Careful attention to the design of this intersection will be taken to ensure it enhances and does not detract from the sense of place of the Village Center. (See description of intersection features in Minor Arterial - Road B section above)

4.5.4 Local Streets

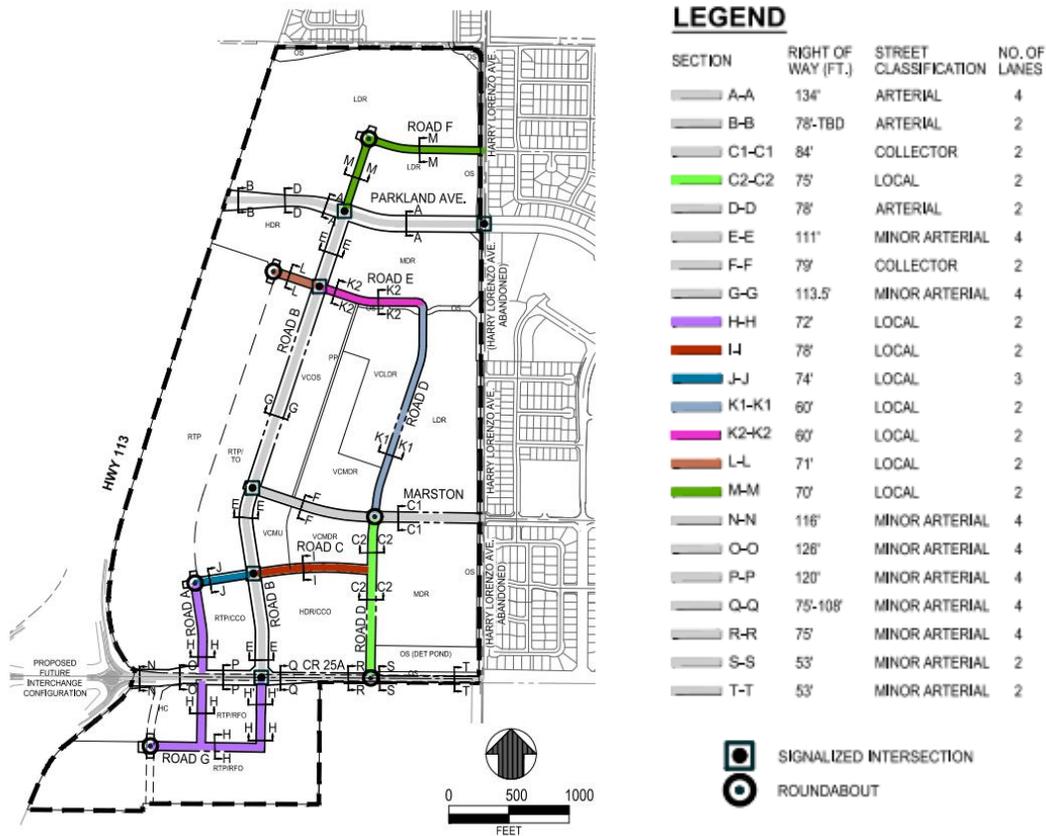


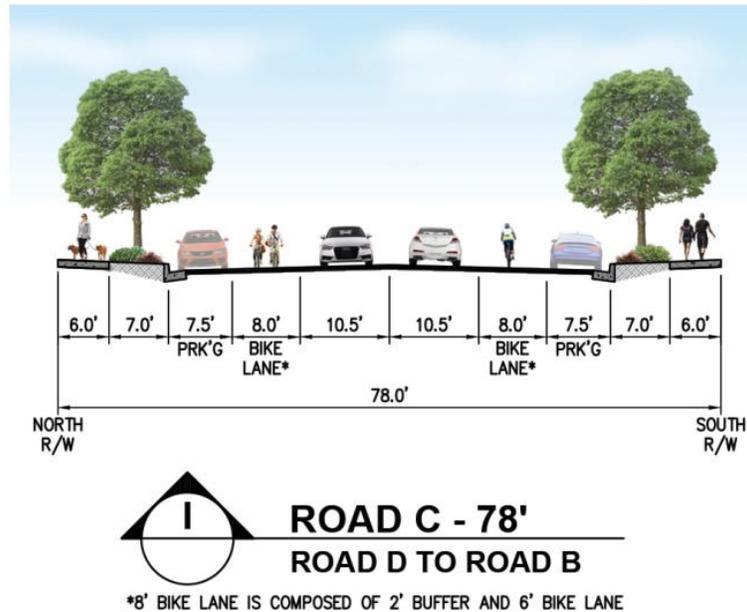
EXHIBIT 4-7: LOCAL STREETS KEY MAP

1. ROAD A

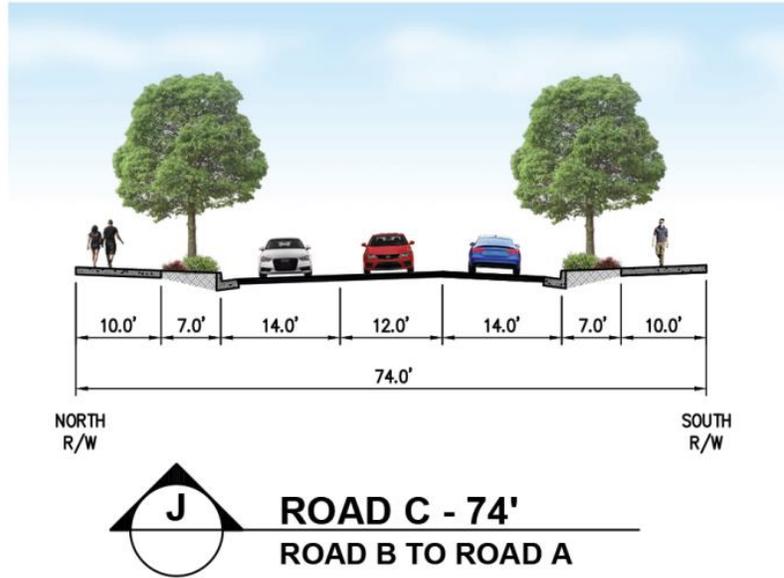
Road A is a local road providing access to the interchange adjacent to portions of the North and South Campus Districts of the Technology Park. North of CR 25A, Road A is designed as a 2-lane street with buffered bike lanes, planting strips, and widened sidewalks (Section H-H on page 4-25). A roundabout is proposed at the intersection of Road C and access is restricted to right-in and right-out turning movements where Road A connects with CR 25A.

2. ROAD C

Road C is a local street that parallels Marston Drive and CR 25A and provides an alternate east-west access between the higher density residential and the Plan Area uses. Between Roads D and B, Road C is designed as a narrower two lane street with on-street parking, bike lanes, planting strips, and 6'-0" wide sidewalks. Multiple driveway access points will be allowed to serve the mixed commercial and residential uses to minimize the need for driveways on the surrounding arterial roadways.

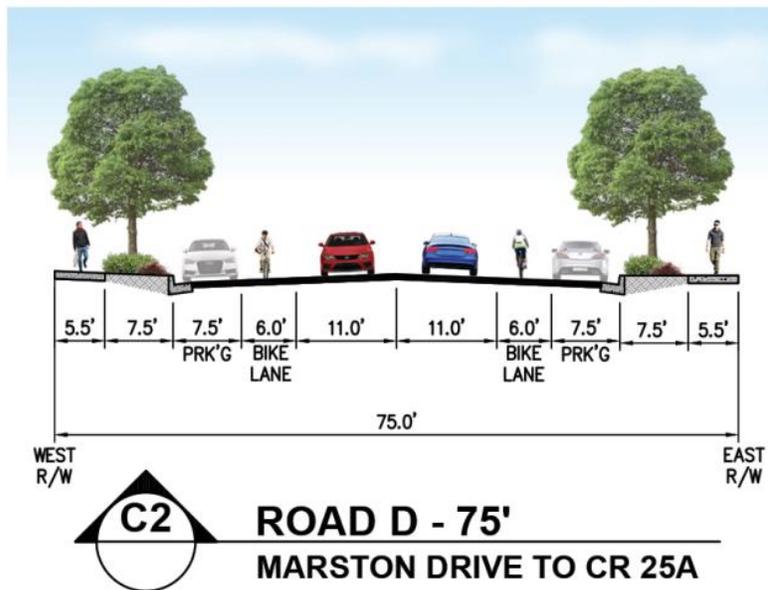


The segment of Road C between Roads A and B provides connection to Plan Area uses in the southern area of the North Campus District. The roadway consists of three travel lanes, two westbound and one eastbound. Due to a center median at CR25A and Road A, prohibiting left turns onto Road A from CR25A, the majority of inbound trips will access the North Campus Plan Area via Road B to westbound Road C; hence the two lane westbound Road C design. See Section J-J below. Full access driveways are permitted but restricted to the mid-section of this segment of Road C to avoid intersections conflicts. Planting strips provide street tree canopy and separation for the Class 1 multi-use paths on both sides of the street. A roundabout is proposed at the intersection of Road A to smooth the flow of traffic.

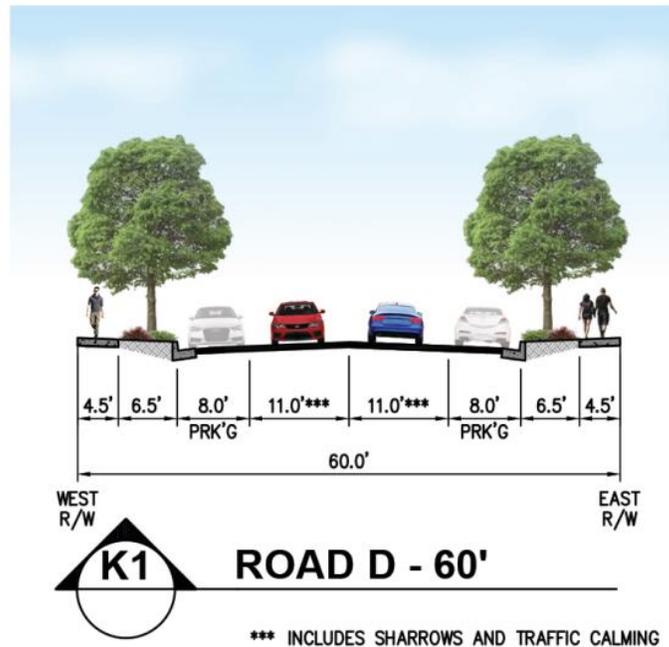


3. ROAD D

Road D parallels Road B through the majority of the Plan Area and is a collector road serving the Village Center District and East Village Districts south of Parkland Avenue. Between CR25A and Marston Drive, Road D is designed as a two lane street with on-street parking, bike lanes, planting strips, and 5'-6" wide sidewalks (Section C2-C2). Multiple driveway access points will be allowed to serve the higher density residential uses and minimize the need for driveways on the surrounding arterial roadways.

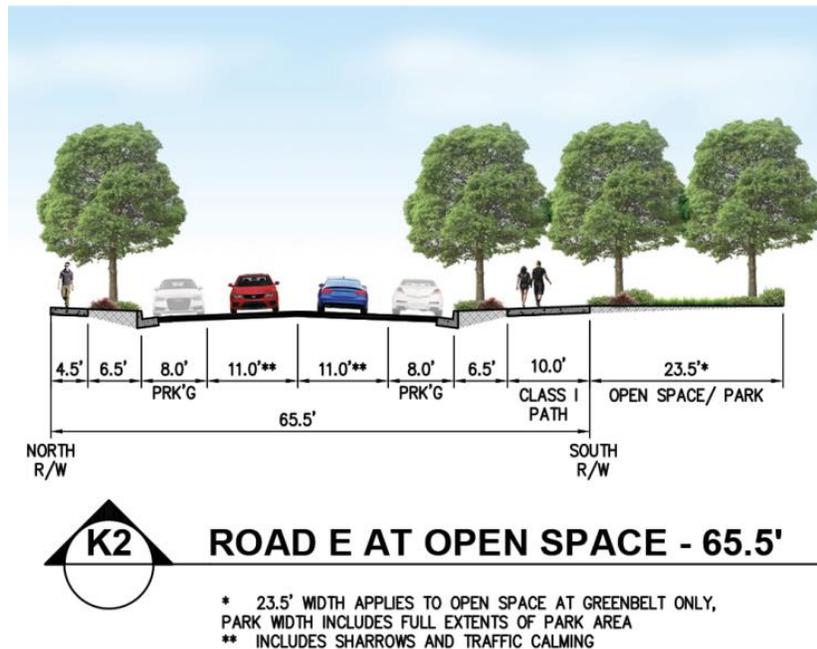


North of the roundabout at Marston Drive, Road D narrows as it transitions into the lower density residential neighborhoods of the Village Center and East Village Districts. The roadway has two travel lanes with on-street parking and detached 4'-6" wide sidewalks. A 6'-6" wide planting strip will provide for a tree lined street that offers generous shade canopy. See Section K1-K1. Traffic calming measures will be integrated into the roadway to keep speeds lower and shared lane markings will be installed to accentuate the shared use nature of the road. Enhanced pavement design and pedestrian safety features will be integrated at the north end of Road D where the multi-use trail crosses. Single-family home driveways accessing Road D are discouraged.

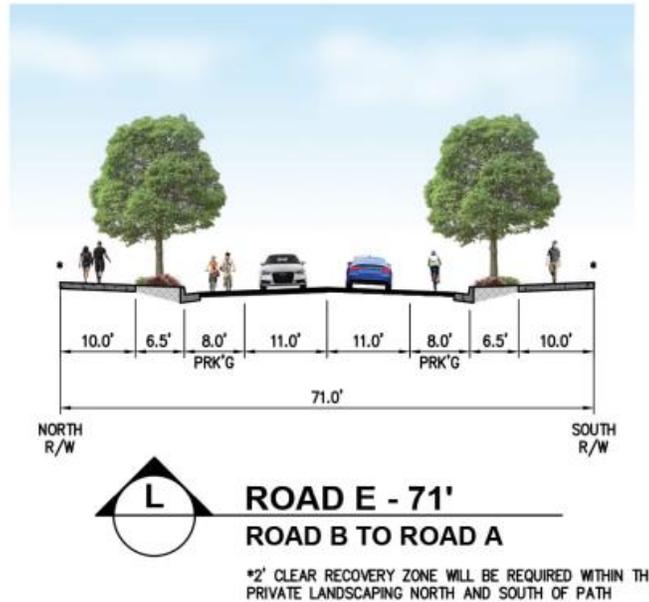


4. ROAD E

Road E runs east-west along the north end of The Yard and connects the Village Center Residential neighborhoods to Road B. Between Road D and Road B, the street consists of a two lane street with on-street parking. A planter strip separates a 4'-6" wide sidewalk on the north and a 10' multi-use trail on the south side of the roadway. Single-family home driveways are prohibited on the south side of the road. Access to the medium density residential uses and/or school site on the north side shall be consolidated to reduce the driveways or roadways connecting into this segment of Road E (Section K2-K2).

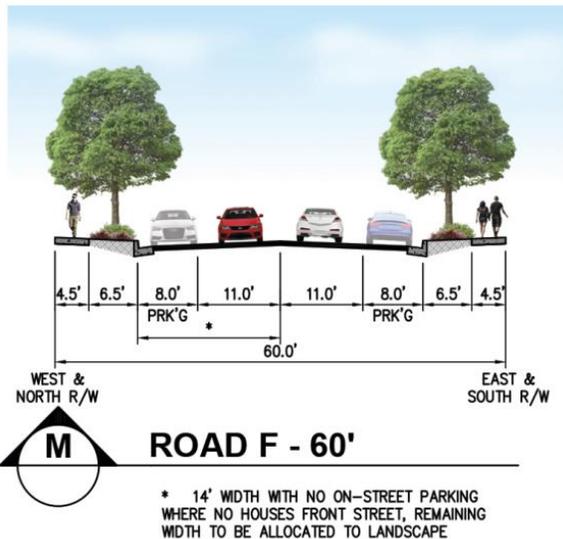


West of Road B, Road E transitions to a more urban mixed character street that serves both high density residential and Plan Area uses. The roadway has two travel lanes with on-street parking and 10 foot wide multi-use paths on both sides of the roadway. A 6'-6" foot wide planting strip will provide for a tree lined street that offers generous shade canopy. See Section L-L. A roundabout at the end of Road E provides access to both the residential and Plan Area uses.



5. ROAD F

Road F is the primary access for the residential neighborhood at the north end of the Plan Area with connections to both Parkland Avenue and Harry Lorenzo Avenue. Road F consists of a two lane street with on-street parking. A planter strip separates a 4'-6" wide sidewalk on both sides of the street (Section M-M). Single-family home driveways are discouraged. On street parking may be replaced with on-street bike lanes if lotting patterns result in no driveway access directly onto Road F. An oversized roundabout with a large specimen tree and native plantings provides a focal point of the neighborhood.



An oversized and landscaped roundabout will serve as a focal feature within the North Village similar to the roundabout located in Woodland's historic Beamer Park neighborhood shown here.

6. ROAD G

Road G is an east-west road connecting the south ends of Road A and Road B providing access to the southern most areas of the Plan Area. Road G consists of a two lane street with Class II on-street bike lanes. A planter strip separates an 8' wide sidewalk on both sides of the street (see Section H-H on page 4-25). A roundabout at the west end of Road G provides access to both the Highway Commercial and Plan Area uses and provides a character defining focal point for the South Campus District.

4.5.5 Other Local Streets

Local Streets in the Village Center residential (VCLDR and VCMDR) and all HDR residential neighborhoods will be traditional in design, have more linear geometry and include 4'6" minimum wide sidewalks separated by a 6' minimum width street tree planted parkway strip. On street parking is required on both sides of the street.



Local streets in the East and North Villages will include on-street parking, shared bike lanes, attractive landscaping and street trees that provide sidewalk shade during summer months.

Local Streets within the East Village and North Village LDR neighborhoods may be less uniform in character reflecting the nature and density of the subdivision within they are located. Where utilized in these neighborhoods, sidewalks shall be a minimum of 4'6" in width;. Parkway strips shall be a minimum of 6' in width. With the goal of reducing paved widths, flexibility shall be provided at Tentative Map implementation in regards to on-street parking, use of sidewalks, and the use of drainage swales and alternative parking surfacing in lieu of traditional curbs and gutters, so long as the City is not left with an increased maintenance burden compared to traditional development standards. Right of way cross section design is subject to review and approval by the Community Development Director.

Local Street sections in all neighborhoods shall be developed to support the character of the neighborhoods and consider the proposed house setbacks, lot widths and sizes, and overall density. All local streets shall contribute to a well landscaped, tree shaded pedestrian friendly environment. Travel lane width should be 10 feet and parking lane width shall be 7 feet for a standard curb to curb width of 34 feet. Additional width may be considered for bike lanes if deemed necessary by the City Traffic Engineer.



Local Street sections are subject to the review and approval of the Community Development Director and shall be publicly maintained.



Alternative pavement and drainage design is encouraged in the North Village to achieve a unique and rural character.

4.5.6 Alleys

Alleys or “rear lanes” serve as accessible rights-of-ways for public and private vehicles, bikes, and pedestrians. In commercial areas, alleys are primarily used for access to parking lots and service areas for businesses but may also incorporate outdoor seating and landscaping where appropriate. Alleys in both commercial and residential areas should be at least 20 feet wide and clear of obstruction between structures to allow for emergency vehicles as well as utility and waste collection vehicles. Design details of the alleys in residential neighborhoods will be defined at the time of Tentative Maps and approved by the city’s Fire Department.

Landscaping shall be integrated to visually soften the alleys and provide stormwater runoff collection. Windows and balconies on residential units should be oriented toward the alley to help add interest and eyes on the alley.



Well designed alleys are encouraged within the residential zones and in the Village Center as a means of reducing driveways and parking lots along street frontages.





4.5.7 Roundabouts and Enhanced Intersections

Roundabouts and enhanced intersection treatments are planned at key intersections throughout the Plan Area to improve intersection operation and reduce vehicle speeds. Five roundabouts are proposed in the Specific Plan in addition to three along the CR 25A corridor at Road D and the off-ramps of HWY 113. Landscaping and public art will be integrated into the roundabouts to enhance community aesthetics and strengthen sense of place.



Roundabouts at key intersections reduce traffic speeds while improving circulation. Roundabouts in the plan area will include attractive landscaping and/or public art.



Crosswalks along Marston Drive, Road B and other key intersections will include textured paving, flashing lights and/or other features to enhance pedestrian visibility.

Enhanced intersection treatments are proposed at each of the controlled intersections along Road B. Additionally, where the north-south greenbelt crosses Marston Drive and Parkland Avenue, enhanced paving and pedestrian safety features will be installed to slow traffic and provide for ease of bike and pedestrian crossing.

4.5.8 On-street Parking

To maximize utilization of land, the Plan Area development standards require limited amounts of off-street parking, increasing the likely demand for on-street parking. Street designs anticipate the need for on-street parking to support adjacent uses and reinforce the urban character of the project. Managing publicly accessible parking, particularly in the Village Center and commercial areas of the plan, will be important to ensure availability for parking for customers and transit users. Utilization of smart parking management tools such as Woodland based JAPA Parking will be a central part of a Parking Management Plan required to be prepared as part of the Plan Area Transportation Demand Management Plan.

4.5.9 Streetscape Amenities

Streetscape amenities are an essential element to creating a strong a sense of place and are a fundamental feature of the Plan Area streetscape design. The development of a Master Design Manual is required prior to approval of the first Final Map and shall incorporate design standards and construction details for the following features:

1. Gateway / Entry Features

An architectural feature(s) identifying the entryway into the Plan Area on CR 25A and/or at the intersection of Road B, as well as entry features indicating entry points into the North Campus and South Campus Districts shall be integrated into the streetscape design.



2. Public Art

Public art in the form of sculptures or other durable forms of art celebrating creativity, ingenuity, and agricultural heritage. Public art shall be integrated throughout the Plan Area streetscape in center medians, roundabouts, and/or other prominent locations as a way to strengthen community identity and a unique sense of place.

3. Street Trees and Landscape Palette

Street trees and plant palette for all landscape installed within the public right-of-way and greenbelt open spaces. Plants shall be appropriately specified to the climatic and soil conditions of the area. Particularly, use of a variety of native oak tree species as well as water-wise and pollinator friendly plants shall be detailed.





4. Stormwater Management

Streetscape design details identifying the use of bio swales and rain gardens along roadways and greenbelts to aid in the treatment and absorption of rain water. Materials and plant species appropriate to the effective functionality and long-term maintenance of these stormwater management features shall be specified.

5. Walls and Fences

The location and design of various walls and fences facing the public right-of-way. Use of walls and fences should be limited to locations where needed for sound attenuation and/or privacy. The type and style of walls/fencing shall be carefully considered to ensure visually cohesive and/or complementary streetscapes. Where walls or fences are to be maintained by the city, durable masonry materials shall be used. Landscaping shall be used to soften the visual presence of walls and fences throughout the project.



6. Street Furniture and Lighting

A design guide specifying the style, material, color, and finish for all furniture including benches, bike racks, trash receptacles, and street signs installed in public right-of-way and greenbelt spaces shall be provided. In limited application, styles of street furniture may vary and correspond to the District where they are specified to reinforce a particular neighborhood character. Similarly, street and pathway lighting shall be uniform throughout the project. Limited variation in style may be proposed to relate to the particular character of the District/neighborhood.

7. LDR and VCLDR Local Street Section Template

Examples of local street design within the LDR and VCLDR zones shall be provided including alternative storm drain design, particularly where traditional sidewalk design is modified. Further, recommendations shall be provided for sidewalk placement in relation to subdivision design and residential setbacks.





5.0 | PUBLIC UTILITIES AND SERVICES





5.1 | OVERVIEW

The goal of the Public Utilities and Services chapter is to identify the necessary public utilities and public services required to serve the Plan Area. The first part of this Chapter addresses the required public utilities of water, recycled water, wastewater, drainage/flood control as well as dry utilities of electric, gas and communications. This Chapter identifies the backbone infrastructure necessary to serve the Plan Area. Utility infrastructure will be constructed to ensure compliance with all applicable service and improvement standards, and state and federal laws and regulations.

The coordinated delivery of public services is essential to ensure a complete community, one that provides public safety, recreational, educational and library services for the residents. The Public Services Element of the General Plan provides information and policy guidance to ensure that services are sufficient to support new development in the Plan Area. Infrastructure extension and financing obligations relating to public utilities services are outlined in the Project EIR and Mitigation Monitoring Plan, development agreements and/or in Implementation Chapter 6 of the Specific Plan. Table 5.1, Utility and Service Providers, lists the entities that will serve the Plan Area.

Table 5.1: Utility and Service Providers			
Utility	Provider	Service	Provider
Water	City of Woodland	Fire Protection	City of Woodland
Recycled Water	City of Woodland	Law Enforcement	City of Woodland
Wastewater	City of Woodland	Parks and Recreation	City of Woodland
Drainage and Flood Control	City of Woodland	K-12 Schools	Woodland Joint Unified School District
Electric Service	PG & E	Post Secondary Schools	Woodland Community College
Natural Gas	PG & E	Library	City of Woodland
Communications	AT&T, WAVE	Other Services	City of Woodland

5.2 | WATER SUPPLY

The City of Woodland Public Works Department currently provides municipal water to the boundary of the WRTP. Treated Sacramento River water supplied by the Woodland-Davis Clean Water Agency's Regional Water Treatment Facility (RWTF) is the primary source of potable water. The City operates three aquifer storage & recovery wells, which store treated surface water in winter months to supplement RWTF supplied water in summer months. Groundwater is a backup

to the surface water supply and will supplement surface water during times of high demand or reduced surface water availability. The City maintains an Urban Water Management Plan (UWMP) that is updated every five years and describes the current and future water uses, sources of supply and its reliability, and existing and planned conservation measures. The 2020 UWMP was adopted by City Council in June 2021. The City also has a Groundwater Management Plan, adopted in 2010, to manage groundwater resources.

5.2.1 Existing Water Distribution System

The municipal water supply distribution system consists of 260 miles of transmission and distribution lines, a 3-million-gallon ground level storage tank, and a 400,000-gallon elevated storage tank, which is generally sufficient for peak demands and to regulate The Woodland-Davis Clean Water Agency (WDCWA) Intake Facility at the Sacramento River. Woodland has a dedicated capacity of 18-million gallons per day (mgd) of supply from the water treatment plant.

Aquifer Storage & Recovery (ASR) wells are also intended to store large quantities of treated surface water to meet peak summer demands and to prepare for future drought conditions. Three ASR wells are operational and can supply over 8 mgd of water with quality equal to WDCWA supplied water.

Currently there are six agricultural wells in use within the Plan Area: four of them are north of CR 25A and two of them are to the south. The two wells along SR 113, north of CR 25A, are to be preserved until no longer needed for irrigation of the surrounding agricultural land, including fields west of Highway 113 and south of CR 25A. These facilities will be secured and adequate setbacks will be provided. An access road will be maintained along the west boundary of the Plan Area, parallel to SR 113, allowing for routine maintenance of the wells by the owner. As development progresses, the well owner(s) shall reserve access rights to the well through the developed portion of the RTP.. The remaining wells shall remain in use to irrigate the surrounding agricultural land until development within SP-1A precludes their use or otherwise incorporated into the SP-1A layout for continued irrigation use within or outside the limits of the Plan Area.

Existing facilities in the general vicinity of the Plan Area include the following:

- 30" east-to-west water transmission main in Farmers Central Road corridor, from RWTF to Ashley Avenue.
- 12" east-to-west water distribution main in Farmers Central Road corridor, connecting to above- referenced 30" transmission pipe.
- 12" north-to-south water main in Harry Lorenzo Avenue, connecting to the above-referenced 12" pipe, from Farmers Central Road to ±500' to the south



- 12" east-to-west water main in Parkland Avenue, assumed to be extended to boundary of WRTP prior to development
- 12" east-to-west water main in Marston Drive
- 8" south-to-north water main in Harry Lorenzo Avenue, connecting to the above-referenced 12" pipe, from Marston Drive to $\pm 1300'$ to the north
- 8" east-to-west water main in CR 25A, stubbed at Harry Lorenzo Ave., assumed to be constructed prior to development.

5.2.2 Proposed Water Distribution System

Potable water will be distributed throughout the WRTP via a pressure system and routed to serve all areas within the plan area, including for irrigation. Once recycled, water from the City Water Pollution Control Facility (WPCF) (See Section 5.2 below) is connected to the WRTP recycled water pipe network, the potable water will be isolated from portions of the plan with public irrigation demands, such as parks, greenbelts, and street medians. Table 5.2 contains an estimate of water demand for the WRTP.

Proposed Water Distribution System, Exhibit 5-1, depicts the backbone infrastructure designed to connect to the existing transmission mains and distribute domestic water throughout the WRTP. The proposed points-of-connection for the system are along the existing 12" main in Harry Lorenzo Avenue: at Fowler Way, Parkland Avenue and Marston Drive. In addition, there will be a connection at the intersection of CR 25A and the former extension of Harry Lorenzo Avenue (removed in favor of a future greenbelt).

The network leading from these connections was designed in accordance with the City of Woodland Engineering Standards (COWES) to provide looping of the system, and minimum spacing of 12-inch lines at approximate one-half (1/2) mile intervals.

The public water supply pipelines are all located within the right-of-way of public streets or roads, with pipelines larger than 8 inches located in collectors and arterials. Additional pipelines were shown connecting the portion south of CR 25A back to the main network to ensure looping in this branch of the system.

Confirmation of adequate flow and pressure within the WRTP will be determined by the City of Woodland with each Tentative Map submittal. Further details on water supply are contained in the WRTP Water Distribution Memo, prepared 1/7/20.

Land Use Designation	Acres	Water Demand Coefficient (gpd/acre)	Add'l R&D Water Use Demand Factor (gpd/acre) ⁴	Average Day Demand (gpm)	Peaking Factor (Average Day to Max Day)	Maximum Day Demand (gpm)	Peaking Factor (Max Day to Peak Hr)	Peak Hour Demand (gpm)
LDR	74.8	3,168	0	165	2.2	362	3.5	576
MDR	35.5	6,480	0	160	2.2	351	3.5	559
¹ HDR	9.7	12,960	0	87	2.2	192	3.5	305
RTP-TO	69.6	1,440	0	70	2.2	153	3.5	243
⁵ RTP -RFO	35.9	2,448	490	61	2.2	135	3.5	215
² RTP -CCO	6.8	1,728	144	8	2.2	18	3.5	29
VCMU	3.5	2,880	0	7	2.2	15	3.5	25
VCMDR	16.7	6,480	0	75	2.2	165	3.5	263
VCLDR	13.1	3,168	0	29	2.2	64	3.5	101
³ HDR-CCO	15.9	7,448	0	83	2.2	182	3.5	289
HC	8.2	2,016	0	11	2.2	25	3.5	40
PP	0.8	2,160	0	1	2.2	3	3.5	4
OS	10.2	2,160	0	15	2.2	34	3.5	54
VCOS	10.8	2,160	0	16	2.2	36	3.5	57
Total	311.4			788		1734		2759

¹ Demand assumed to be double that of Medium Density Residential.

² Land use assumed to be 50% Research and Technology Park and 50% Community Commercial.

³ Land use assumed to be 50% High Density Residential and 50% Community Commercial.

⁴ 20% increase in demand for agricultural research and development tenants in Research Park/Community Commercial and Research Park/Research Flex.

⁵ Land use assumed to be 100% Industrial/Agricultural.



LEGEND:

- 12" W PROPOSED WATER MAIN
- 10" W EXISTING WATER MAIN
- 30" W EXISTING TRANSMISSION WATER MAIN

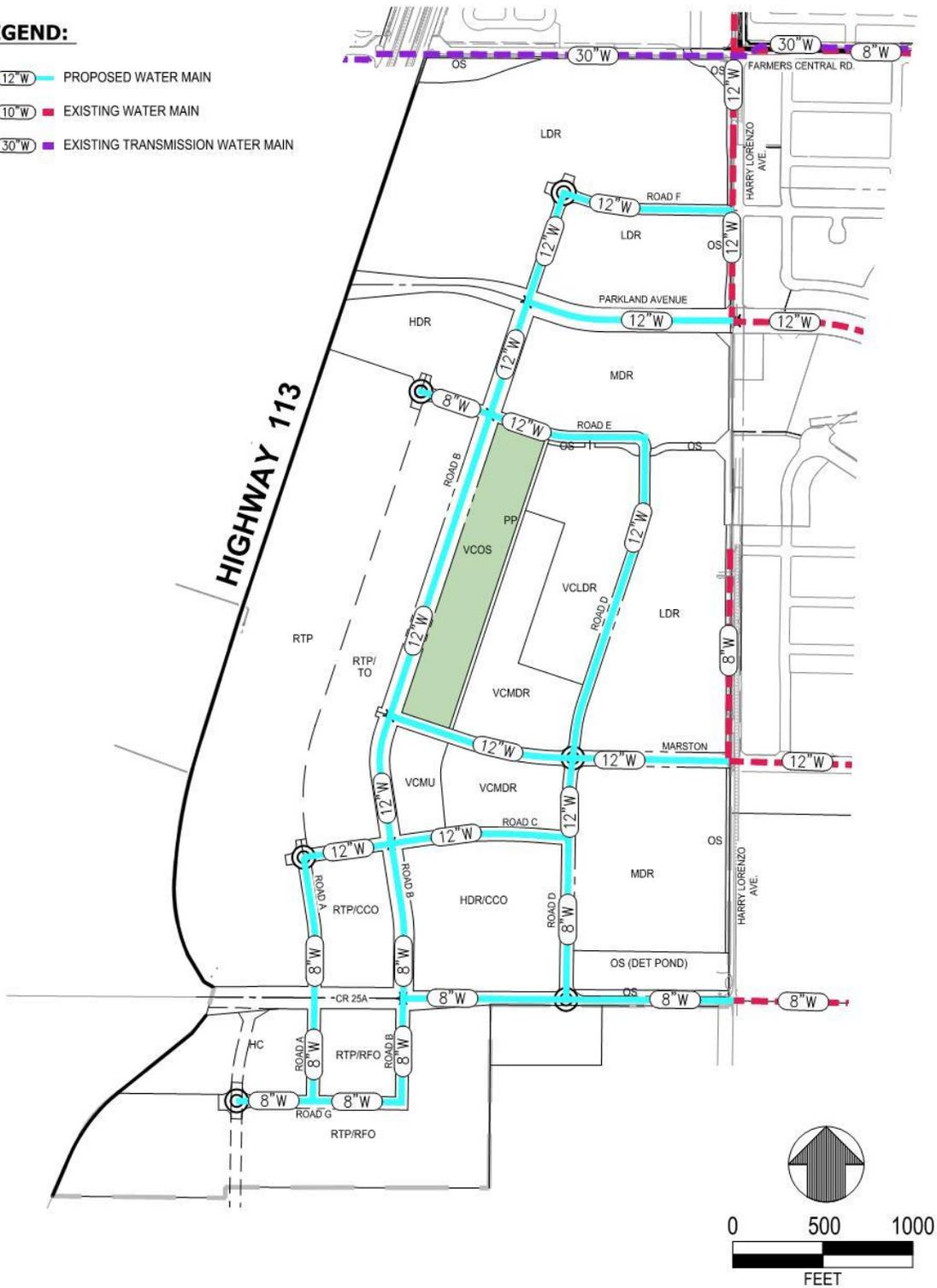


EXHIBIT 5-1: PROPOSED WATER DISTRIBUTION SYSTEM

5.3 | RECYCLED WATER DISTRIBUTION

Recycled water refers to wastewater treatment plant effluent which has received a level of treatment such that it meets the State requirements for direct non-potable use. Recycled water use is regulated by the State Water Resources Control Board which applies stringent water quality, treatment, and disinfection standards. Recycled water in Woodland meets the Title 22 requirements for tertiary level treatment with disinfection.

5.3.1 Existing Recycled Water System

The City of Woodland Public Works Department does not currently provide a continuous recycled water main connection from the Water Pollution Control Facility (WPCF) to the boundary of the WRTP. The recycled water utility currently serves parks and the north industrial area. The recycled water utility is planned to be expanded into the Spring Lake and WRTP areas to serve primarily commercial uses and landscape irrigation. This expansion project is currently planned for construction in 2024.

Additional connections to the recycled water system have been deemed infeasible.

5.3.2 Proposed Recycled Water Distribution System

Recycled water will be conveyed to the WRTP via a pressure system and routed to serve areas with commercial and public irrigation demands.

Tables 5.3 and 5.4 contain estimates of recycled water demand for the WRTP. Further details are contained in the WRTP Recycled Water Distribution Memo, prepared 3/31/20.

Table 5.3 – Recycled Water Demand Coefficients by Land Use Classification

Land Use Code	Land Use Classification	GPM/Gross-Acre
OS	Open Space	1.5

Table 5.4 – Average Day, Max Day and Peak Hour Demands

Land Use	Acres	Average Day Demand Factor (gpd/acre)	Average Day Demand (gpd)	Average Day Demand (gpm)	Peaking Factor (Average Day to Max Day)	Max Day Demand (gpm)	Peaking Factor (Max Day to Peak Hr)	Peak Hour Demand (gpm)
Park (OS)	10.8	2,160	23,263	16.2	2.2	35.5	3.5	56.5
Greenway (OS)	6.0	2,160	12,982	9.0	2.2	19.8	3.5	31.6
Median (OS)	1.6	2,160	3,456	2.4	2.2	5.3	3.5	8.4
Total	18.4		39,701	27.6		60.7		96.5

The recycled water network within the project is designed to provide service to areas with commercial and public irrigation demands such as medians, parks, and greenways. Due to the nature of this development, a main is planned to be stubbed south of CR 25A to provide service for potential future demands of the agricultural research that is anticipated to be conducted in the Research Flex Overlay land use. The public recycled water supply pipelines are all planned to be located within the right-of-way of public streets and greenways.

The preliminary recycled water layout in Exhibit 5-2 depicts the backbone infrastructure designed to connect to the existing transmission mains and to distribute recycled water throughout the WRTP. The proposed point-of-connection for the system is at the existing 8-inch main south of Osborn Drive and Farmers Central Road.

The main will then be routed south down the greenway along the west side of Harry Lorenzo Avenue, terminating at Marston Drive where it will connect to existing recycled pipelines within the Spring Lake Specific Plan area. From Harry Lorenzo, the WRTP network will extend along Parkland Avenue Marston Drive, and Road B. Refer to the Water Distribution technical memorandum for more details.

LEGEND:

-  EXISTING RECYCLED WATER MAIN
-  PROPOSED RECYCLED WATER
-  PROPOSED RECYCLED WATER (ANTICIPATED TO BE CONSTRUCTED VIA OTHER PROJECTS FUNDED BY THE CITY OF WOODLAND)

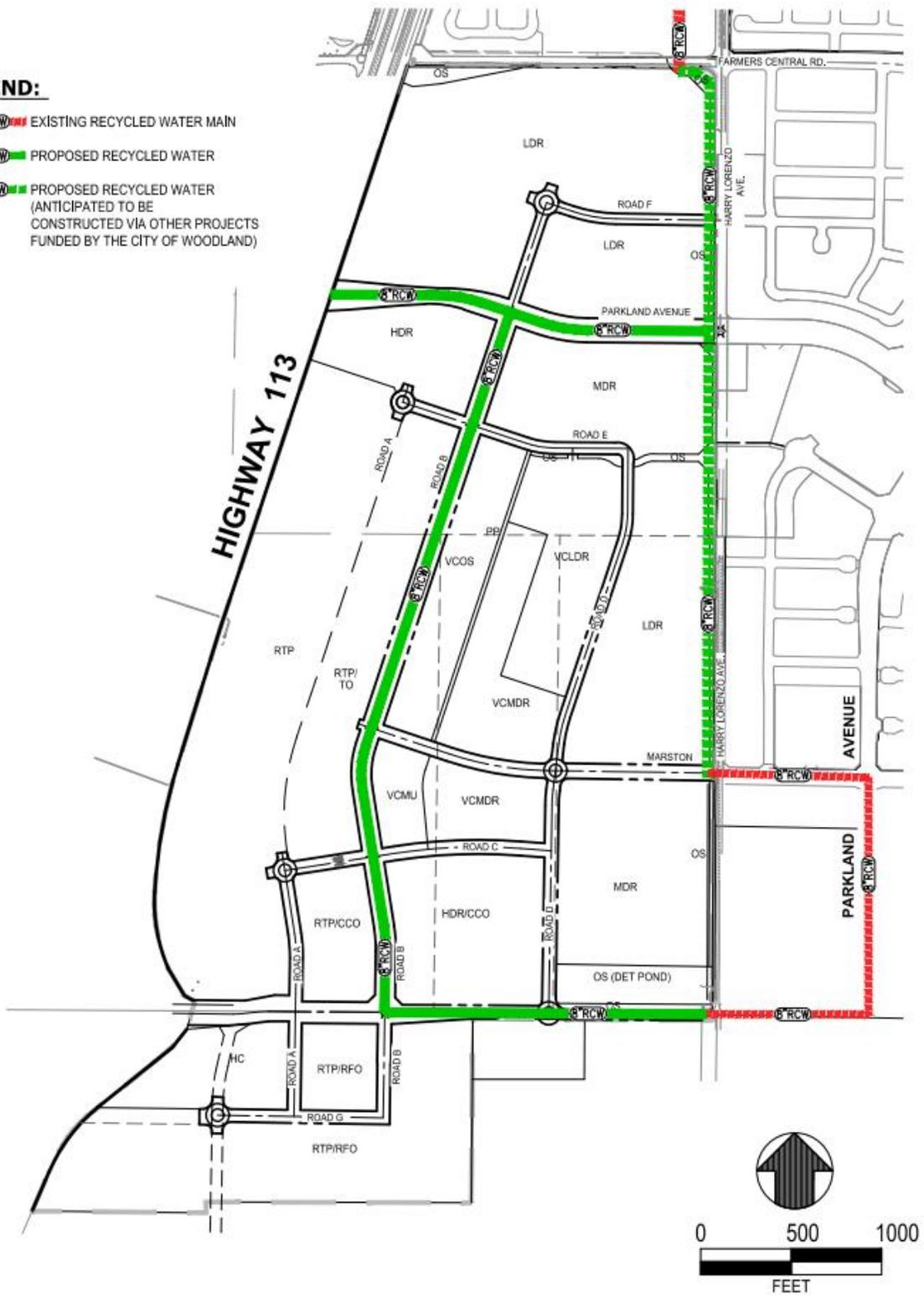


EXHIBIT 5-2: PROPOSED RECYCLED WATER DISTRIBUTION SYSTEM

5.4 | WASTEWATER

The City of Woodland provides wastewater collection and treatment throughout the City limits. The City constructed the current WPCF in 1989. Since that time, the City has upgraded the facility three times—once in 1999 and a second time in 2006, when the City expanded and upgraded the treatment plant’s hydraulic capacity from 7.8-mgd to 10.4-mgd. The City expanded the plant a third time in 2016 to increase solids handling capacity among other upgrades. In recent years, hydraulic inflows to WPCF have gone down due to water conservation and the City’s efforts to reduce infiltration and inflow, through sewer collection system rehab projects.

5.4.2 Existing Wastewater System

The City of Woodland’s Public Works Department is the community’s wastewater service provider. Woodland’s wastewater collection system conveys wastewater by gravity pipelines to the WPCF located east of the city along CR 103, where it is treated and then discharged to a large unimproved channel. Treated wastewater eventually drains to the Tule Canal on the east side of the Yolo Bypass. Woodland’s wastewater collection system consists of 270 miles of sewer main and 80 miles of service line. Projected future flows to the WPCF are about 9.2-mgd for average wastewater flows, which could serve up to 105,000 residents.

Downstream existing facilities in the general vicinity of the Plan Area include the following:

- 2 x 12” force mains, from the SLSP Pump station at Farmers Central Road and Mickle Avenue, west-to-east on Farmers Central Road, south-to-north on CR 102 and west-to-east on Gibson Road to the WPCF
- 15” to 21” west-to-east sewer main in Farmer’s Central Road corridor, from SR 113 to the SLSP Pump Station at Mickle Avenue
- 15” west-to-east main in Heritage Parkway. This will need to be extended from the terminus of Heritage Parkway to the east edge of the WRTP. It is essential that an easement be reserved for the proposed 15” sanitary sewer pipe extending from Heritage Parkway.
- 10” west-to-east main in Marston Drive.

The Spring Lake Specific Plan (SLSP) Pump Station was constructed at the intersection of Farmers Central Road and Mickle Avenue in order to get the wastewater from Spring Lake to the WPCF, a regional pumping station. The facility is designed to run at maximum design capacity of 6.1-million gallons per day (mgd). There are currently no wastewater mains or services located within the Plan Area.

5.4.3 Proposed Wastewater System

A combination of gravity and pressure sewers will be required to convey new wastewater flows to the WPCF. Upgrades made to the WPCF since 1989 will allow the facility to accept the project flows. The SLSP Pump Station has the capacity for the wastewater flows at buildout of the WRTP; however, there are portions of SP-1A and SP-1B that will require a lift/pump station in order to convey wastewater runoff to the existing gravity main in SLSP.

The Proposed Wastewater System, Exhibit 5-3, shows the components of the proposed backbone infrastructure locations and sizing to convey the wastewater produced by the WRTP to the WPCF depicting the sizing and locations of major onsite wastewater collection facilities.

The proposed points-of-connection for the system are at the existing 8" main in Harry Lorenzo Avenue at Fowler Way (future), the proposed 15" main extending from Heritage Parkway, and the existing 10" main in Marston Drive. It is essential that an easement is reserved for the proposed 15" main extending from the future Heritage Parkway in the event that the pipeline is not constructed prior to initiating development of the WRTP.

The WRTP site is relatively flat, (elevation drop of about 10 feet west-to east and negligible elevation change north-to-south). With these general grades driving the proposed gravity sewer network design, the proposed points-of-connection are located along the eastern boundary of the WRTP. Preserving the existing drainage patterns of the site, the overall shed has been divided into three main lines: Line J (North Shed, north of Parkland Avenue), Line I (Central Shed, between Parkland Avenue and Marston Drive) and Line U (Southern Shed, south of Marston Drive). Line designations have been selected to coincide with the 2004 SLSP Infrastructure Study Report. Because of the flat topography within the Site, it is expected that grading plans and street profiles will generally be undular, with alternating shallow crests and sags. It is essential that sufficient cover is provided for the wastewater pipes at the roadway sags.

The SLSP Pump Station, located at Farmers Central Road and Mickle Avenue, has the capacity for 6.1- mgd of wastewater at buildout utilizing two 90 horsepower (HP) pumps with an additional pump for redundancy. The facility was not originally designed for the updated Urban Limit Line (ULL). An analysis of projected flows from the SLSP, SP-1A (including the updated ULL), the Community Center, and the Prudler Subdivision, prepared by the Project Applicant's engineer (Cunningham Engineering Company) indicated that upon buildout of these areas the cumulative peak wastewater flow would be 5.45-mgd. The limits of the properties that account for the projected 5.45 mgd of wastewater flows are depicted below in Exhibit 5.4. Based on that analysis, it was concluded that the SLSP pump station, as it is currently designed, would have adequate capacity for the SLSP and SP-1A.



Subsequent peer review by City wastewater consultant (Water Works Engineers), identified that the City Design Standards methodology used by the Project Applicant's engineer to calculate anticipated water infiltration and inflow (I&I) was not applicable to existing development within the Spring Lake Specific Plan development. Rather, the City's current methodology for determining I&I for existing development is to use the rain derived inflow and infiltration (RDII) via the "RTK" hydrological analysis using 5yr return 24hr duration design storm conditions. In using the 5yr/24hr RTK RDII methodology for existing development (approx. 680 acres) it was determined that projected build-out flows of Spring Lake Specific Plan and WRTP (SP-1A) would be approximately 7.4 mgd, which exceeds the current capacity of the SLSP Pump Station.

The existing pump station is currently operating with 2 aging 90 HP pumps, which have always been anticipated to be replaced at the end of their useful life of approximately 20 years. Installed in 2004, they will require replacement in the next 3 to 5 years.

Water Works Engineers prepared a Spring Lake Pump Station Capacity Enhancement Assessment Technical Memorandum wherein they identified alternatives to for increasing the capacity of the pump station through pump replacement, pump upsizing, and electrical upgrades. The report included the following conclusions:

- Existing flows from Spring Lake are nearing the capacity of the existing pumps operating one at a time; hence, without the installation of a planned third pump, the pump station will no longer have redundancy.
- Replacing the existing 2 pumps with modern 90 HP, efficient pumps will increase the capacity to approximately 3.7 mgd with redundancy for the current measured flows.
- New rain gauges have been installed that will provide updated information for future analysis.
- Adding a 3rd new 90 HP pump will increase the pump station capacity to approximately 7.5 mgd with the 3rd pump providing redundancy.

The timing of pump replacements and additions will be determined by the City based on the recommendations presented by Water Works Engineers; however, addition of a 3rd pump for backup redundancy will be required to develop SP-1A. Additional pump station upgrades will need to be analyzed in the future, or an alternative solution provided for subsequent SP-1B and/or SP-1C developments.

The discussion above focuses on the capacity of the SLSP Pump Station. An analysis was also conducted to determine if future wastewater generated from properties located within the ULL update area, specifically the portion of the Plan Area south of CR 25A could be conveyed via gravity flow. , The area hatched in the southwest corner of the Proposed Wastewater System (Exhibit 5.3)

(approximately 7.3 acres) will require either a lift station or several feet of fill soil to convey wastewater runoff to the existing gravity main in SLSP. The pump size for a lift station option has not yet been determined and would need to be based on wastewater flow rate generated by the land use at the time of site design. The cost for this improvement has not been included in the WRTP CIP. The capital and maintenance costs will need to be shared by the property owners that require the pump station for the development of their property.

The area in SP-1B that cannot be serviced via gravity in SP-1A is shown in Exhibit 5-4. A pump station along 25A and 3,000 LF of force main will be required to connect SP-1B development to the WPCF. Further details on the wastewater system are contained in the WRTP Wastewater Collection System Memo, prepared 3/31/20.

5.5 | STORMWATER MANAGEMENT

The proposed onsite drainage system will consist of collection and conveyance facilities that will carry stormwater via gravity generally from west to east through the SLSP area. Wherever feasible, storm drain pipes have been designed to flow full under gravity in the 10-year storm. Based on peak flow in major storm drain pipes during a 10-year storm, the existing downstream conveyance system has adequate capacity to accept the stormwater generated from the WRTP.

5.5.1 Existing Conditions and Stormwater Management System

The WRTP is relatively flat, with an elevation drop of about 10 feet west-to-east and a negligible elevation change north-to-south. West-to-east ground slopes range from 0.2% to 0.4%.

There are no natural streams or waterways on the WRTP, nor is there currently a developed onsite drainage system capable of managing future flows from development of the WRTP. Minor roadside ditches accept runoff from county roadways, and agricultural ditches and culverts currently manage local agricultural runoff within the Plan Area.

Under developed conditions, the drainage from the project site can be divided into six main sheds, as shown in the WRTP Stormwater Management Memo (7/12/23). The limit lines of the six proposed sheds are generally consistent with the shed limits originally identified in the 2004 Spring Lake Specific Plan (SLSP) Infrastructure Study Report (ISR) and reflected in subsequent updates to the City's South Urban Growth Area SDFMP.

LEGEND:

-  PROPOSED WASTEWATER LINE
-  PROPOSED WASTEWATER LINE (NOT YET CONSTRUCTED, ASSUMED TO BE IMPLEMENTED PRIOR TO SP-1A)
-  EXISTING WASTEWATER LINE

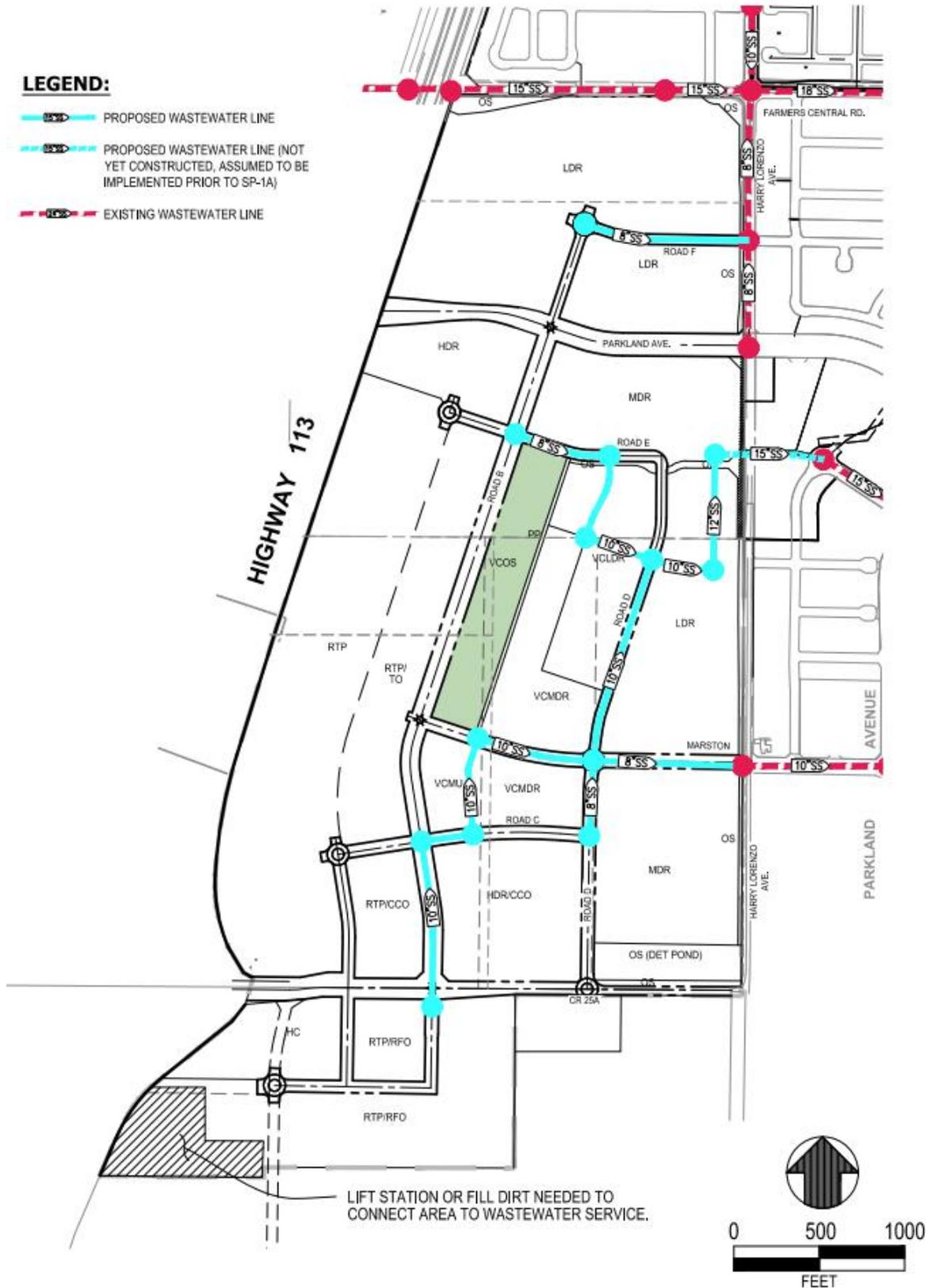


EXHIBIT 5-3: PROPOSED WASTEWATER SYSTEM



the eastern green belt is constructed, development phasing plans will need to account for the relocation of agriculture storm drainage conveyances that are currently located in the Harry Lorenzo Avenue corridor.

The Federal Emergency Management Agency (FEMA) delineates the boundaries of the FEMA 100-year floodplain on the basis of hydrology, topography, and modeling of flows during specified design rainstorms. The maps indicate that the WRTP is located in Zone X, which depicts areas determined to be outside the 0.2% chance annual floodplain. There are no special development considerations or insurance requirements affecting the site.

According to the Natural Resources Conservation Service (NRCS) Soils Survey, the site area contains soils classified as Hydrologic Soil Group (HSG) 'B' (56% of site area) and HSG 'C' (41%), together with a small pocket of HSG 'A' (3%). North of Parkland Avenue, virtually all the site area is mapped as having HSG 'B' soils. South of Parkland, HSG 'B' predominates on the western portion of the site; HSG 'C' on the easterly portion.

5.5.2 Adjacent/Downstream Drainage Facilities

Downstream existing drainage facilities in the general vicinity of the WRTP include the following:

- 60" to 72" west-to-east storm drain pipe in Farmers Central Road corridor, from SR 113 to the head of the Farmers Central Channel (FCC) near Pioneer Avenue.
- 30" to 48" south-to-north storm drain pipe in Harry Lorenzo Avenue (HLA), draining to the above- referenced 72" pipe.
- 60" to 66" south-to-north storm drain in Pioneer Avenue, draining to FCC (with an upstream 54" to 60" pipe proposed between HLA and Pioneer).
- 48" to 2-66" west-to-east storm drain pipe in Heritage Parkway, plus a 30" west-to-east storm drain pipe extension which may be installed prior to initial construction of the WRTP. If it is not constructed by others prior to SP-1A, then an easement will need to be reserved for a proposed 30" storm drain pipe extending from Heritage Parkway.
- 42" north-to-south storm drain within the existing right-of-way of HLA extending from the south side of CR25A approximately 40 north of the northern 25A right-of-way. Said pipe outfalls into an unlined channel located along the south side of CR 25A (see next item below).
- West-to-east unlined channel along the south side of the CR 25A corridor, beginning east of HLA and continuing east.

5.5.3 Proposed Stormwater Management System

The proposed onsite drainage system will consist of a system of collection and conveyance facilities, which will carry stormwater via gravity generally from west to east towards HLA. From there, existing offsite facilities will convey runoff to the east through the SLSP area.

Major constraints affecting storm drainage for the WRTP include limited fall for gravity drainage, (particularly in a north/south direction), and a significant upstream area discharging agricultural runoff through the development area.

1. PROPOSED CONVEYANCE SYSTEM

The major parameters to be used in the design of storm drainage facilities for the WRTP will include the computed peak flow rates and runoff volumes for selected design. For the onsite conveyances, the key parameter is the peak runoff rate (10-year for pipes, 100-year for channels and overland release). For offsite conveyances (such as those originating from SP-1B), the key parameter is the 100-year peak runoff rate. For regional detention facilities, key sizing parameters are the 100-year peak flow and runoff volume.

10-Year Storm Routing. Developed on-site flows will be conveyed generally from west to east through new storm drainage pipes. Pipes will usually be located in public streets or in proposed storm drain easements. Storm drain pipes will be designed to flow full under gravity in the 10-year storm. In general, the 10-year hydraulic grade line (HGL) is typically contained within the pipe wherever feasible. If the 10-year HGL rises above the pipe soffit, designs will seek to maintain the HGL at least 1 foot below finish grade.

100-Year Storm Routing and Release Point. The downstream (east) edge of the overall WRTP shed lies along the HLA corridor, and 100-year release points from the overall shed are in general expected to be along HLA between CR 25A and the FCC.

Within the Plan Area, on-site flows in excess of pipe capacities (i.e. in excess of the 10-year flows) will be conveyed overland via collector and arterial streets, and in greenbelt corridors. Release points for the 100-year storm will be provided for all onsite developed drainage sub-areas. It is expected that the overland flow patterns will generally follow the direction of piped (10-year) flows.

In general, local street profiles are expected to be designed such that low points overflow when the water depth reaches a maximum of 1 foot. Overflows should be able to “cascade” over successive local street high- points to an outlet downstream. Individual development sites (such as single-family subdivisions, multifamily apartment sites, or portions of the research park/business park element) should typically be designed such that their 100-year release points discharge to an abutting collector or arterial street,

Such streets will have an overland flow path to the downstream end of the WRTP. Overland release may also be to an abutting channel, sized for the 100- year/24-hour peak flow with one foot of freeboard. Alternatively, overland flow may be conveyed in greenbelts provided that (a) the



greenbelt connects directly to a downstream channel or arterial street, (b) the greenbelt can be shown to have sufficient hydraulic capacity to carry the 100-year flow from its ultimate developed contributing area, and (c) frequent flows will not compromise the primary function of the greenbelt as an amenity.

Runoff from offsite shed HW20, located west of SR 113 and north of CR 25A, will be intercepted by a proposed onsite interceptor/conveyance channel to run along the east side of SR 113 and then for about 500 LF along the north side of CR25A. It will be sized for the estimated 100-year peak discharge from the existing (undeveloped) HW20 shed.

Allowance will be made on one side of the channel for a 12- foot maintenance/access road. The downstream segment of the swale along CR 25A will discharge to a proposed 54" to 60" storm drain pipe just west of the proposed intersection of CR25A with WRTP Road 'A'. The pipe, to be sized for the 100-year peak flow from HW20 plus contributing areas in the WRTP, will cross onto the south side of CR 25A and continue east to the proposed South Regional Pond (SRP).

The SRP will be located east of SR113, just south of CR25A. Adequate storage should be provided to manage development from a portion of the sheds located east of SR113. The sizing of the pond is currently shown on Figure 5-5 as approximately 4.5 acres. This sizing is preliminary and will be refined in conjunction with the design of backbone infrastructure for the southern portion of the WRTP.

Outfall from the SRP will be via a drainage channel (approximate size is shown on Section T included in Chapter 4) that will connect to the existing channel located along the southern boundary of Spring Lake. There is an existing 8" gas main in a north-south alignment along the west side of the gravel extension of Harry Lorenzo Ave which the channel will need to cross to connect to the existing channel. To connect the channels together and avoid vertical relocation of the gas main, one 4' high by 6' wide box culvert will be required.

2. STORMWATER QUALITY, LOW IMPACT DEVELOPMENT (LID) AND HYDROMODIFICATION CONSIDERATIONS

Based on the requirements of the State Water Resources Control Board (SWRCB) NPDES General Permit, water quality (WQ) treatment design for the WRTP will be addressed by implementing LID measures, standard Treatment Control BMPs, and 'end-of-pipe' WQ storage within existing and proposed detention basins and upland LID-style runoff-reduction measures and end-of-pipe detention storage within existing and/or proposed detention basins. Key LID elements will include:

At-source drainage management. This entails integration of small-scale distributed drainage management features such as shallow, decentralized surface detention areas and/or infiltration

areas that are consciously designed into streetscapes and individual site landscapes throughout the project area. An at-source drainage management approach encourages the use of drainage as a design element, rather than solely as a functional requirement. Landscape and engineering designs for proposed developments within the project site should be developed collaboratively from the outset.

Reduction of new impervious areas. This can be accomplished with compact building footprints, alternative driveway layouts and/or materials, narrower roadway cross-sections (as appropriate), pervious pavement and efficient parking layouts. For all parking lots, the following elements can help reduce impervious areas:

- Efficient layouts that seek to minimize the overall area of the lot on a per-parking-space basis.
- Examination of parking minima/maxima, with the intention of not 'overparking' site designs.
- Construction of parking areas from pervious materials.
- Exploration of shared parking opportunities for adjacent non-concurrent uses.
- Use of tree canopies for runoff interception.

Disconnection of new impervious areas. This can be accomplished through judicious site design that places pervious areas (landscaping and/or pavement) downstream of a site's impervious surfaces (roofs and conventional pavement), with site grading/landscaping designs that provide for sheetflow from those impervious surfaces onto pervious surface areas. This would include avoidance of direct connection of roof downspouts to the receiving storm drain pipe system and landscaped areas immediately upstream of DI's receiving runoff from new pavement areas. Treatment Control BMP's will typically include:

- Vegetated swales. Swales can be readily integrated into most site developments provided they are planned for at an early stage of the site design. The key is in integrated planning and design collaboration between all members of the site design team.
- Stormwater planters/rain gardens. Similarly, integrated design of site improvements can allow for the incorporation of rain gardens into planned low points in the site landscape areas.
- Pervious pavements. These can be well-suited to parking lots and access drives. They may consist of pervious asphalt, pervious concrete or pavers.
- End-of-Pipe Water Quality Storage. Proposed detention basins serving the Site will include a water quality component, typically a shallow (roughly 1' to 3' deep) water-quality retention pool below the detention storage pool.



5.5.4 Phasing of Drainage Facilities

Phasing of drainage facilities within the Plan Area will be mostly guided by development phasing. However, installation of major facilities to serve the Plan Area is expected to proceed from downstream to upstream (i.e. east-to-west from HLA towards SR113).

The SDFMP estimates that 30 cfs of pumping and construction of the North Regional Pond will accommodate the buildout of the SLSP plus approximately 80 additional acres of currently unbuilt residential development, flowing to the existing Farmers Central Channel, in addition to the SLSP. These improvements have recently been constructed. Since these improvements benefit other properties, the WRTP will be required to contribute a pro rata financial share toward the construction of these regional facilities. It is assumed that non-residential development could alternatively be accommodated, as long as the development acreage is hydrologically equivalent to 80 acres of residential use. Development of more than the equivalent of 80 acres within the WRTP will trigger further improvements to the new pump station, the East Main Channel and the Yolo Bypass Outfall.¹

As a result of modeling being performed for the City's North Area, by the City's drainage consultant (Wood Rodgers) it has been determined that a greater allowable flow over the downstream High Line Ditch than previously modeled is likely. With the recently installed downstream infrastructure upgrades described above and an assumed higher allowable spill over the High Line Ditch, it is anticipated that more development in the Plan Area can occur before triggering additional Plan Area or downstream improvements.

Revised baseline conditions modeling and detailed information defining the operational capacity of the recently installed infrastructure will need to be determined prior to allowing development beyond 80 residential equivalent acres to proceed.

5.5.5 Land Acquisition for Drainage Easements

Land acquisition or easement requirements for regional drainage facilities will consist of:

- An easement on the Beeman property south of CR25A for the proposed SRP.
- Easement for a north-to-south interceptor/conveyance and gravel access road along the east side of SR 113 (north of CR25A), and a west-to east conveyance and gravel access road along CR25A to convey runoff from SDFMP subshed 'HW20' to the SRP.
- Easement for a south-to-north conveyance east of SR113 (south of CR25A) to convey runoff from SDFMP subshed 'SB101' to the SRP.

Exhibit 5-5 depicts the proposed backbone infrastructure for drainage and stormwater management. More detailed information is contained in the WRTP Stormwater Management Memo (7/12/23).

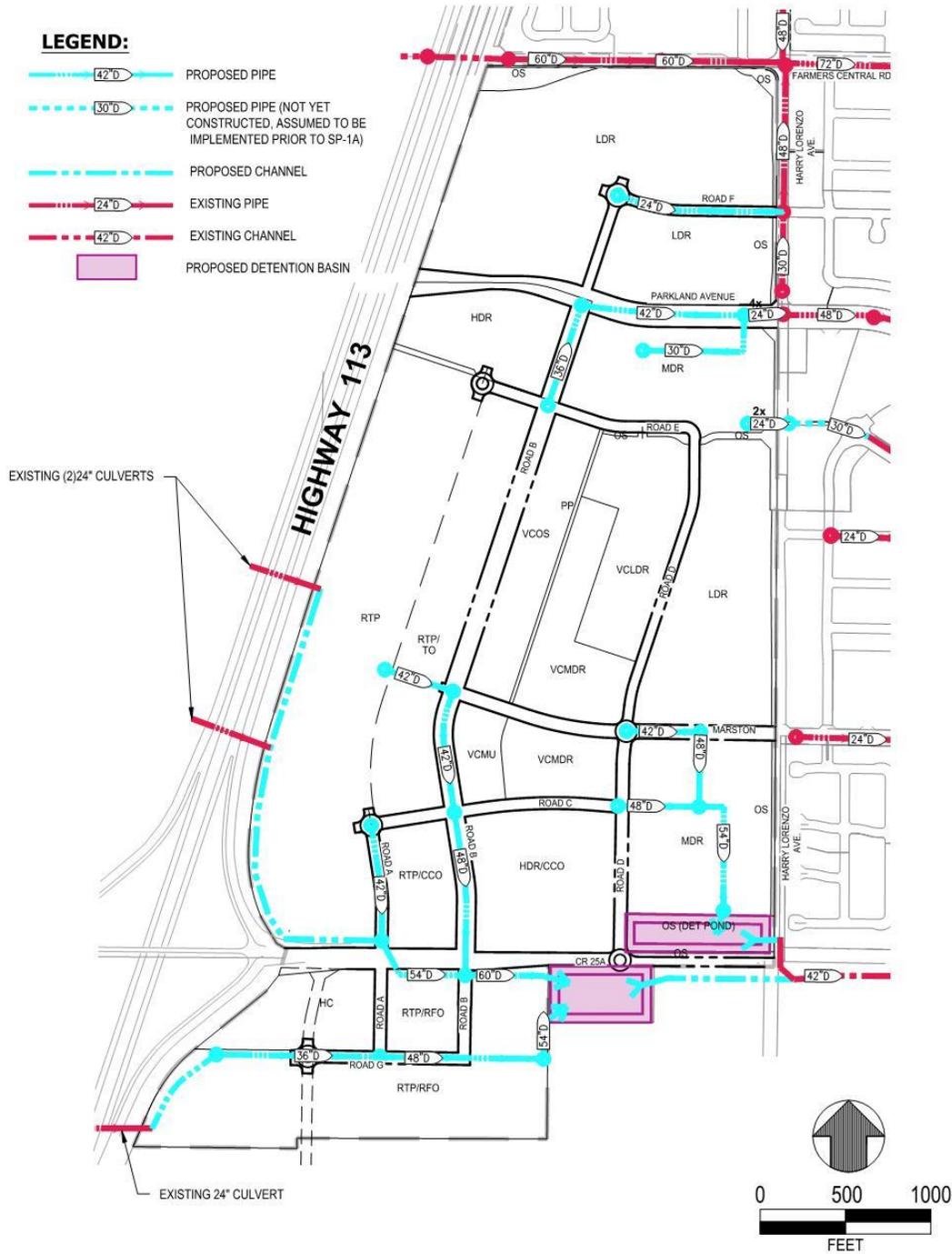


EXHIBIT 5-5: PROPOSED DRAINAGE AND STORMWATER SYSTEM



5.6 | ELECTRIC POWER

5.6.1 Existing Conditions

Pacific Gas and Electric (PG&E) provides electricity and gas to the City. In June 2018 Valley Clean Energy began providing electricity in Woodland as “cleaner energy” alternative; however, distribution of said electricity remains the responsibility of PG&E.

PG&E operates and maintains a 60 kV electric transmission line, along with corresponding distribution facilities, adjacent or within the project site boundaries. Land uses are restricted along the transmission facilities.

The 60kV electric transmission line is located immediately north of the WRTP within the Farmers Central utility corridor. Generally, it is considered infeasible to underground high voltage electric transmission lines; however, the poles are located along the northern portion of the Farmers Central utility corridor and are not anticipated to be an impact to developing the project.

A 12kV overhead electric distribution line currently exists along the east boundary of the WRTP along Harry Lorenzo Avenue from Farmer’s Central Road to and continuing south of CR 25A. The poles are located along the west side of Harry Lorenzo Avenue from Farmers Central Road and transition to the east side of the roadway approximately ½-mile to the south. A service pole line extends into the interior of the WRTP approximately 2,300 feet north of Harry Lorenzo Avenue to provide power to the agricultural buildings.

Similarly, there is another 12kV distribution line along the north side of CR25A which continues northwards within the WRTP adjacent to the Highway 113 northbound onramp, to a point approximately 1,000 feet north of 25A where it provides power to an irrigation well prior to crossing HWY 113 toward the west. A second irrigation well located approximate 3,100 feet north of 25A along HWY 113 receives power from a service line from the west side of the highway, terminating at the well.

5.6.2 Planned Improvements

Implementation of the Specific Plan will require expansion of electrical distribution and transmission lines and related facilities. In addition to adding new distribution feeders, the range of electric system improvements needed to accommodate new growth may also include upgrading existing substation and transmission line equipment, expanding existing substation(s) to their ultimate build-out capacity, building new substations, and interconnecting transmission lines. The new development will be responsible for the costs associated with the necessary expansion and upgrading of the systems. Any authorized provider may provide the service.

More specifically, existing 12kV overhead electric distribution lines along Harry Lorenzo Avenue and/or CR25A may remain in place until adjacent property within the Plan Area is developed, at which time they shall either be placed underground or eliminated if the development is served from an alternative corridor. Similarly, as existing facilities within the Plan Area are taken offline overhead services to them shall be removed and new underground service in a joint trench shall be provided for the WRTP facilities.

5.7 | NATURAL GAS

5.7.1 Existing Conditions

PG&E operates and maintains an 8-inch gas transmission line, within and extending south of the project site along the west side of Harry Lorenzo Avenue.

Implementation of the Specific Plan will require the expansion of distribution and gas transmission lines, and related facilities to serve the WRTP. The new development will be responsible for the costs associated with the necessary expansion and upgrading of these systems. Any authorized provider may provide the service.

5.7.2 Planned Improvements

Initial gas service will likely be extended from gas distribution mains installed for the Spring Lake Specific Plan. Tying into the existing 8-inch gas transmission line would likely require the installation of a regulation station. The service provider will determine the desired distribution system.

Preliminary information regarding the depth of the 8-inch main indicates that to accommodate the proposed outfall conveyance channel from the South Regional Detention Pond a 4' x 6' box culvert will be required to be installed below the main to connect to the existing downstream channel. See Section 5.5.3 of this chapter for additional information regarding the box culvert.

5.8 | TELECOMMUNICATIONS

5.8.1 Existing Conditions

AT&T provides telephone service to existing land uses within the WRTP boundaries. Implementation of the Specific Plan will require the expansion of telephone system to serve the WRTP Area. The new development will be responsible for the costs associated with the necessary expansion and upgrading of any authorized system. Initial service will likely be extended into the WRTP along Harry Lorenzo Avenue.

Wave Broadband provides television cable and broadband to the Plan Area.



5.9 | FIRE PROTECTION

The project site is within the jurisdiction of the Woodland Fire Department which provides fire protection services and emergency medical services within the City and to unincorporated areas in the vicinity. The City of Woodland serves its community with three permanent fire stations, each of which is staffed by firefighters 24 hours a day, 7 days a week.

The closest station to the Plan Area is Station Three located at 1550 Springlake Court, on the east side of SR 113, approximately 2 miles north of the Plan Area. The Departments' current Insurance Services Office (ISO) rating is three, with one indicating excellent service and ten indicating minimal or no protection. ISO ratings reflect firefighting personnel, equipment, and response times. The City has a maximum "first response" standard of four minutes.

The City plans to relocate Fire Station Three to the former Willow Spring Elementary school site, which is located at the northwest corner of Bourn Drive and Gibson Road., as shown on Exhibit 5-6, Public Services Proximity. The future Fire Station Three, located approximately one-half mile north of the Plan Area, will provide service to the Plan Area.

5.10 | LAW ENFORCEMENT

The Woodland Police Department will provide law enforcement services in the Plan Area. The Department currently services the community through a four police beat deployment model. With the geographic expansion of the city as the General Plan Growth areas build out, a fifth police beat is planned to be added; however, it is not anticipated to be located within the Plan Area. The existing Police Station and Administration building at Lincoln Avenue and 6th Street is sufficiently sized to provide Police service levels through build out of the Plan Area. The Police Department also provides parking enforcement services for the community. With the mixed use and denser nature of the project, the need for permit parking and time parking zones is likely. In the event this occurs, use of new technologies will be utilized to reduce resources needed for enforcement and monitoring.

5.11 | PARKS, RECREATION FACILITIES, AND PROGRAMS

Parks and recreational facilities and programs are provided by the City's Community Services Department. The Community Services Department also provides recreation programs to City residents and visitors. Examples of programs include youth sports, adult sports, youth and adult aquatics classes, senior services, youth and adult enrichment programs and various other leisure and recreation opportunities.

The City of Woodland proudly provides more than 394 acres of parks and recreation facilities, including 149 acres of developed parkland and 24 acres of other facilities, including mini neighborhood and community parks, a community sports park, a 50-meter aquatics complex, and six recreational facilities, including the 13-acre Woodland Community & Senior Center. Park sites within proximity to the Plan Area are shown on Exhibit 5-6.

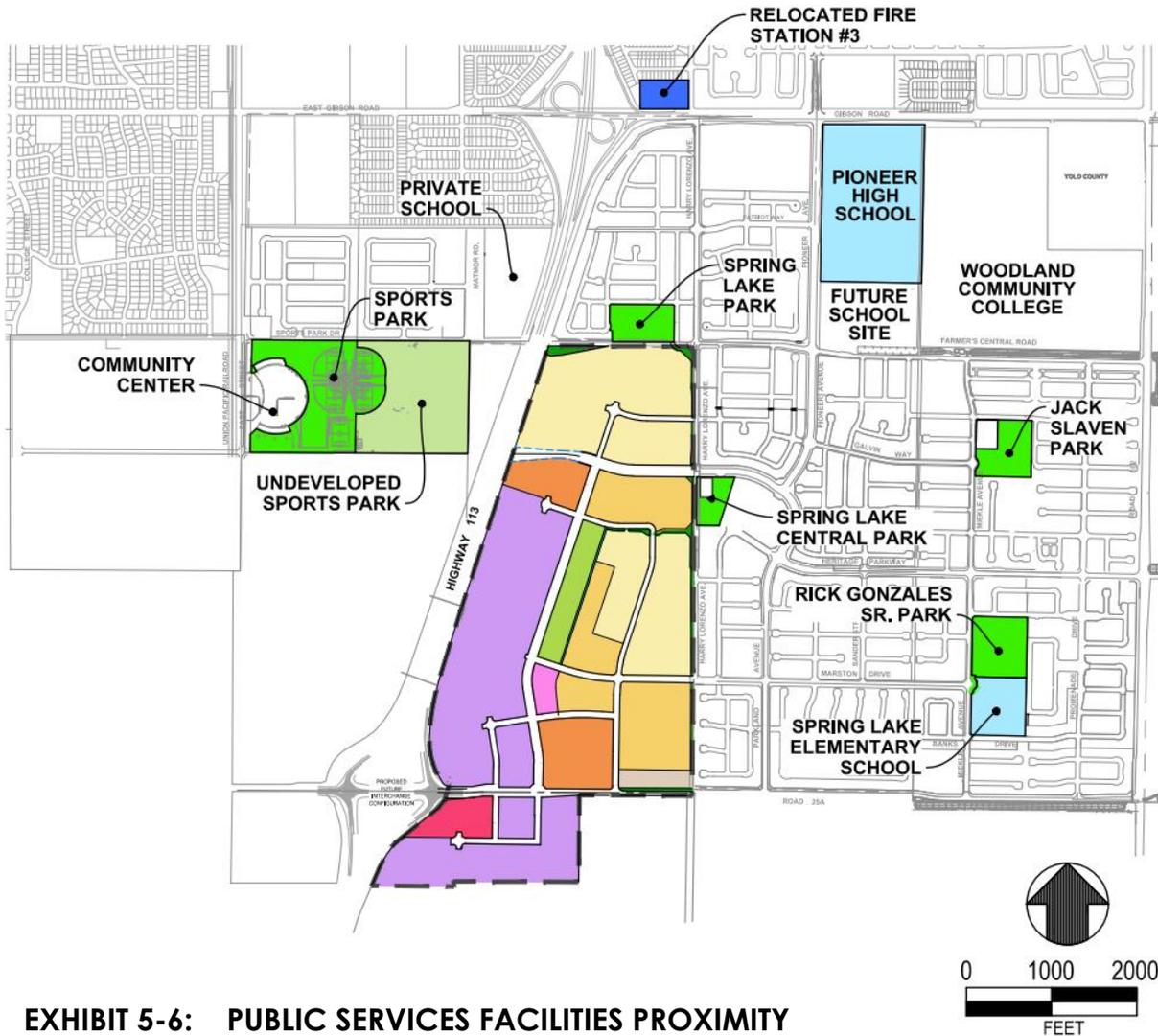


EXHIBIT 5-6: PUBLIC SERVICES FACILITIES PROXIMITY

The City’s General Plan establishes a goal of providing 6.0 acres of parks for every 1,000 residents and encourages the distribution of parks such that every residence is within a one-quarter mile of a neighborhood park. Based on average person per household factor, the Specific Plan would result in a build-out population of 4,386 which equates to providing approximately of 26.3 acres of parks, as shown in Table 5.5.

5.11.1 Proposed Park and Recreation Facilities

The parks and open space system is a key feature required in order to meet the health, wellness and support of community values envisioned for the Plan Area. The interconnected open space, active and passive recreation facilities will provide ample places for physical activity and provide places for residents to meet, socialize and play. Together, the park and open space amenities provide a full range of active and passive recreation opportunities to help form a unifying element of the community. The land use plan allocates 11.6 acres of park and 6.0 acres of greenbelts. Park and open space acreage calculations are shown in Table 5.6.

Table 5.5 Estimated Park Land Requirements				
Land Use	Estimated D.U.	PPH Factor	Population	Plan Area Park Acres 6 acres*/1000
Low Density	500	3.01	1,505	
Medium Density	600	3.01	1,806	
High Density	500	2.15	1,075	
Total units	1,600		4,386	26.316 acres

Source for Population per household factors is WMC Section 16.48.020.

Parks and open space provided in the Plan Area are shown on Exhibit 5-7 and include the following park classifications:

1. MINI/POCKET PARKS AND PLAZAS

Privately maintained mini/pocket parks, plazas, courtyards and common areas, typically range from 0.1 to 1.0 acres, and serve as amenities within the multi-family developments and within the RTP and commercial zones. A 0.5-acre pocket park, located east of Road E along the HLA greenbelt, will function as a neighborhood amenity, a trailhead and provide a connector between the Plan Area and the adjacent Spring Lake Community Park.

2. NEIGHBORHOOD PARKS

Neighborhood parks provide mostly passive recreation uses, serving neighborhoods within a range of about one-half mile (or a 10-minute walking distance). The 10.8 acre park, named The Yard, is designated as the neighborhood park to serve the Plan Area.

LEGEND

- VCOS VILLAGE CENTER OPEN SPACE - THE YARD
- OS OPEN SPACE - GREENBELTS

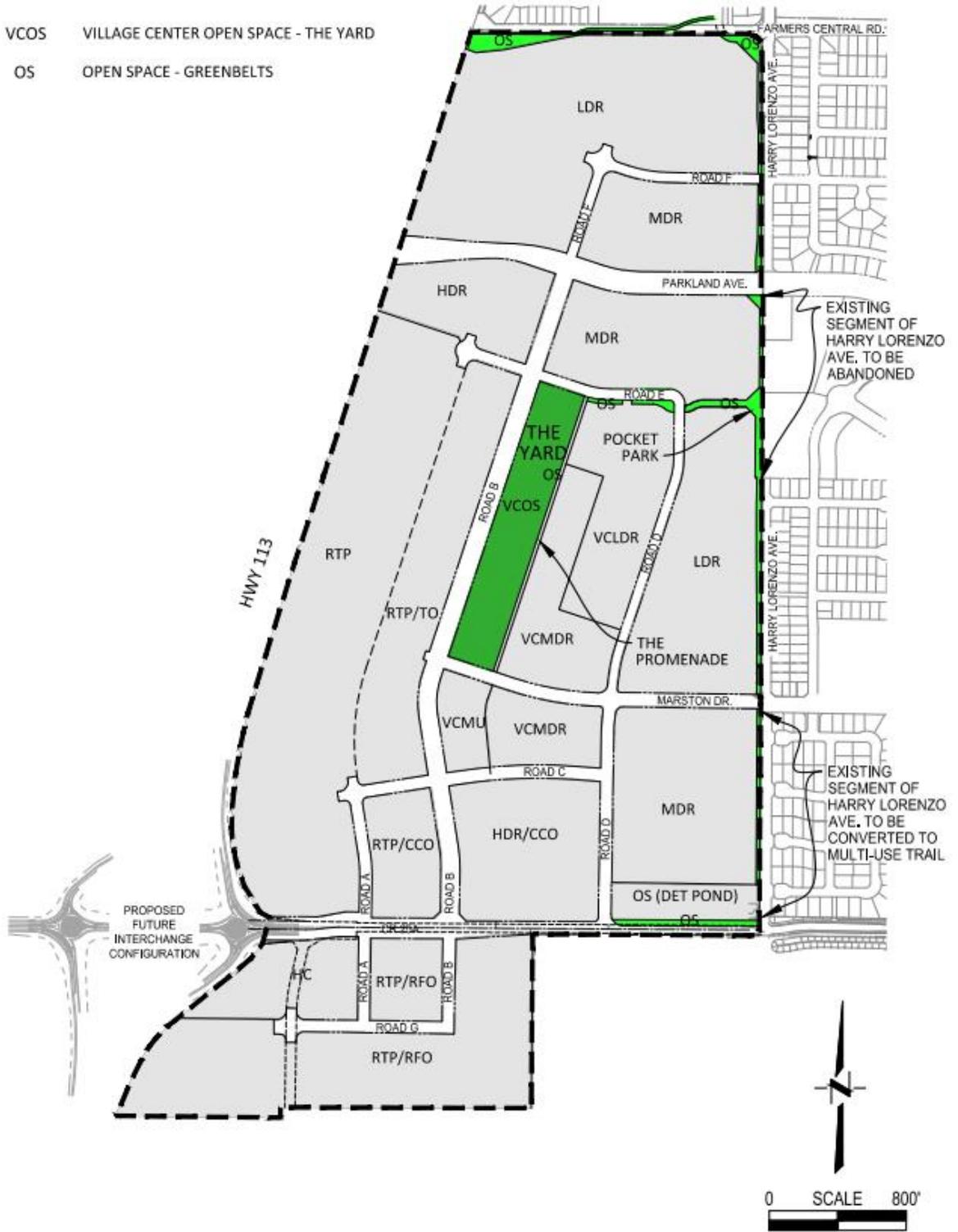


EXHIBIT 5-7: PROPOSED PARKS/GREENBELTS



3. GREENBELTS AND MULTI-USE TRAILS/PATHS

Greenbelts are envisioned as landscaped, open space areas to be used for recreation and non-motorized transportation. Greenbelts may be designed to include playgrounds, open turf or planted areas, shade trees, plazas, picnic areas, and are connected by bike/walking paths. Greenways for the WRTP also provide stormwater management, including drainage and connections to open space areas used for stormwater detention/retention. The Land Plan allocates a total of 6.0 acres of open space for green belts, as shown in Exhibit 5-7. Additionally, the Plan includes 5 miles of multi-use trails and paths that along with the greenbelts and parks, create a network of off-street biking and walking paths.

The greenbelts provide vital pedestrian and non-motorized connectivity within the Plan Area and to the adjacent land uses. The primary greenbelt facilities in the Plan Area are located along Harry Lorenzo Avenue, along the extension of Farmer's Central Road and as a connection from the proposed Spring Lake Central Park to the Plan Area's Central Green.

The greenbelt extending from the Road E/Road D intersection to the Eastern Greenway Connector will be 40 feet wide and have a 10 foot wide shared use path.

Eastern Greenway Connector – Harry Lorenzo Multi-Use Trail

A greenway along the eastern edge of the Plan Area will serve as a central spine, connecting the Plan Area to the existing Spring Lake development to the east. Located along the western edge of Harry Lorenzo Avenue, the greenway will extend the entire length of the Plan Area between CR 25A and Farmers Central Road. South of Marston Drive, the existing pavement in Harry Lorenzo Ave will be removed and the existing right-of-way will be utilized for the greenway. This facility will be more informal in nature with a 10-foot wide shared use path that gently meanders through landscaped open spaces. Multiple trail connections to Plan Area subdivisions will be provided to connect pedestrian and bicycle traffic to the linear greenway and to/from the adjacent Spring Lake community. Intersections with Marston, Parkland, and Road F will be designed with a distinctive treatment to clearly delineate each crossing.

PG&E maintains and operates an existing high-pressure gas transmission main within the corridor. Landscaping within the vicinity of the transmission main shall conform with PG&E planting zone guidelines. The planting zones are identified as the Pipe Safety Zone, the Border Zone, and the Outer Zone. Generally allowable groundcovers, shrubs, and tree sizes for each zone are defined below:

Pipe Safety Zone: Lawns, flowers, low-profile grasses and low-growing herbaceous plants.

Border Zone: Small to medium shrubs with a trunk or main branch less than 8" in diameter at full maturity, measured at 4.5 feet from the ground.

Outer Zone: Large shrubs and small trees with a trunk less than 36" at full maturity measured at 4.5 feet from the ground.

The relationship of the path location relative to the gas main and the corresponding landscape zones are depicted on the following Sections U-U through Y-Y. The path and greenbelt amenities shall be designed in accordance with the Multi-Use Trail section in Chapter 4.

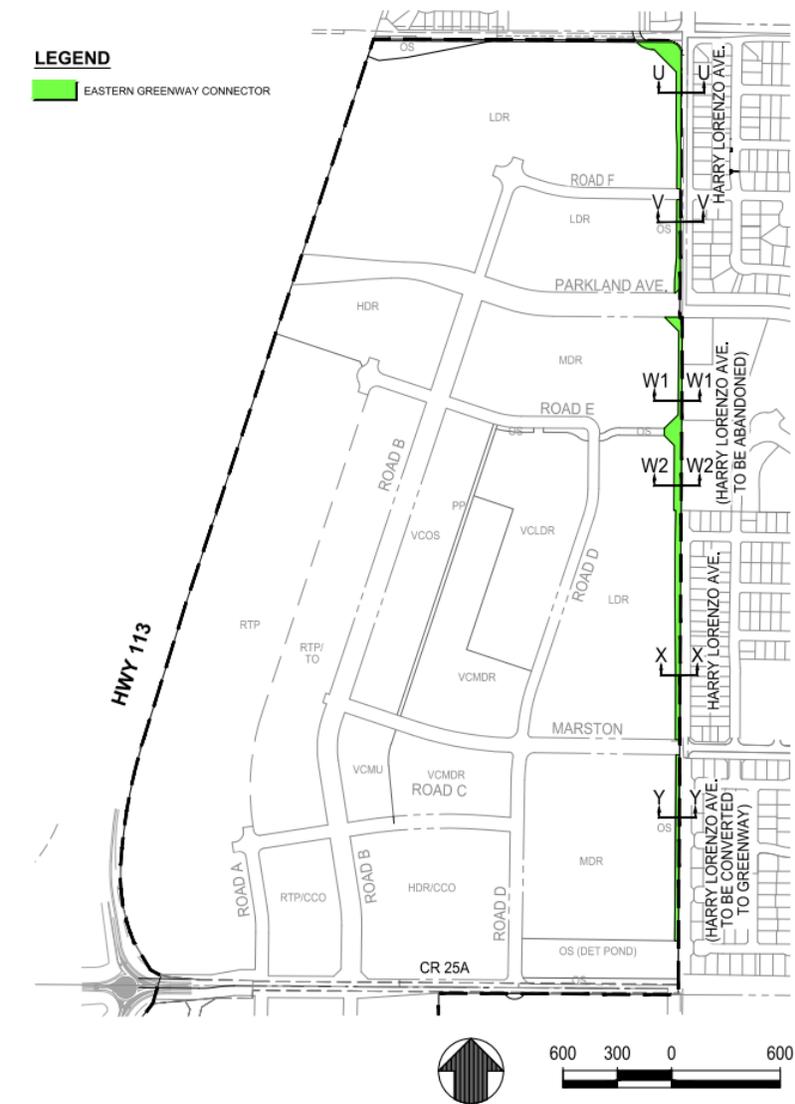
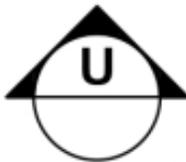
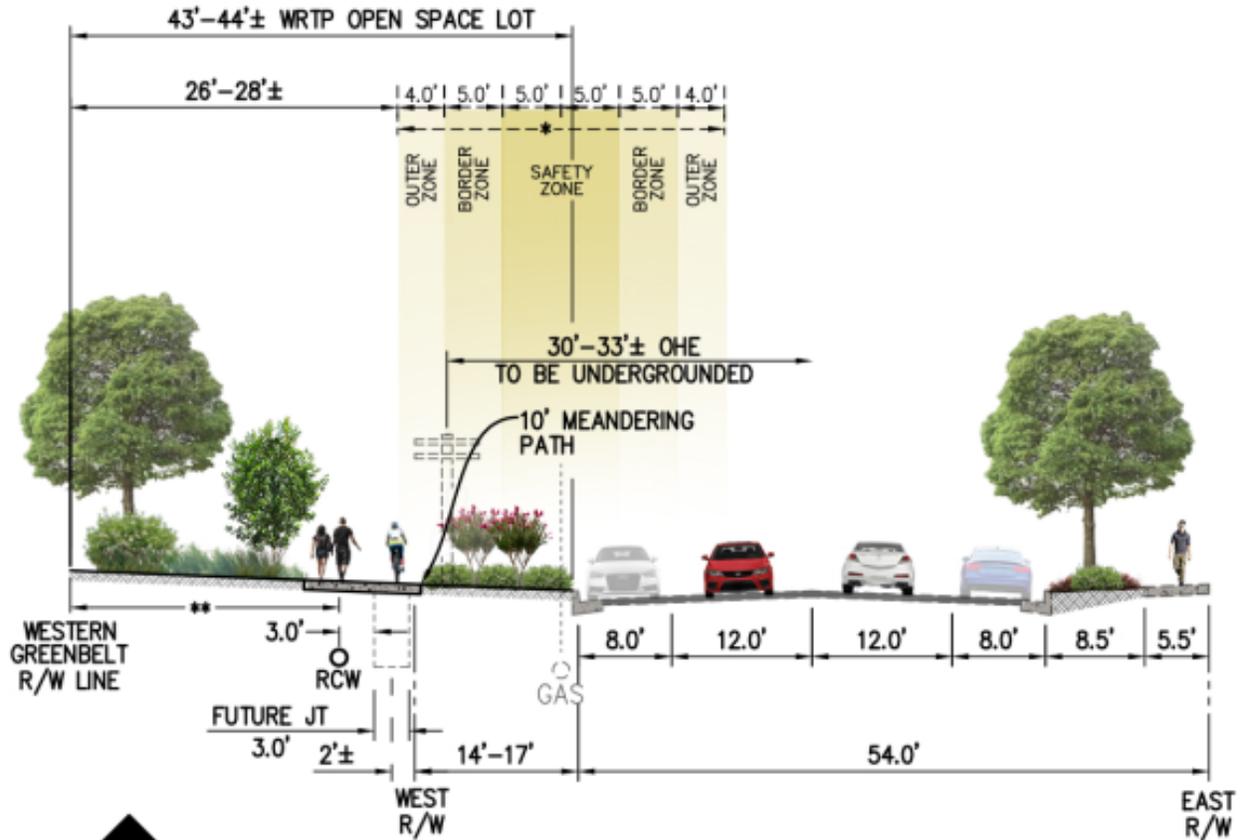


EXHIBIT 5-8: EASTERN GREENWAY CONNECTOR

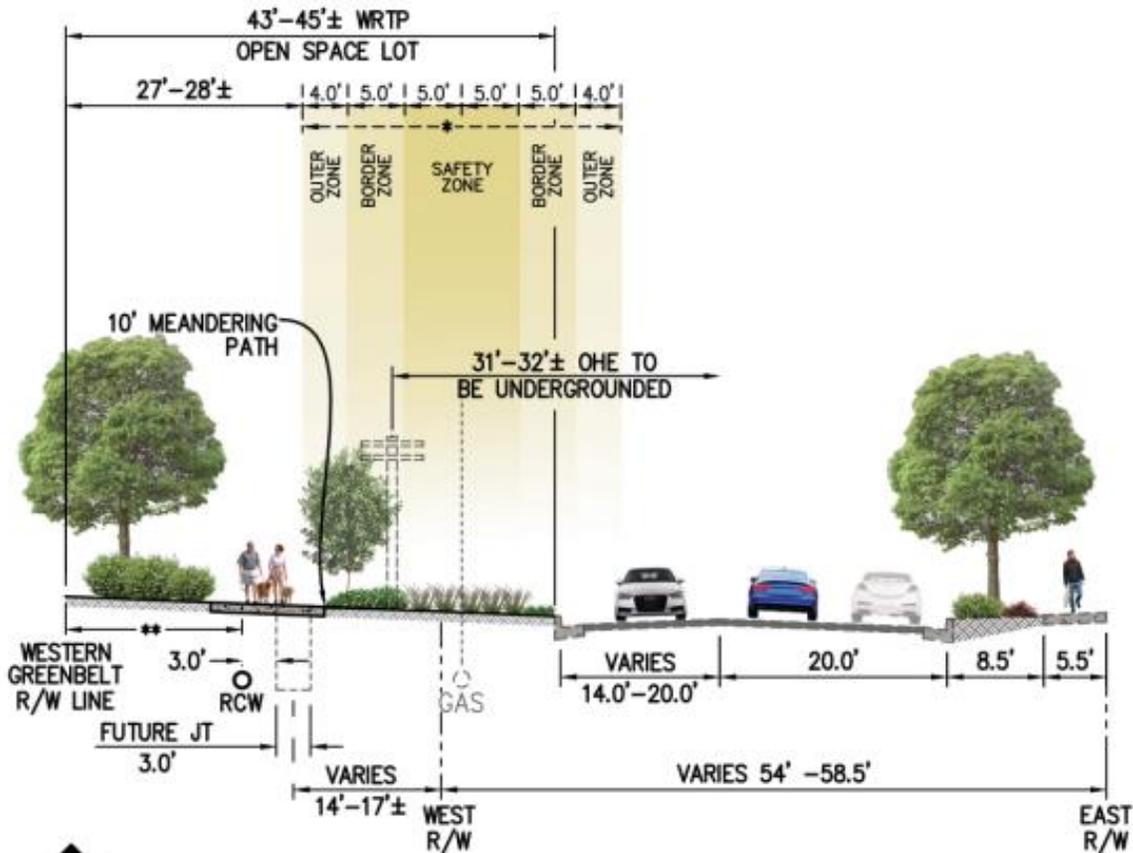


HARRY LORENZO AVENUE ADJACENT TO HEIDRICK RANCH, PH 2 & 3

- * PG&E RESTRICTED PLANTING ZONES
- ** IT IS THE INTENT OF THE SPECIFIC PLAN TO PROVIDE 22' CLEAR FROM THE WESTERNMOST UTILITY TO THE WESTERN GREENBELT PROPERTY LINE. IF THE JOINT TRENCH IS PLACED WEST OF THE RECYCLED WATER, THEN THE SIZE OF THE GREENBELT SHALL BE INCREASED TO PROVIDE 22' CLEAR DISTANCE.

LEGEND:

SAFETY ZONE	LAWNS, FLOWERS, LOW-PROFILE GRASSES AND LOW-GROWING HERBACEOUS PLANTS.
BORDER ZONE	SMALL TO MEDIUM SHRUBS WITH TRUNKS OR MAIN BRANCH LESS THAN 8" IN DIAMETER AT FULL MATURITY. DIAMETER IS MEASURED FOUR AND A HALF FEET FROM THE GROUND.
OUTER ZONE	LARGE SHRUBS AND SMALL TREES WITH A TRUNK LESS THAN 36" AT FULL MATURITY. DIAMETER IS MEASURED FOUR AND A HALF FEET FROM THE GROUND.



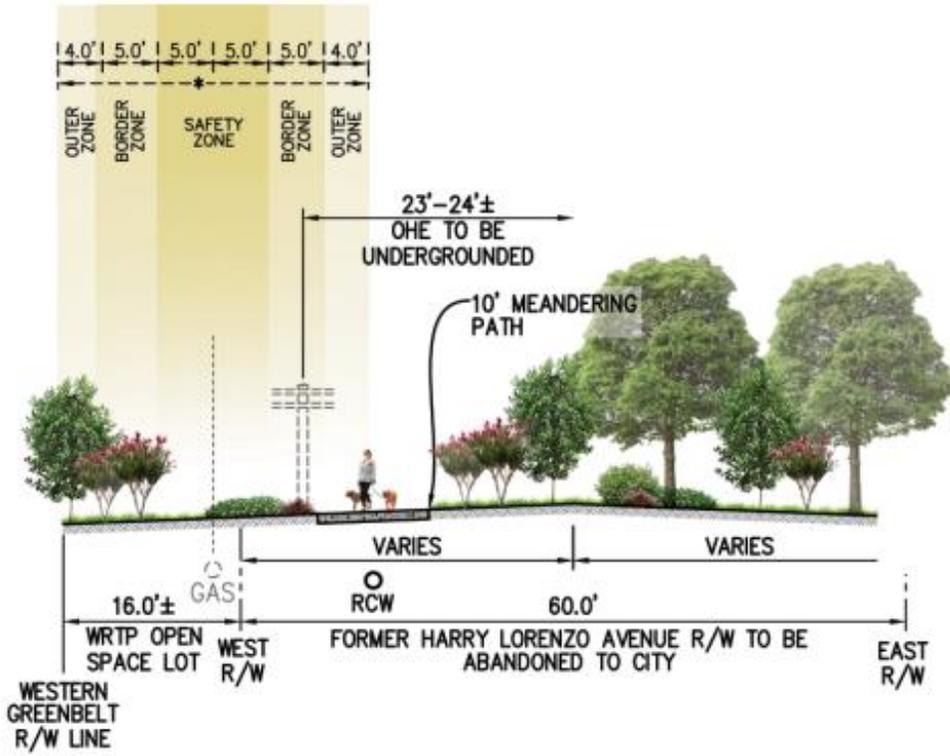
HARRY LORENZO AVENUE

ADJACENT TO SPRING LAKE CENTRAL - PHASE 3

- * PG&E RESTRICTED PLANTING ZONES
- ** IT IS THE INTENT OF THE SPECIFIC PLAN TO PROVIDE 22' CLEAR FROM THE WESTERNMOST UTILITY TO THE WESTERN GREENBELT PROPERTY LINE. IF THE JOINT TRENCH IS PLACED WEST OF THE RCW, THEN THE SIZE OF THE GREENBELT SHALL BE INCREASED TO PROVIDE 22' CLEAR DISTANCE.

LEGEND:

- SAFETY ZONE** LAWNS, FLOWERS, LOW-PROFILE GRASSES AND LOW-GROWING HERBACEOUS PLANTS.
- BORDER ZONE** SMALL TO MEDIUM SHRUBS WITH TRUNKS OR MAIN BRANCH LESS THAN 8" IN DIAMETER AT FULL MATURITY. DIAMETER IS MEASURED FOUR AND A HALF FEET FROM THE GROUND.
- OUTER ZONE** LARGE SHRUBS AND SMALL TREES WITH A TRUNK LESS THAN 36" AT FULL MATURITY. DIAMETER IS MEASURED FOUR AND A HALF FEET FROM THE GROUND.



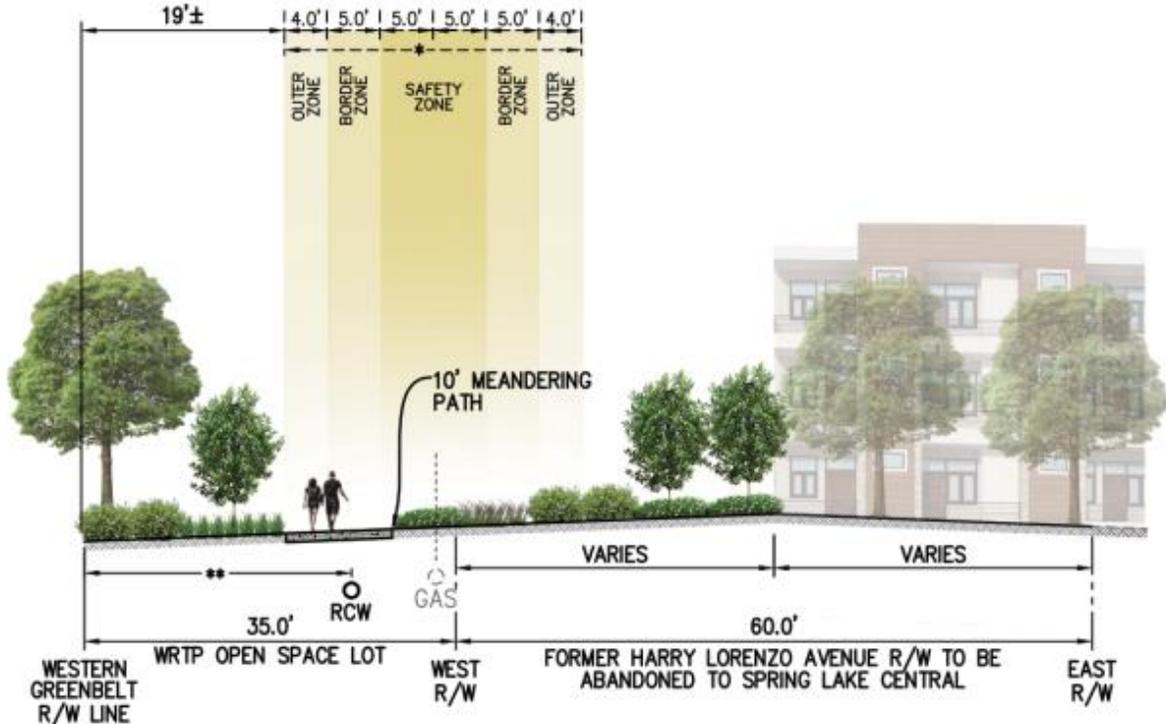
HARRY LORENZO AVENUE

ADJACENT TO SPRING LAKE CENTRAL PARK

* PG&E RESTRICTED PLANTING ZONES

LEGEND:

- SAFETY ZONE** LAWNS, FLOWERS, LOW-PROFILE GRASSES AND LOW-GROWING HERBACEOUS PLANTS.
- BORDER ZONE** SMALL TO MEDIUM SHRUBS WITH TRUNKS OR MAIN BRANCH LESS THAN 8" IN DIAMETER AT FULL MATURITY. DIAMETER IS MEASURED FOUR AND A HALF FEET FROM THE GROUND.
- OUTER ZONE** LARGE SHRUBS AND SMALL TREES WITH A TRUNK LESS THAN 36" AT FULL MATURITY. DIAMETER IS MEASURED FOUR AND A HALF FEET FROM THE GROUND.



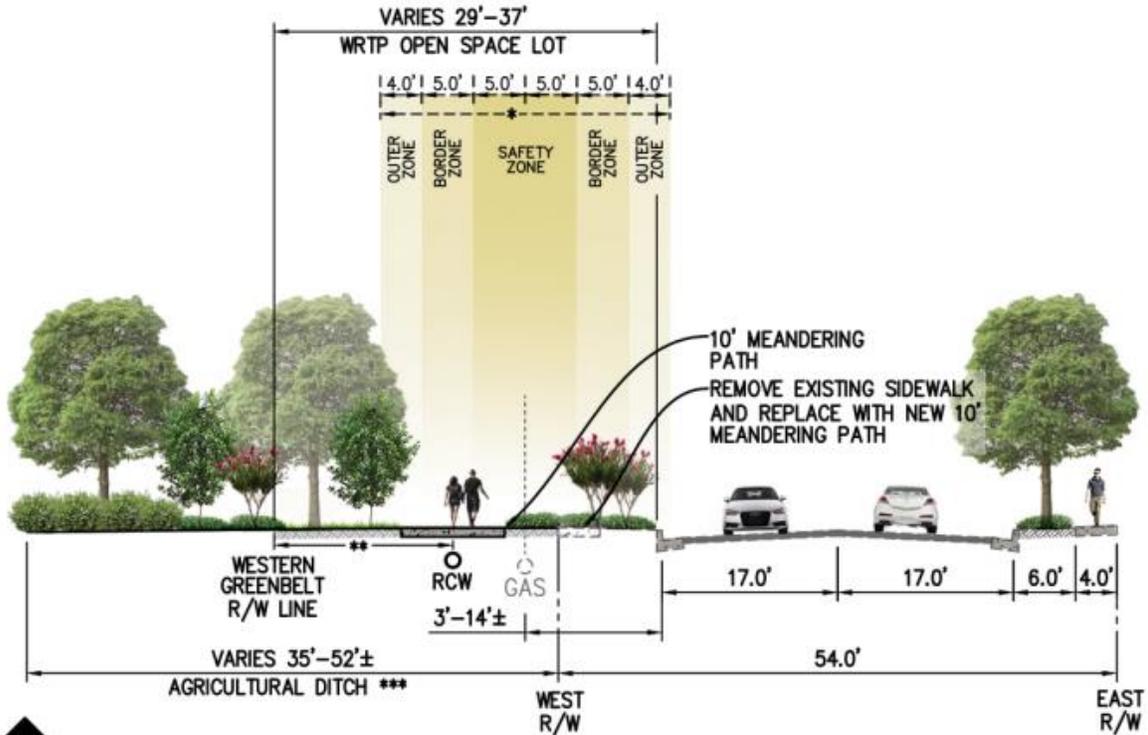
HARRY LORENZO AVENUE

ADJACENT TO SPRING LAKE CENTRAL MULTI-FAMILY SITE

- * PG&E RESTRICTED PLANTING ZONES
- ** IF JOINT TRENCH IS EXTENDED IN THE GREENBELT IN THIS CROSS SECTION THEN THE WIDTH OF THE GREENBELT SHALL BE INCREASED TO PROVIDE 25' CLEAR FROM THE WESTERMOST UTILITY TO THE GREENBELT PROPERTY LINE.

LEGEND:

SAFETY ZONE	LAWNS, FLOWERS, LOW-PROFILE GRASSES AND LOW-GROWING HERBACEOUS PLANTS.
BORDER ZONE	SMALL TO MEDIUM SHRUBS WITH TRUNKS OR MAIN BRANCH LESS THAN 8" IN DIAMETER AT FULL MATURITY. DIAMETER IS MEASURED FOUR AND A HALF FEET FROM THE GROUND.
OUTER ZONE	LARGE SHRUBS AND SMALL TREES WITH A TRUNK LESS THAN 36" AT FULL MATURITY. DIAMETER IS MEASURED FOUR AND A HALF FEET FROM THE GROUND.

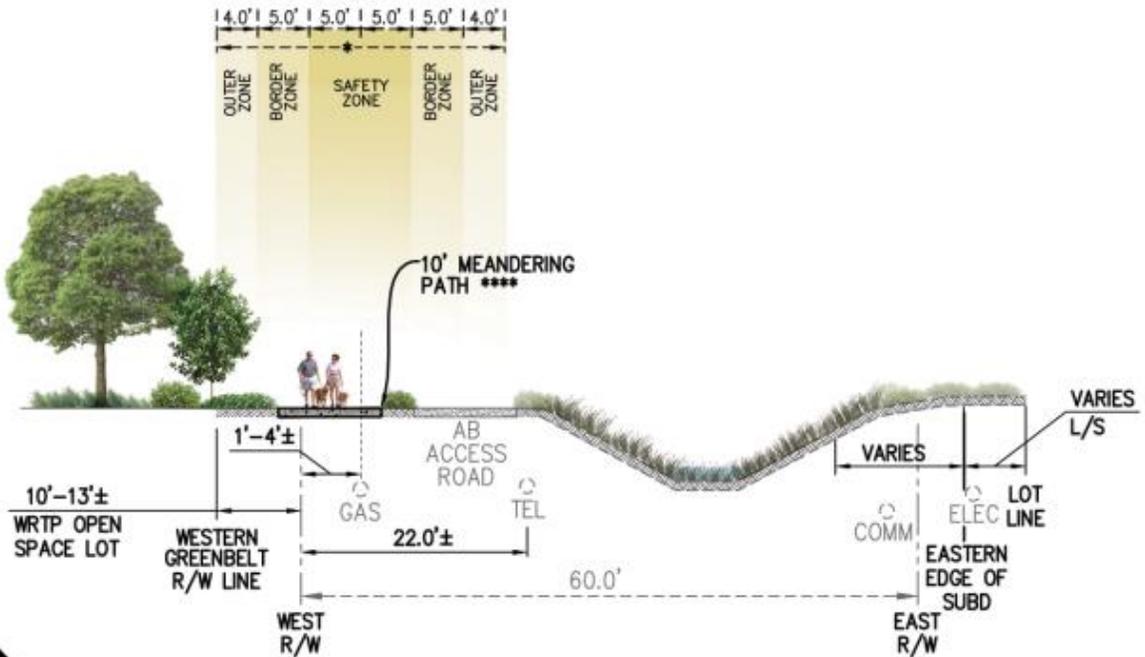


EX. HARRY LORENZO AVENUE ADJACENT TO SOLARA RANCH

- * PG&E RESTRICTED PLANTING ZONES
- ** IF JOINT TRENCH IS EXTENDED IN THE GREENBELT IN THIS CROSS SECTION THEN THE WIDTH OF THE GREENBELT SHALL BE INCREASED TO PROVIDE 25' CLEAR FROM THE WESTERNMOST UTILITY TO THE GREENBELT PROPERTY LINE.
- *** WITH CONSTRUCTION OF GREENBELT, DITCH SHALL BE FILLED IN AND STORMWATER TO BE REROUTED OUTSIDE OF GREENBELT.

LEGEND:

SAFETY ZONE	LAWNS, FLOWERS, LOW-PROFILE GRASSES AND LOW-GROWING HERBACEOUS PLANTS.
BORDER ZONE	SMALL TO MEDIUM SHRUBS WITH TRUNKS OR MAIN BRANCH LESS THAN 8" IN DIAMETER AT FULL MATURITY. DIAMETER IS MEASURED FOUR AND A HALF FEET FROM THE GROUND.
OUTER ZONE	LARGE SHRUBS AND SMALL TREES WITH A TRUNK LESS THAN 36" AT FULL MATURITY. DIAMETER IS MEASURED FOUR AND A HALF FEET FROM THE GROUND.



EX. HARRY LORENZO AVENUE ADJACENT TO OYANG SOUTH

- * PG&E RESTRICTED PLANTING ZONES
- *** WITH CONSTRUCTION OF GREENBELT, DITCH SHALL BE FILLED IN AND STORMWATER TO BE REROUTED OUTSIDE OF GREENBELT.
- **** PATHWAY SHALL CONNECT TO CUL-DE-SAC PATHWAYS IN OYANG SOUTH.

- NOTES: 1) GREENBELT IMPROVEMENTS EAST OF THE EXISTING WESTERLY R/W WILL BE CONSTRUCTED BY WRTP WITH REIMBURSEMENT FROM SPRING LAKE.
 2) WRTP OPEN SPACE LOT BASED ON PROVIDING 14' FROM GAS LINE TO WESTERN R/W LINE.

LEGEND:

SAFETY ZONE	LAWNS, FLOWERS, LOW-PROFILE GRASSES AND LOW-GROWING HERBACEOUS PLANTS.
BORDER ZONE	SMALL TO MEDIUM SHRUBS WITH TRUNKS OR MAIN BRANCH LESS THAN 8" IN DIAMETER AT FULL MATURITY. DIAMETER IS MEASURED FOUR AND A HALF FEET FROM THE GROUND.
OUTER ZONE	LARGE SHRUBS AND SMALL TREES WITH A TRUNK LESS THAN 36" AT FULL MATURITY. DIAMETER IS MEASURED FOUR AND A HALF FEET FROM THE GROUND.



Table 5.6 Park/Open Space Provided		
Parks	Zone	Acres
Neighborhood Park- The Yard	VCOS	10.8
The Promenade*	VCOS	0.8
SUBTOTAL		11.6
Greenbelts	Zone	Acres
Greenbelts	OS	6.0
SUBTOTAL		6.0
TOTAL PARKS/GREEN BELT ACRES PROVIDED		17.6

*Designated as Pedestrian Promenade on the Land Use Plan Layout to denote specific location of hardscape. Designated herein as VCOS because it is part of The Yard.

The Specific Plan will meet its remaining park obligation through the payment of project impact fees.

5.12 | SCHOOLS

The Woodland Joint Unified School District (WJUSD) provides public education from kindergarten through 12th grade within Woodland, as well as nearby unincorporated areas of Knight's Landing, Yolo, and Zamora. The District includes 11 elementary schools, one charter elementary school, two middle schools, two comprehensive senior high schools for grades 9-12, and one continuation high school. Additionally, there are three alternative education programs, six pre-schools, three infant-toddler programs, and an adult education center.

The Plan Area is located in close proximity to the Woodland Community College, at 2300 East Gibson Road, within the Spring Lake Specific Plan Area. The Community College has been at this location since 1990. The Community College offers associate degrees and continuing education certificates in Agriculture and Agricultural Engineering/Technologies; Advanced Technology, Manufacturing, Clean Economy, Food Production clusters, Business Administration logistics, Culinary Arts, Elementary Education and Health Care.

Table 5.7 shows the estimated number of students to be generated from the Plan Area. The WJUSD will evaluate the attendance boundaries of existing and planned schools in the Plan Area to determine the school locations for Plan Area students. Existing schools in the Plan Area vicinity are Pioneer High School and Spring Lake Elementary School.

WJUSD owns a 20-acre Future School Site located directly south of Pioneer High School, approximately one-half mile from the Plan Area. The City and District have entered into an agreement to allocate 2.5 acres of this vacant property for the purpose of the City constructing

and operating a second community swimming pool and aquatic facility. WJUSD has requested that the Specific Plan identify an alternative site for an elementary school, should it be needed. Up to a 12-acre portion of the MDR parcel located at the northeast corner of Road B and Road E will be reserved for a potential school site, in accordance with the terms of the development agreement. The primary vehicular access to the school will be on Road E. No vehicular access to the school will be allowed on Parkland Avenue.

Table 5.7 Estimated Student Generation/School Site Demands							
Land Use	Estimated D.U.	K-6 Factor	Total K-6	7-8 Factor	Total 7-8	9-12 Factor	Total 9-12
Singe Family	1,100	0.2233	246	0.0619	68	0.1413	155
Multi Family	500	0.2596	130	0.0711	36	0.1342	67
Total units	1,600						
Total students			376		104		222

Source: Residential and Commercial/Industrial Development School Fee Justification Study for the Woodland Joint Unified School District, by Cooperative Strategies, April 6, 2020

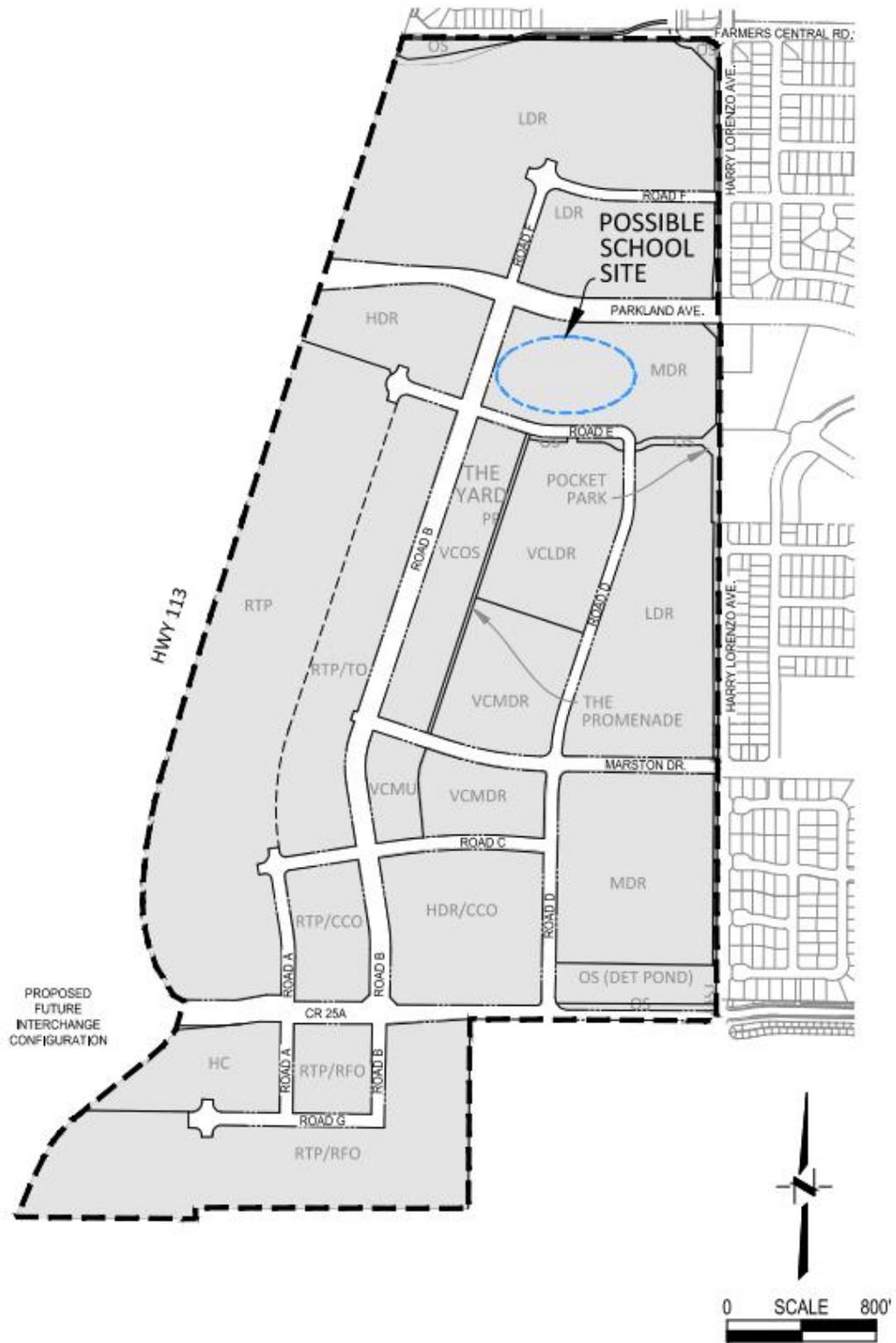


EXHIBIT 5-9: POSSIBLE SCHOOL SITE

5.13 | LIBRARY SERVICES

Library services are provided to the City of Woodland through the Yolo County Library System. The Woodland Public library is in central Woodland at 250 First Street. This facility will provide library access, including expanded virtual programming, youth activities, and e-book check out for Plan Area residents.

5.14 | OTHER GOVERNMENT SERVICES

City governmental services (e.g. planning department, building inspection, public works administration, city manager and city council, etc.) are provided primarily at City Hall at 300 First Street in downtown Woodland. As population increases so do the demands on these general government services. These services are paid for primarily from discretionary funds in the City's general fund and user fees for development related services.

5.15 | SOLID WASTE

Solid waste and yard waste collection and disposal and curbside recycling in Woodland are provided by a franchise agreement with Waste Management, with disposal and material processing at Yolo County Central Landfill. The General Plan policies ensure a consistent, citywide level of service for refuse collection, neighborhood cleanup, sanitization enforcement, and recycling programs. Existing waste disposal facilities are adequate to maintain a sufficient level of service for Specific Plan build out.



6.0 | IMPLEMENTATION





6.1 | OVERVIEW

California Government Code Section 65451 requires that Specific Plans include programs of implementation strategies related to regulatory changes, programs, financing strategies and public works projects needed to carry out the proposed land use, infrastructure and development standards outlined in the Specific Plan.

This chapter provides a summary of the methods by which the Specific Plan will be implemented and provides discussion on the sequencing and financing programs that demonstrate compliance with General Plan policies and the vision, goals and policies of the Specific Plan.

The goal of this chapter is to describe how infrastructure and public facilities will be constructed and how public services will be delivered in a timely manner. The purpose of the implementation procedures is to ensure orderly development of the Plan Area over a multi-year time frame in coordination with the provision of the necessary infrastructure and services and with sufficient flexibility to respond to fluctuations in the economy, market demand, and evolving innovation in transportation opportunities. To achieve this intent, the Specific Plan includes a conceptual program for development sequencing and financing of infrastructure to support development in logical increments. Implementation of the Specific Plan will occur in accordance with the terms and conditions of several related planning and program documents, including the Development Agreement, Environmental Impact Report and associated Mitigation Monitoring Program, Financing Plan, Nexus Study, Public Infrastructure Reimbursement Rules and the Woodland General Plan.

6.2 | IMPLEMENTATION AND PROGRAMMATIC STEPS

The Specific Plan provides for a comprehensively planned infrastructure system with the coordinated extension of infrastructure and the construction of facilities. In order to achieve the vision and goals of the Specific Plan, a number of Technical Memorandums were prepared in conjunction with or prior to adoption of the Specific Plan, and others shall be prepared and adopted subsequent to Specific Plan adoption.

6.2.1 Background on Pre-Specific Plan Actions

The City of Woodland adopted its General Plan on May 16, 2017, which identified the Plan Area as New Growth Area, SP-1A. The Plan Area is specifically envisioned within the General Plan as a natural extension of the city, ideally located and positioned for jobs and housing development. Subsequently, as part of the implementation program for the General Plan, the City petitioned to and LAFCo has amended Woodland's Sphere of Influence (SOI) to include all land within the Urban Limit Line (which includes the Plan Area) and negotiated a tax sharing agreement with Yolo County

that addresses anticipated annexation of various lands including SP-1A, now known as the Woodland Research and Technology Park (WRTP). The City has also updated its Major Projects Financing Plan, adopted an interim zoning ordinance, revised the Affordable Housing Ordinance, approved the Agricultural Mitigation Ordinance, and joined the Yolo Habitat Conservation Plan/Natural Community Conservation Plan. These actions provide the foundation for implementation of the Specific Plan.

The Plan Area is located directly adjacent to the Spring Lake Specific Plan, abutting along the entire eastern edge. In anticipation of future development of the WRTP Plan Area, connection points for all municipal utilities, streets and bicycle and pedestrian connections were provided as part of the overall development for the neighboring Spring Lake Specific Plan. These connection points are stubbed at the east and north boundaries of the Plan Area.

6.2.2 Plan Area Infrastructure Planning

The coordinated delivery of public services is essential to ensure a complete community, one that provides public safety, recreational, educational and library services for the residents. The proposed public utility and public service providers are identified in Chapter 5, Public Utilities and Services. Technical Memorandums, listed below, provide the additional detail for the identified infrastructure improvements to support Plan Area development. Chapter 4, Mobility and Circulation, describes the comprehensive transportation network featuring a hierarchy of complete streets and off street bike paths. These studies may need to be updated or future studies may be prepared to inform the successful buildout of the Specific Plan, as required. Specifically, timing for certain off-site infrastructure components will need to be monitored to ensure timely completion of those facilities/improvements and adequate capacity at all stages of development.

The WRTP Infrastructure Technical Memorandums include:

- Water Technical Memorandum dated January 7, 2020.
- Reclaimed Water Distribution Technical Memorandum dated March 31 2020.
- Wastewater Collection System Technical Memorandum dated March 31, 2020.
- Stormwater Management Technical Memorandum dated January 31, 2020.
- Traffic Impact Analysis completed for the DEIR dated April 2021.

A Public Facilities Financing Plan (Appendix A-1), which presents a strategy to finance backbone infrastructure and other public facilities required to serve the proposed land uses in the Plan Area was prepared by Economic & Planning Systems, Inc. (EPS). The financing plan demonstrates how the infrastructure requirements and the associated costs are reasonably balanced throughout

each segment of development and that the provision of services, as well as the operations and maintenance costs, are in sync as development progresses. The Public Facilities Financing Plan is described in Section 6.4.

6.2.3 Subsequent Implementation Documents/Analysis

The following supplemental plans must be prepared either prior to approval of the first development application or residential tentative map or as otherwise required by the conditions of approval and/or Environmental Impact Report and associated Mitigation Monitoring Program, or by the Community Development Director, as appropriate:

- Master Plan for The Yard to guide phased park improvements, and ensure pedestrian and bike paths internal to the park connect to the external network trail and greenbelt system. Master Plan for The Yard to include, at minimum: farmer's market pavilion including electrical power; hardscape plaza/bandshell for entertainment/events including electrical power; opportunities for placement of public art; emphasis on pollinator and native landscaping, consideration of native/pollinator learning garden; consideration of Native Valley Oak tree planting; multipurpose court(s); tot lot and older children playground; covered/shaded seating areas throughout; restroom facility(ies); trails (DG and concrete) throughout; pedestrian overhead lighting throughout (bollard lights where appropriate); bike racks; bike repair station. Development application for a Business Incubator or affordable (income restricted) high-density residential project associated with land dedication sites, per Development Agreement stipulations, are exempt from this requirement.
- Comprehensive planting and street tree palette shall be developed for each District to guide both private and public landscaping improvements. Street Furnishings and Lighting Palettes, and the design of enhanced crossings, roundabouts/traffic circles and Gateway monuments/enhanced landscape design shall be included as part of this effort. Gateways shall be located at CR25A and Road B, and Road B and Marston Drive and shall include enhanced landscaping, entryway illuminated signage/monument, public art and/or other decorative element(s) that reflect the agricultural and/or life science focus of the Research and Technology Park and create an identifiable "gateway" to both the north and south RTP campus. Corner gateway features shall at minimum be 3,500 square feet unless otherwise approved by Community Development Director. Development application for a Business Incubator or affordable (income restricted) high-density residential project associated with land dedication sites, per Development Agreement stipulations, are exempt from this requirement.

- Comprehensive Transportation Demand Management/Vehicle Miles Traveled Reduction Program (“TDM/VMT Program”). The Master TDM/VMT Program shall: 1) establish transportation strategies, programs, facilities or services for the purpose of VMT reduction that are financed by and consistent with the strategies and requirements of the Development Agreement;¹ and 2) provide project specific VMT reduction strategies that all property owners/tenants shall be required to implement through individual Project-level TDM Plans consistent with the Master TDM Program. These measures shall in combination achieve a 10 percent reduction in Plan Area VMT per capita compared to baseline conditions by 2035. The Master TDM/VMT Program will include a monitoring plan for collecting VMT data in the interim years to 2035, every five years as input to citywide GHG monitoring, so that the effectiveness of the VMT reduction strategies can be confirmed and any required strategy adjustments made to reach project VMT reduction targets. Monitoring reports shall be reviewed by the City who may make adjustments to reach project VMT reduction targets, as necessary. Development application for a Business Incubator or affordable (income restricted) high-density residential project associated with land dedication sites, per Development Agreement stipulations, are exempt from the Master TDM/VMT Program development obligations as set forth in this paragraph, but are not exempt from the 10 percent VMT reduction obligations at the project level if the Master Program has yet to be established.
- Coordination with the Yolo County Transportation District, Yolobus and UC Davis with policies of the Specific Plan to insure timely provision of transit service and appropriate funding mechanisms in place.
- Enhanced Climate Action Plan Checklist, to achieve WRTP energy efficiency and sustainable design goals and to achieve consistency with citywide and 2035 General Plan Greenhouse Gas Reduction targets.

6.3 | INFRASTRUCTURE SEQUENCING

The City of Woodland General Plan has a strong focus on economic development for the future of Woodland, in order to achieve sustainable development through continued jobs/housing balance in the community. The Woodland Research and Technology Park is a key component in providing the means and opportunity to achieve this goal. Facilitation of early or “first-in”

¹ / In order to specify the manner in which the necessary infrastructure, public facilities, and other programs or services as provided in this Specific Plan will be constructed and/or operated and financed, among other matters, the City and the project applicant intend to enter a development agreement. The terms and conditions of the development agreement will be consistent with the goals and policies of this Specific Plan and shall set forth and require financing strategies, sources, and mechanisms to ensure short-term and long-term funding for implementation and monitoring of the TDM/VMT measures.

development of the Research and Technology Park (RTP) parcels is a key strategy of the Plan Area implementation, as described in the Financing Plan and the Development Agreement.

This section describes, rather than dictates, how connections to existing circulation and infrastructure improvements can physically be provided to serve the various areas of the Specific Plan. Actual extension of infrastructure and development within the Specific Plan will be determined by factors such as market demand, financial feasibility, infrastructure constraints, project Development Agreement terms, and City policy relating to jobs-housing goals. In general, development sequencing strategy is structured to advance the backbone infrastructure improvements needed to support associated development, in compliance with City policies and standards, and to ensure that the development area can support the costs of the required improvements.

6.3.1 Sequencing Approach

The overarching approach to development of the Plan Area is measured flexibility. From a physical standpoint, the Plan Area can begin development in any geographic area of the Plan Area and areas can be developed simultaneously. Development may move forward independently provided that parcels can meet the public services requirements, the guiding policies and development standards outlined in the Specific Plan, the development agreement and are to the satisfaction of the City of Woodland Community Development Department. All public utilities were stubbed in multiple connection points along the eastern Plan Area boundary with the adjacent Spring Lake development and were sized with capacity to accommodate the Plan Area utility demands. The Plan Area has the ability to develop in sequential increments yet provides some flexibility for individual development areas to progress independently.

In addition to the planned infrastructure connection points, there are additional offsite infrastructure components which require ongoing monitoring and evaluation to determine triggers for the timing of improvements. Timing for these off-site infrastructure components will need to be monitored to ensure that adequate capacity is available. These include wastewater and stormwater improvements, described in Sections 5.4.3 and 5.5.3 respectively, and the future improvements to the Hwy 113/25A interchange.

Traffic Impact Analysis prepared for the Project indicates State Route 113 and Road 25A interchange will ultimately require improvements prior to full build-out of the Plan Area. Subsequent development projects within the Plan Area shall be required to prepare a project specific traffic analysis to determine the exact timing for construction of the interchange to the satisfaction of the Community Development Director or City Engineer, in consultation with the State of California Transportation Department.

6.3.2 Sequencing Principles and Policies

Infrastructure requirements for each phase of development include on-site backbone infrastructure and off-site facilities necessary for each phase to proceed. Each improved area shall include roadway, sanitary sewer, water, recycled water, storm drainage, dry utilities, other facilities and improvements. All in-tract sewer, storm drain, water and dry utilities will be installed as part of individual project improvements. The listing and specific details relating to the on-site and off-site improvements required to serve the Plan Area are included in the infrastructure technical memorandums for traffic, water, reclaimed water, wastewater and drainage and in the Specific Plan development agreement(s).

In some instances, selected off-site infrastructure items, roads, and/or advancement of public services may be needed to serve a project area, while some items may be deferred to a later project area to develop. The general sequencing policies are as follows:

- At a minimum, two points of vehicular access are required for each project area. One of the accesses may be temporary emergency vehicle access subject to the approval of the Community Development Director.
- A looped water system is required for each project area.
- All roadways, pedestrian facilities, and bicycle routes shall be constructed in logical segments to provide safe and adequate access with each project area.
- Improvements constructed along the boundary of a project area shall include utility stubs to the adjacent phase for subsequent connection by the future area(s).
- Development of a non-contiguous project area will require the construction of backbone utility infrastructure not contained within defined area in order to connect to existing "downstream" infrastructure per the routing shown in the infrastructure technical memorandums. These improvements will be subject to reimbursements and fair share costs as outlined in the Financing Plan, Development Agreements and Public Infrastructure Reimbursement Rules.
- A requirement to grant easements and receive reimbursement for on-site roadway and utility infrastructure required for a non-contiguous area shall be included in the Financing Plan, Public Infrastructure Reimbursement Rules and Development Agreements for the Specific Plan.

6.4 | FINANCING AND MAINTENANCE OF PUBLIC FACILITIES

The construction of public facility improvements to serve the Plan Area will be funded by a variety of mechanisms. Specific financing requirements, improvement obligations, fees, reimbursements, land and easement dedications and conveyances, maintenance, and other financing and

improvement related obligations are detailed in the development agreements, Public Infrastructure Reimbursement Rules, Financing Plan and Nexus Study.

Funding of public improvements necessary to implement the Specific Plan will include a variety of mechanisms including city and school impact fees, Specific Plan Infrastructure Fee and other potential methods. A separate Financing Plan has been prepared for the Woodland Research and Technology Park that details the costs for construction and operation of backbone infrastructure, public facilities, and operational costs necessary for development anticipated under the Specific Plan. New development within the Plan Area shall be fiscally and financially neutral, both in terms of capital costs (financing of infrastructure) and ongoing costs (generation of sufficient general fund revenue to support ongoing maintenance and operational costs) to the greatest extent feasible.

The Financing Plan will serve as a framework to guide and support the objectives of the Specific Plan. As development progresses, the timing and mix of cost and funding sources may change. The assumptions and results are estimates at this time. Regardless of the extent to which certain financing mechanisms are used or funding sources are available, the overall cost burden has been calculated for the purpose of determining the most appropriate and feasible financing strategies and mechanisms to proceed with development under the Specific Plan. Specifically, the Financing Plan, along with the Development Agreements, Public Infrastructure Reimbursement Rules and Nexus Study, will:

- Describe the project and sequencing of needed infrastructure.
- Provide a summary of the infrastructure and public facility requirements to serve future development within the Plan Area.
- Include infrastructure cost estimates by land use and by development phase per acre, and the estimated infrastructure cost at build-out of the Plan Area.
- Include summarized costs for infrastructure and public facilities needed to serve the Specific Plan, including costs needed to serve the Plan Area at buildout.
- Review and include existing fees, taxes and assessments which may be applicable to the Specific Plan.
- Present cost estimates for operating and maintaining the required infrastructure and for ongoing municipal services.
- Identify potential funding sources for both the construction of infrastructure and provision of municipal services,
- Present the overall cost burden by land use and by development area on a per acre basis.
- Provide recommended action steps for implementation of the infrastructure financing.

6.5 | DENSITY TRANSFER

To ensure project consistency with Specific Plan environmental analysis, General Plan development assumptions and with project financing objectives, the following Density Transfer provisions shall apply to residential area buildout. The following Density Transfer provisions ensure diversity of housing types and densities while maintaining consistency with the above-mentioned objectives including the anticipated development of 1,600 residential units and a minimum average gross residential density of eight dwelling units per acre (8 du/ac).

6.5.1 Assumptions

The following assumptions underlie the Density Transfer provisions:

- 1,600± residential units are anticipated at plan area buildout and assumed for CEQA environmental analysis purposes, unless modified by subsequent CEQA analysis
- At buildout, the residential component of the plan area shall provide a minimum average density of 8 du/ac.
- Twelve acres reserved for a possible future school site are not allocated residential units; those units are reallocated throughout the remaining residential zone(s).
- If it is determined that a school site is not needed within the Specific Plan area, a point in time analysis shall be conducted to reallocate units to the twelve-acre reservation site. Under this condition, the developer may be required to perform additional environmental analysis.
- No residential units shall be assigned to the Village Center Mixed Use zone or Community Commercial Overlay zone. Development of residential units in the VCMU and/or CCO zones may not have to pay Specific Plan fees if they meet the requirements of the development agreements (when residential units are constructed above ground floor retail and financial obligations of the Residential Development Zone, described below, are met).

6.5.2 Density Transfer Provisions

The following provisions guide unit yield assumptions within Residential Development Zones ("RDZ") and the process by which residential densities are established and, if permitted by the Community Development Director, transferred to other RDZ's.

- Residential unit yields within each of six RDZs shall be those established by the following Exhibit 6-1 and Table 6.1. (RDZ boundaries generally follow property lines as existed at the time of plan development.)

- When any portion or all of an RDZ is mapped/subdivided (large or small lot), unit yields/densities shall be determined and allocated across the entire RDZ effected, and shall meet the Density Requirements outlined in Section 6.5.3 below.
- Owner/Builder may develop up to the assigned number of units within the RDZ at densities permitted by the Specific Plan for the underlying zoning (LDR, MDR, HDR).
- Owner/Builder may request an increase in units up to 10 percent above the assigned units within the RDZ, subject to CEQA consistency review.
- If an RDZ is developed *under* the assigned number of units, a Specific Plan fee make up shall be required, unless at time of tentative map, an alternate RDZ picks up the unit deficit, subject to City approval and provided that the owner of the property located outside the RDZ enters into a written agreement to the satisfaction of the City that it shall adjust the density of its property to ensure consistency with the overall residential yields for the Specific Plan Area.
- If an RDZ is developed *over* assigned units, overage units must be removed from an alternate RDZ at tentative map, subject to City approval and provided that the owner of the property located outside the RDZ enters into a written agreement to the satisfaction of the City that it shall adjust the density of its property to ensure consistency with the overall residential yields for the Specific Plan Area.
- City has sole authority to reallocate units or to add units within each RDZ.
- If a future applicant/developer proposes land use changes or an increase in the number of units, then the City will reallocate units or add units within each RDZ and fees shall be updated with a nexus study.

LEGEND

LDR	LOW DENSITY RESIDENTIAL
VCLDR	VILLAGE CENTER LOW DENSITY RESIDENTIAL
MDR	MEDIUM DENSITY RESIDENTIAL
VCMDR	VILLAGE CENTER MEDIUM DENSITY RESIDENTIAL
HDR	HIGH DENSITY RESIDENTIAL
HDR/CCO	HIGH DENSITY RESIDENTIAL WITH COMMUNITY COMMERCIAL OVERLAY
VCMU	VILLAGE CENTER MIXED USE
RTP	RESEARCH & TECHNOLOGY PARK
RTP/TO	RESEARCH & TECHNOLOGY PARK WITH TRANSITIONAL OVERLAY
RTP/CCO	RESEARCH & TECHNOLOGY PARK WITH COMMUNITY COMMERCIAL OVERLAY
RTP/RFO	RESEARCH & TECHNOLOGY PARK WITH RESEARCH FLEX OVERLAY
HC	HIGHWAY COMMERCIAL
OS	GREENBELTS, OPEN SPACE
PP	PEDESTRIAN PROMENADE
VCOS	VILLAGE CENTER OPEN SPACE
DET POND	DETENTION POND

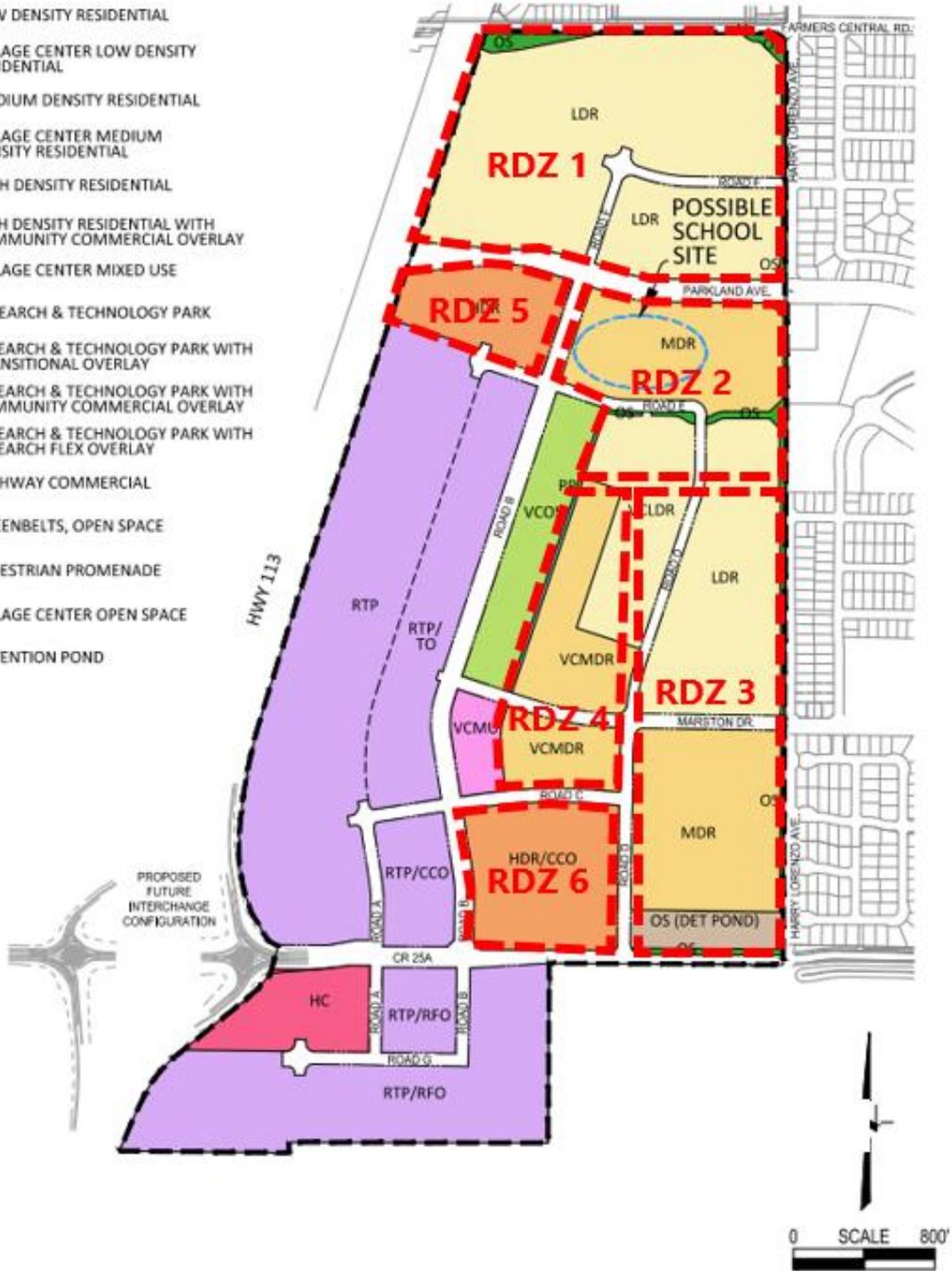


EXHIBIT 6-1: RESIDENTIAL DEVELOPMENT ZONES

Table 6.1 Density Transfer					
RDZ	Acreage	Total Unit Yield	LDR Unit Yield	MDR Unit Yield	HDR Unit Yield
1	52.29	366	366		
2	25.07	140	71	69	
3	42.19	398	161	237	
4	19.44	248	30	218	
5	9.67	232			232
6	11.91	286			286

6.5.3 RDZ Density Requirements

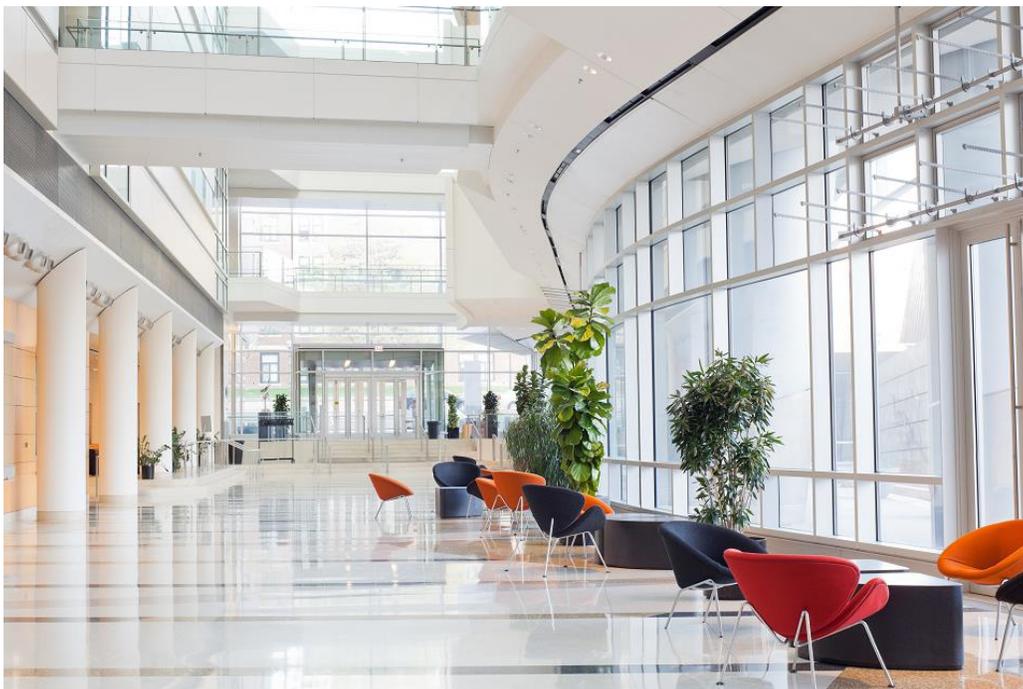
The following Density Requirements guide development density within each RDZ:

- Within RDZ 3, Village District East, MDR Zone, units fronting Marston Drive shall be developed at or above 15 du/ac, consistent with a townhome or row house product type.
- Within RDZ 4, Village Center, VCMDR Zone, units fronting The Yard and fronting Marston Drive shall be developed at or above 15 du/ac, consistent with a townhome or row house product type
- Within each RDZ 5 and 6, Urban Village North and South, a minimum of 60 units (120 units total) shall be built at a minimum density of 30du/ac (exclusive of 2.5 acre land dedication sites in each area). This provision may be modified by City if land dedication sites build out at densities greater than 30 du/ac and City Housing Element objectives for the Plan Area are reached.

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7.0 | ADMINISTRATION





7.1 | OVERVIEW

This chapter outlines the Specific Plan process and its relationship to the subsequent entitlement processes that are necessary to allow construction of individual projects. This chapter also describes the administrative procedures that will occur to implement, amend, interpret and enforce the Specific Plan.

7.2 | SPECIFIC PLAN PROCESS AND RELATED ENTITLEMENTS

This section describes the project entitlements, the relationship between the concurrent approvals which occurred with the adoption of the Specific Plan and the subsequent entitlement process to occur after the WRTP Specific Plan adoption.

7.2.1 Specific Plan and Concurrent Approvals/Actions

Approval of the Specific Plan took place concurrent with the following entitlements and related actions:

1. Final Environmental Impact Report (FEIR)
2. Adoption of the Specific Plan
3. Approval of a General Plan Amendment and Pre Zoning
4. LAFCo Approval and Annexation
5. Approval of Development Agreements by and between the City and the landowners.
6. Approval of a Public Facilities Financing Plan and Nexus Study for the Specific Plan that describes a Capital Improvement Program and a funding and financing strategy for planned improvements.
7. Approval of Public Infrastructure Reimbursement Rules by and between the City and the landowners.

7.2.2 Subsequent Entitlements/Actions

Individual development projects are subject to review and approval of subsequent permits and entitlements by the City of Woodland and other agencies. Once the actions described in Section 7.2.1 have occurred, and all pre-development obligations as outlined in the Development Agreement or the Specific Plan have been satisfied unless otherwise permitted by the Community Development Director, landowners may submit and process applications for subsequent entitlements, as required, to implement the Specific Plan. Future approvals may include, but are not limited to:

1. Tentative Subdivision Maps
2. Tentative Parcel Maps

3. Lot Line Adjustments
4. Site Plan Review/Design Review
5. Use Permits
6. Variances

7.2.3 Environmental Clearance

An Environmental Impact Report (EIR) was prepared for this Specific Plan and was certified concurrent with the approval of the Specific Plan. The EIR serves as the base environmental document for subsequent entitlement approvals within the Plan Areas. Section 65457(a) of the California Government Code and Regulations Section 15182 and 15183 of Title 14 of the California Code of Regulations (pertaining to the California Environmental Quality Act (“CEQA”)) provide that no EIR or negative declaration is required for any residential project undertaken in conformity with an adopted Specific Plan for which an EIR has been certified. Moreover, no additional environmental review is required for projects that are consistent with the zoning for which an EIR was certified, except as might be necessary to examine whether there are project-specific effects that are peculiar to the project or its site. The EIR certified for this project has been written to qualify all future residential projects for these exemptions, assuming they are consistent with the adopted Specific Plan and fulfill all conditions and CEQA mitigation measures outlined in the EIR. Non-residential projects may also rely upon the EIR assuming they are consistent with the adopted Specific Plan and fulfill all conditions and CEQA mitigation measures.

The City will assess consistency of each development application within the Specific Plan area with the Environmental Review and determine whether additional review is required under CEQA Section 15183 and 15182(a), or whether no further review is required.

Additional CEQA review may be required in the following instances: substantial changes to the Specific Plan are proposed, substantial changes occur to the circumstances under which the project is being undertaken, or new information becomes available that could not have been known upon certification of the Environmental Review. A supplemental review or an addendum may be required if any of these circumstances ensue.

The Specific Plan allows for a range of densities and intensities of development consistent with the goals and policies of the 2035 General Plan. However, it is anticipated that the Specific Plan area at build out will realize approximately 1,600 residential units and 2.2 million square feet of non-residential uses including business park, light industrial, retail and hospitality related commercial uses. Similarly, the EIR for the WRTP Specific Plan anticipates and analyzed these yields for purposes of environmental impacts. Should development within the Specific Plan exceed the anticipated residential or commercial development yields as analyzed within the EIR, discretionary review of such project(s) shall be required. The project proponent may be required

to submit documentation substantiating said development is in conformance with the Specific Plan, EIR, its mitigation monitoring program, the General Plan, and Municipal Code, or a subsequent EIR may be required.

A mitigation monitoring program (Appendix A-2) has been adopted with the EIR in accordance with Public Resources Code 21081.6 to ensure implementation of EIR mitigation measures.

7.2.4 Specific Plan Findings

Approval of all land division, planning entitlements and building permits shall be contingent upon a determination of substantial compliance with the provisions of this Specific Plan, the City of Woodland General Plan and with applicable provisions of the Zoning and Subdivision Ordinances. All proposed development projects or subdivision applications shall demonstrate the following:

1. Consistency with the goals and policies of the Specific Plan and all applicable laws and regulations.
2. Consistency with the overall intensity of land uses as provided for in the Specific Plan
3. Consistency with the Specific Plan implementation strategy and availability of infrastructure to serve the development.
4. The project will not degrade services and/or facilities beyond the capacities approved by the Specific Plan.
5. Consistency with the project scope and impacts addressed in the Specific Plan EIR or that environmental impacts of the project are addressed in subsequent environment impact studies that may be required under the California Environmental Quality Act (CEQA).

7.3 | SUBSEQUENT ENTITLEMENTS

Following adoption of the Specific Plan, subsequent development and/or proposed land use, including tentative map applications, within the Plan Area will be reviewed for consistency and compliance with the Specific Plan and any other City regulations in effect at the time of development. Application and processing requirements shall be in accordance with the City's Zoning Ordinance and other regulations, unless otherwise modified by this Specific Plan. In acting to approve a project or permit, the City may impose conditions as are reasonably necessary to ensure that the project is in substantial compliance with the Specific Plan and all applicable plans and regulations.

7.4 | SPECIFIC PLAN ADMINISTRATION

The Specific Plan serves as an implementing tool of the Woodland 2035 General Plan, which provides the foundational elements, vision and direction for the Specific Plan. As an implementing

tool, the Specific Plan establishes zoning, land use and development standards for projects within the Plan Area boundary.

Upon adoption of the Specific Plan, all land use regulations, development standards, and design guidelines of this Specific Plan shall supersede those of the Zoning Ordinance and Community Design Standards. Where direction is not provided in this Specific Plan, the standards of the City's Zoning Ordinance shall apply including use permit, variance, public notice and hearing and appeals provisions.

The City's Subdivision Ordinance will regulate individual requests for land divisions within the Specific Plan Area, unless otherwise addressed herein. To the extent that a component or regulation of the Specific Plan differs from a requirement of the Ordinance, the Specific Plan will take precedence. Where the Specific Plan is silent or vague, the Subdivision Ordinance will be used for the purpose of interpretation, and/or directly applied as appropriate.

Unless otherwise approved as part of the Specific Plan, off-site improvements under the control of the City shall be subject to City of Woodland regulations and requirements in effect at the time the improvement plans are submitted. Improvements not under the control of the City (e.g. improvements to State highways) shall be subject to the regulations and requirements of the responsible agency.

7.4.1 Major Specific Plan Amendments, Minor/Administrative Modifications

During the long-term build out of the Plan Area, amendments to the adopted Specific Plan may be necessary because of changed circumstances. Additionally, because of unforeseen circumstances, some design guidelines or development standards may not be feasible on a particular parcel or may hinder innovative design concepts. In these situations, the procedures listed below will be followed to amend the adopted Specific Plan or allow for minor/administrative modification from development or design standards.

1. MAJOR SPECIFIC PLAN AMENDMENTS

Any changes proposed to the Specific Plan that could significantly increase environmental impacts or result in any of the following substantive changes shall require a Specific Plan amendment:

1. Substantively change the boundaries of the Specific Plan.
2. Substantively change the text, figures, or table of the Specific Plan.
3. Substantially change the location of planning districts, zones or key elements of the Plan including the linear park. (See Minor/Administrative Modifications below for zone boundary adjustments.)

4. Substantively change any element of the Plan or implementing documents (infrastructure plans, design guidelines, capital improvement plan, nexus study, development agreements, public infrastructure reimbursement rules and financing plan) such that they would be inconsistent with Specific Plan policy, text or land use.

The Specific Plan may be amended by the same procedure as the Plan was adopted and shall require a recommendation of approval by the Planning Commission and approval by the City Council. An amendment may be initiated by the City Council, the Community Development Director (Director), property owner or any authorized agent of the property owner. Any amendment requested by a property owner or project applicant may be subject to the fee schedule adopted by City Council. Any amendment to the Specific Plan must be consistent with the City's General Plan.

2. MINOR/ADMINISTRATIVE MODIFICATIONS

Modification(s) to the standards as proposed by a developer/builder during site development and design review of individual Planning Areas and/or Tentative Maps, may be considered subject to the following provisions. A minor/administrative modification to Specific Plan design and/or development standards may be considered and approved or conditionally approved by the Community Development Director if it is determined by the Director that the revision is in substantial conformance with all of the applicable criteria:

The Director determines that the modification does not have a significant impact on the character of the Plan.

1. The proposed modification to the development standards or design guidelines are offset by the merits of the design and do not significantly change the anticipated physical characteristics of the development.
2. The proposed modification to the development standards further the goals of the Specific Plan and enhance the appearance or functionality of the project, while remaining compatible with and not detrimental to adjacent properties or uses.
3. The proposed modification does not modify or exceed the original standard by more than 10 percent (e.g. 10% deviation from setback standard, lot coverage, building height or similar. Does not apply to density which must remain in conformance with General Plan guidelines).
4. The proposed modification would not be detrimental to the health, safety and general welfare of the public or injurious to the property or improvements in the vicinity or the zone in which the subject property is located or the modification proposed.

5. Proposed changes to the alignment of streets, street segments or street sections, which if adopted, would not substantially alter the overall land use and will continue to support the circulation concepts set forth in this Specific Plan.
6. Proposed changes to Specific Plan zone boundaries that result in less than 1 acre or 10 percent acreage difference, that remain compatible with and not detrimental to adjacent properties or uses; residential densities proposed along the Harry Lorenzo/linear greenbelt do not change; overall development densities remain consistent with development concepts set forth in this Specific Plan; zone boundary adjustments take place prior to or at the time of tentative map approval (See Specific Plan Amendments above for planning district amendments.)
7. Adverse environmental impacts are not significantly increased, as defined by the CEQA Guidelines Sections 15162, 15182 and 15183, as a result of the proposed variance.

In granting a Minor / Administrative Modification, the Community Development Director may impose conditions to safeguard public health and safety and to ensure development remains consistent with the objectives and intent of the Specific Plan.

At the discretion of the Community Development Director, any proposed minor/administrative modification may be referred to the Planning Commission and/or City Council for action. An administrative modification may be appealed to the Planning Commission or City Council, as applicable, consistent with the provisions of the Woodland Municipal Code for Zoning Administrator Appeals.

7.5 | INTERPRETATIONS

The Director shall have the responsibility to interpret the provisions of this Specific Plan. If an issue or situation arises that is not sufficiently provided for or is not clearly understandable, those General Plan policies and/or regulations of the Zoning Ordinance that are most applicable and relevant may be used as a guide for resolving the issue.

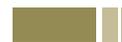
In determining land use where a specific use is not listed, the Director shall have the authority to make an interpretation as to whether the use is similar to other allowed uses in a particular zoning district, or whether such use should not be permitted based on the guiding principles and vision for a particular development district within the Plan Area. The Director may refer any matter for interpretation to the Planning Commission for their consideration or the Director's interpretation may be appealed to the Planning Commission. The Planning Commission's decision, when applicable, may be appealed to the City Council. The City shall keep records of the official determinations on file for future reference and to ensure consistency of interpretations over time.

7.6 | SEVERABILITY AND ENFORCEMENT

If any regulation, condition, program or portion of the Specific Plan is held invalid by a California or Federal Court of competent jurisdiction, such portions shall be deemed separate, distinct, and independent provisions, and the invalidity of such provisions shall not affect the validity of the remaining provisions thereof.

Any person who violates a requirement of this Specific Plan or fails to obey an order issued by the City or comply with a condition of approval of any permit or entitlement issued under this Specific Plan shall be subject to the penalty provisions as provided in the City of Woodland Municipal Code.

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A | APPENDICES





Final Report

Woodland Research and Technology Park Public Facilities Financing Plan

The Economics of Land Use



Prepared for:
City of Woodland

Prepared by:
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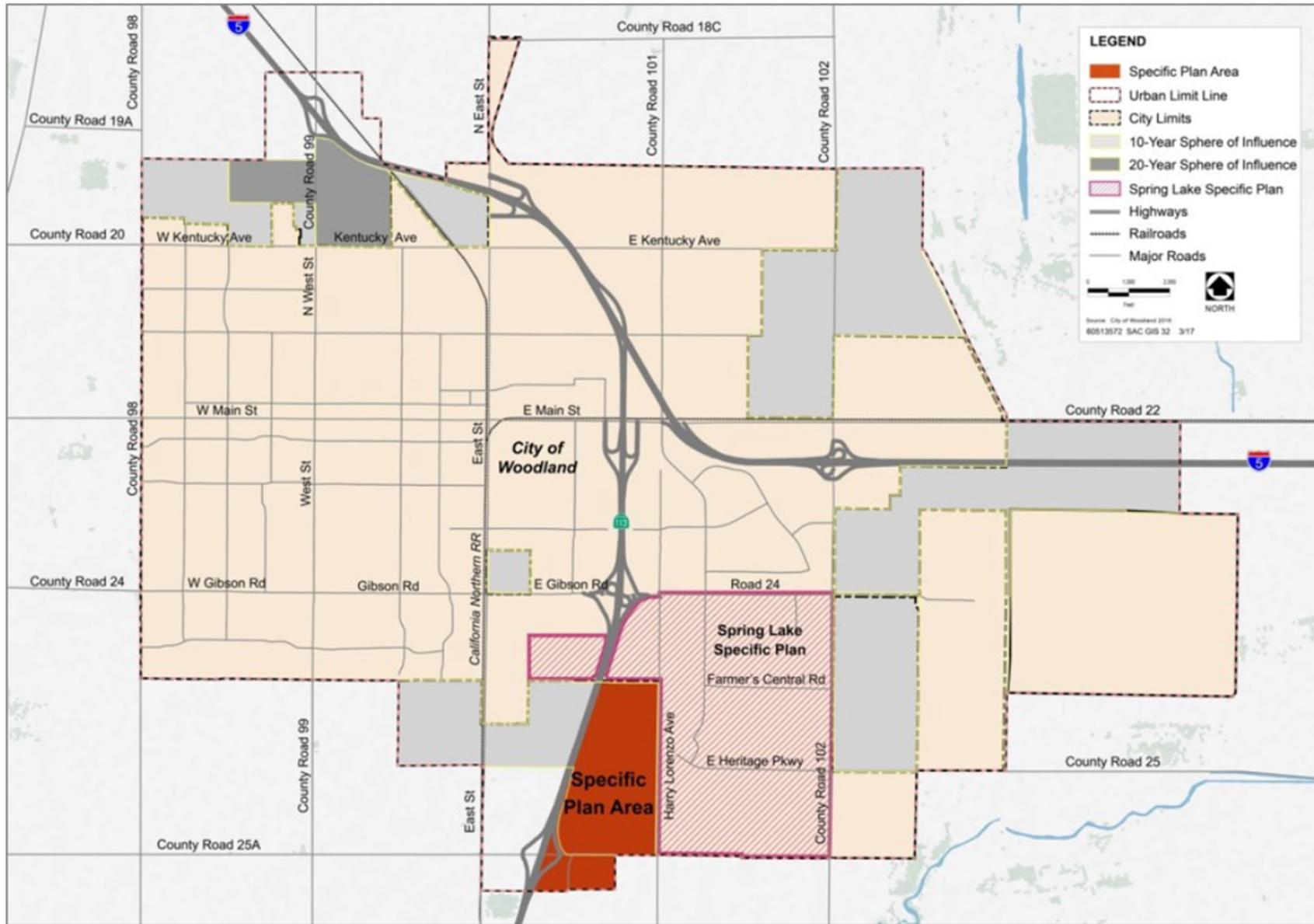
1. Introduction and Executive Summary

This Public Facilities Financing Plan (Financing Plan) presents a strategy to finance backbone infrastructure and other public facilities required to serve the proposed land uses in the Woodland Research and Technology Park Specific Plan Area (WRTP). The financing strategy is designed to be flexible enough to accommodate the development plans of different WRTP property owners while assuring the City of Woodland (City) that the required facilities are constructed when necessary. The Financing Plan includes the use of the following funding mechanisms: existing development impact fee programs, a new Woodland Research and Technology Infrastructure Fee Program (RTIF Program), land-secured debt financing, developer funding, City funding, and funding from other plan areas.

Project Description and Proposed Land Uses

The WRTP is located on approximately 350 acres just south of the City and borders the Spring Lake Specific Plan Area (SLSP), which is located to the east and north of the WRTP. **Map 1-1** shows the WRTP location in relation to the City of Woodland. The WRTP is included in the City's 2035 General Plan as a natural extension of the City and is planned for annexation into the City. As stated in the Public Review Draft WRTP Specific Plan, the WRTP is planned to be a "self-sustaining, enterprising mixed-use community anchored by a technology and innovation campus, with vibrant retail and supporting uses, integrated residential neighborhoods, recreation, open space and public uses." The planned uses in the WRTP at buildout include 1,670 residential dwelling units and approximately 2.3 million square feet of nonresidential retail and office development. **Chapter 2** further details the land use plan.

Map 1-1. Plan Area Location



Backbone Infrastructure and Public Facilities

Definitions of Backbone Infrastructure and Public Facilities

This Financing Plan will use the following definitions to more precisely define backbone infrastructure and public facilities:

- **Backbone Infrastructure:** This term includes most of the essential public service-based items that are underground or on the surface. These items include storm drainage, water, reclaimed water, sewer, and major roads. Backbone infrastructure is sized to serve numerous individual development projects in the WRTP and in some cases serves the broader region's development areas.
- **Public Facilities:** This group of items provides amenities to the WRTP (e.g., park facilities and libraries) or houses employees providing services to the area (e.g., fire facilities). In this Financing Plan, this term includes the following public facilities:
 - Parks and Open Space
 - Schools
 - Libraries
 - Fire Stations and Equipment
 - Police Facilities
 - City and County General Government Facilities
 - Public Transit
 - Agricultural and Habitat Mitigation Land
 - Affordable Housing
- **Public Improvements:** This term is used generically in the Financing Plan to include a combination of backbone infrastructure and public facilities when a precise breakdown is not required.

Total Public Improvements Cost Estimates

This Financing Plan includes approximately \$255.5 million in public improvement requirements associated with WRTP Buildout. The public improvements consist of the improvements specifically required to serve the WRTP that are included in the WRTP Capital Improvement Program prepared by Cunningham Engineering Corporation, as well as additional City, County, and school district public improvements toward which the WRTP must contribute. The public improvement costs are detailed in **Chapter 4**.

Financing Strategy Summary

The purpose of this Financing Plan is to propose a financing strategy to fund the necessary public improvements required to serve the WRTP. The financing strategy is flexible enough to ensure the required improvements are constructed when necessary. The financing mechanisms used will depend on the types and timing of the needed facilities and are guided by objectives and policies established by the Specific Plan.

A combination of funding sources will be used to fund the costs of the public improvements, as summarized below:

- The proposed new WRTP development impact fee program, the RTIF Program (defined earlier in this chapter), will help fund those backbone infrastructure and public facilities costs that are not funded by existing fee programs or other funding sources identified below. Facilities included in the RTIF Program include those facilities with plan-wide benefits (i.e., serve multiple individual subdivisions), the costs of which should be distributed among RTIF land uses and ownership interests.
- Existing City, Yolo County (County), and school district development impact fee programs fund public improvements that benefit the WRTP as well as other developments. The WRTP will contribute to the construction of these public improvements through participation in the existing fee programs. Examples of these fee programs are the City Major Projects Financing Plan Fee Program and the Woodland Joint Unified School District Impact Fee Program.
- Other plan areas will participate in funding facilities for which they have a shared benefit with the WRTP.
- The WRTP developers will be responsible for privately funding a portion of the streets, storm drainage, water, and sewer costs.
- The City will contribute to the funding of several projects with Citywide benefit.
- One or more Mello-Roos Community Facility Districts (CFDs) or other public financing mechanisms will fund infrastructure improvements needed during the development of the WRTP before the collection of sufficient fees or other sources of revenue for reimbursement. The bonds will be repaid through special taxes levied on property through the CFDs. One or more CFDs also will be formed to fund annual maintenance and services costs.
- Developers will be conditioned through development agreements (DAs) to construct and fund certain public improvements needed in the initial phases of the WRTP before the collection of sufficient fees or other funds to construct the improvements. Fee credits and reimbursements for advance-funded facilities included in the RTIF Program will be available to the developers.

Supporting Documents

This Financing Plan relied on the following documents that have been prepared by or for the City or are being prepared in conjunction with this analysis:

- WRTP Specific Plan
- Development Agreements between the City and WRTP Property Owners
- Master Reimbursement Agreement between the City and WRTP Property Owners
- WRTP Development Impact Fee Nexus Study
- WRTP Capital Improvement Program

Organization of Report

This Financing Plan is organized into the following chapters:

- **Chapter 1** included an introduction and executive summary of the Financing Plan.
- **Chapter 2** details the WRTP land uses and population estimates.
- **Chapter 3** details the policy and financing framework for the Financing Plan established in the WRTP Specific Plan.
- **Chapter 4** details the required backbone infrastructure and public facility requirements and associated costs.
- **Chapter 5** describes the overall financing strategy and funding sources.
- **Chapter 6** summarizes the proposed RTIF Program.
- **Chapter 7** examines the feasibility of the WRTP Financing Plan.

In addition, the Financing Plan contains the following appendices:

- **Appendix A:** Summary of WRTP Capital Improvement Program
- **Appendix B:** Development Impact Fee Analysis
- **Appendix C:** Infrastructure Cost Burden Comparison

2. WRTP Land Uses and Phasing

Land Use Summary

The WRTP is located on approximately 350 gross acres, with an estimated 150 acres planned for residential development and 130 acres planned for nonresidential retail and office development. The remaining WRTP areas are planned for public uses, including parks, trails, greenbelts, other open space, roads, and a school. **Table 2-1** summarizes the planned acres, dwelling units, and nonresidential building square feet by land use. It also includes the average square feet per unit for each residential category. These average square feet were estimated based on building permit data provided by the City, as detailed in the WRTP Nexus Study (Nexus Study). **Table 2-2** further details the planned acres, dwelling units, and nonresidential building square feet by the specific plan land uses. A total of 1,670 dwelling units and approximately 2.3 million square feet of nonresidential building space are planned in the WRTP. **Map 2-1** depicts the WRTP land use plan.

Population Summary

Table 2-3 summarizes the estimated population at buildout of the WRTP. For each land use, the population is estimated as the number of dwelling units multiplied by the estimated persons per household (PPH). It is estimated that the WRTP will have a population of approximately 4,300 people at buildout. The methodology for estimating the PPH factors is detailed in the Nexus Study.

Development Phasing

A WRTP development phasing plan has not been developed, but it is anticipated that low density and medium density residential units generally will develop concurrently with or just ahead of commercial development and before high-density residential units. Development phasing will be driven by market demand.

Each developer will be required to submit a Phasing Plan for each development phase prior to approval of a tentative subdivision map for the property in the development phase. The Phasing Plan will be consistent with the infrastructure phasing rules detailed in the DA between the City and the developer and will include the required infrastructure to be constructed by the developer. Phasing of infrastructure will be guided by development phasing. The required infrastructure that the developer must construct could include both onsite and offsite infrastructure and will be determined in consultation with the City Engineer.

**Table 2-1
Woodland Research and Technology Park Financing Plan
Land Use Summary**

Land Use	Density	Building Square Feet per Unit	Buildout		
			Acres	Dwelling Units	Building Square Feet
<i>Source:</i>			<i>Table 2-2</i>	<i>Table 2-2</i>	<i>Table 2-2</i>
Residential	<i>units per acre</i>	<i>avg per unit</i>			
Low Density [1]	7.1	2,300	87.9	628	
Medium Density [2]	12.7	1,850	41.1	524	
High Density [3]	24.0	990	21.6	518	
Total Residential			150.6	1,670	
Nonresidential	<i>FAR</i>	<i>per acre</i>			
Village Center/Community Commercial [4]	0.43	18,930	16.4		310,948
Business Park [5]	0.40	17,424	113.6		1,980,115
Total Nonresidential			130.1		2,291,063
Public					
Open Space			20.3		
Right of Way			37.9		
School			10.0		
Total Public			68.2		
Total			348.8	1,670	2,291,063

Source: Woodland Research and Technology Park Specific Plan; City of Woodland

[1] Includes Low Density and Village Center Low Density.

[2] Includes Medium Density and Village Center Medium Density.

[3] Includes High Density and High Density/Community Commercial (residential part).

[4] Includes Community Commercial, High Density/Community Commercial (commercial part), Village Center Mixed Use, and Village Center Commercial.

[5] Includes Research and Technology Park, Highway Commercial, and Research Flex.

Table 2-2
Woodland Research and Technology Park Financing Plan
Land Use Detail

Land Use	Density	Buildout		
		Acres	Dwelling Units	Building Square Feet
Residential	<i>units per acre</i>			
Single Family				
Low Density	7.3	74.8	549	
Village Center Low Density	6.0	13.1	79	
Medium Density	13.2	25.5	337	
Village Center Medium Density	12.0	15.6	187	
Subtotal Single Family		129.0	1,152	
Multifamily				
High Density	24.0	9.7	232	
High Density/Community Commercial (75%/25%)	24.0	11.9	286	
Subtotal Multifamily		21.6	518	
Total Residential	11.1	150.6	1,670	
Retail/Commercial	<i>FAR</i>			
Village Center/Community Commercial				
Community Commercial	0.40	6.8		118,065
High Density/Community Commercial (75%/25%)	0.40	4.0		69,173
Village Center Mixed Use [1]	0.50	4.2		91,040
Village Center Commercial	0.50	1.5		32,670
Business Park				
Research and Technology Park	0.40	69.6		1,212,170
Highway Commercial	0.40	8.2		142,981
Research Flex (Light Industrial)	0.40	35.9		624,964
Total Retail/Commercial		130.1		2,291,063
Parks and Open Space				
Open Space		11.0		
Village Center Open Space (net of Commercial)		9.3		
Subtotal		20.3		
School		10.0		
Right of Way		37.9		
Total		348.8	1,670	2,291,063

Source: Woodland Research and Technology Park Specific Plan; City of Woodland

[1] Assumes parcel C-VCMDR3 (0.702 acres) develops as village commercial mixed use.

Map 2-1. Land Use Plan

LEGEND

LDR	LOW DENSITY RESIDENTIAL
VCLDR	VILLAGE CENTER LOW DENSITY RESIDENTIAL
MDR	MEDIUM DENSITY RESIDENTIAL
VCMR	VILLAGE CENTER MEDIUM DENSITY RESIDENTIAL
HDR	HIGH DENSITY RESIDENTIAL
HDR/CCO	HIGH DENSITY RESIDENTIAL WITH COMMUNITY COMMERCIAL OVERLAY
VCMU	VILLAGE CENTER MIXED USE
RTP	RESEARCH & TECHNOLOGY PARK
RTP/TO	RESEARCH & TECHNOLOGY PARK WITH TRANSITIONAL OVERLAY
RTP/CCO	RESEARCH & TECHNOLOGY PARK WITH COMMUNITY COMMERCIAL OVERLAY
RTP/RFO	RESEARCH & TECHNOLOGY PARK WITH RESEARCH FLEX OVERLAY
HC	HIGHWAY COMMERCIAL
OS	GREENBELTS, OPEN SPACE
PP	PEDESTRIAN PROMENADE
VCOS	VILLAGE CENTER OPEN SPACE
DET POND	DETENTION POND



Table 2-3
Woodland Research and Technology Park Financing Plan
Estimated Population at Buildout by Land Use Category

Land Use	Dwelling Units	PPH	Population
Single Family			
Low Density	549	3.09	1,696
Village Center Low Density	79	3.09	244
Medium Density	337	2.39	806
Village Center Medium Density	187	2.39	447
Subtotal Single Family	1,152		3,193
Multifamily			
High Density	232	2.15	499
High Density/Community Commercial (75%/25%)	286	2.15	615
Subtotal Multifamily	518		1,114
Total Residential	1,670		4,307

Source: City of Woodland; EPS; U.S. Census Bureau PUMS data; Solano County Assessor

3. Policy and Financing Framework

Overview

This chapter provides an overview of the WRTP policy framework related to the Financing Plan. The Financing Plan must be prepared in accordance with the requirements of the WRTP Specific Plan document (Specific Plan), the DAs, and the Master Reimbursement Agreement. The purpose of each of these documents is described briefly below:

- **Specific Plan.** The Specific Plan establishes the land use plan and zoning regulations for development within the WRTP. The Specific Plan governs the development of the WRTP and requires completion of this Financing Plan to describe the financing strategy and mechanisms to fund the backbone infrastructure and public facilities needed for the WRTP development.
- **DAs.** Each property owner is entering into a DA with the City that allows the property owner vested rights to develop their property in accordance with the terms and conditions of the Specific Plan. The DAs detail the specific requirements for the property owners to be allowed to develop, including requirements for the construction of certain backbone infrastructure and public facilities.
- **Master Reimbursement Agreement.** The Master Reimbursement Agreement is an agreement between all of the developers and the City that details the procedures and requirements for the developers to receive RTIF Program credits and reimbursements for the dedication of land and construction of backbone infrastructure and public facilities.

Financing Plan Requirements

As stated in the Specific Plan, “the Financing Plan will serve as a framework to guide and support the objectives of the Specific Plan.” This Financing Plan provides the estimated costs to construct identified public improvements needed to implement the Specific Plan and describes the proposed funding mechanisms to fund those costs.

The overall costs and funding sources in the Financing Plan have been estimated for the purpose of providing feasible financing strategies and mechanisms to construct the backbone infrastructure and public facilities required for WRTP development. The development, costs, and funding sources in this Financing Plan are based on the best information available at this time and may need to be updated as development progresses.

The Specific Plan includes the following requirements for the Financing Plan:

- Describe the project and sequencing of needed infrastructure.
- Provide a summary of the infrastructure and public facility requirements to serve future development within the Plan Area.
- Include infrastructure cost estimates by land use and by development phase per acre, and the estimated infrastructure cost at build-out of the Plan Area.
- Include summarized costs for infrastructure and public facilities needed to serve the Specific Plan, including costs needed to serve the Plan Area at buildout.
- Review and include existing fees, taxes and assessments which may be applicable to new development in the Specific Plan.
- Present cost estimates for operating and maintaining the required infrastructure and for ongoing municipal services.
- Identify potential funding sources for both the construction of infrastructure and provision of municipal services.
- Present the overall cost burden by land use and by development area on a per acre basis.
- Provide recommended action steps for implementation of the infrastructure financing.

Financing Plan Goals

The elements of the Financing Plan must work together to provide the optimal balance of fees, private, and other financing to not overly burden undeveloped land, while assuring that necessary facilities are constructed when needed. The goals of the Financing Plan are summarized below:

1. Fully fund all facility and infrastructure improvements when the improvements are needed to serve the Project.
2. Use existing fee programs to the extent possible.
3. Identify new fee programs or increases in existing fee programs to the extent required.
4. Make use of “pay-as-you-go” mechanisms, while allowing for timely construction of public improvements.
5. Make appropriate use of private financing, existing fee programs, new fee programs, and credit/reimbursement agreements to fund improvements when needed.

4. Backbone Infrastructure and Public Facilities

This chapter summarizes the WRTP backbone infrastructure and public facilities (public improvements) that are required to serve the WRTP. The backbone infrastructure and public facilities consist of the improvements specifically required to serve the WRTP that are included in the WRTP Capital Improvement Program (CIP) prepared by Cunningham Engineering Corporation, as well as additional City, County, and school district public improvements toward which the WRTP must contribute. **Table 4-1** summarizes public improvements and the WRTP estimated cost obligation for these improvements.

Appendix A contains a summary of the CIP. The additional public improvements not included in the CIP, such as citywide road improvements, schools, and libraries are generally located off-site and funded through existing development impact fee programs.

Backbone Infrastructure

As shown in **Table 4-1**, the WRTP has a backbone infrastructure cost obligation of approximately \$156.9 million. The CIP contains an estimated \$100.6 million of improvements. The WRTP will have an additional \$56.2 million obligation for other Citywide backbone infrastructure. All costs are expressed in 2023 dollars. The following types of backbone infrastructure are included:

- Streets
- Storm Drainage
- Water
- Sewer

Streets

Map 4-1 displays the road circulation plan for the WRTP. The CIP includes an estimated \$71.0 million in land acquisition, right of way and utilities, agricultural mitigation and habitat conservation plan (HCP), and construction costs for transportation improvements required to serve the WRTP. An additional \$29.7 million in Citywide transportation improvements will be payable through the City Major Projects Financing Plan (MPFP) Fee Program.

**Table 4-1
Woodland Research and Technology Park Financing Plan
Estimated Backbone Infrastructure and Public Facilities Costs (2023\$)**

Item	Total	WRTP CIP	Other Improvements
Backbone Infrastructure			
Streets	\$100,727,322	\$71,046,000	\$29,681,322
Storm Drainage	\$20,841,000	\$20,841,000	\$0
Water	\$15,792,537	\$5,228,000	\$10,564,537
Sewer	\$19,547,446	\$3,549,000	\$15,998,446
Subtotal Backbone Infrastructure	\$156,908,304	\$100,664,000	\$56,244,304
Public Facilities			
Parks and Open Space	\$44,413,170	\$30,119,000	\$14,294,170
Schools	\$13,452,711	\$0	\$13,452,711
Libraries	\$967,434	\$0	\$967,434
Fire	\$9,856,258	\$0	\$9,856,258
Police	\$2,720,698	\$0	\$2,720,698
General Government	\$3,329,707	\$0	\$3,329,707
Public Transit	\$471,196	\$0	\$471,196
Affordable Housing	\$7,782,340	\$0	\$7,782,340
Agricultural Mitigation	\$4,499,785	\$0	\$4,499,785
County Facilities	\$6,263,783	\$0	\$6,263,783
Habitat Conservation Plan	\$4,860,368	\$0	\$4,860,368
Total Public Facilities	\$98,617,450	\$30,119,000	\$68,498,450
Total	\$255,525,754	\$130,783,000	\$124,742,754

Source: Cunningham Engineering; City of Woodland; EPS.

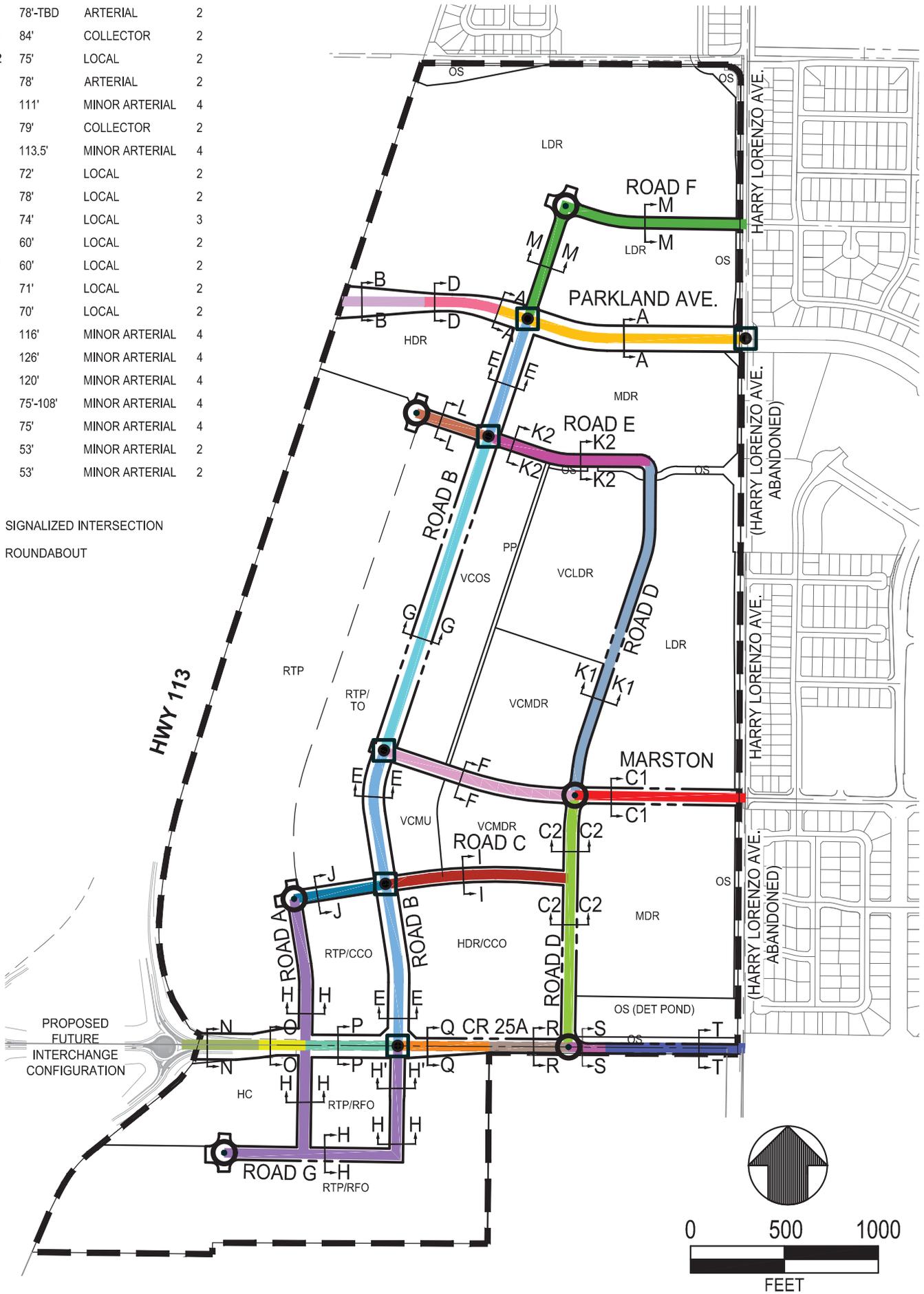
[1] WRTP CIP Streets costs differ from costs on Table A-1 because this table includes a \$1.45 million interchange contingency that is not included in the CIP costs shown in Table A-1.

LEGEND

SECTION	RIGHT OF WAY (FT.)	STREET CLASSIFICATION	NO. OF LANES
A-A	134'	ARTERIAL	4
B-B	78'-TBD	ARTERIAL	2
C1-C1	84'	COLLECTOR	2
C2-C2	75'	LOCAL	2
D-D	78'	ARTERIAL	2
E-E	111'	MINOR ARTERIAL	4
F-F	79'	COLLECTOR	2
G-G	113.5'	MINOR ARTERIAL	4
H-H	72'	LOCAL	2
I-I	78'	LOCAL	2
J-J	74'	LOCAL	3
K1-K1	60'	LOCAL	2
K2-K2	60'	LOCAL	2
L-L	71'	LOCAL	2
M-M	70'	LOCAL	2
N-N	116'	MINOR ARTERIAL	4
O-O	126'	MINOR ARTERIAL	4
P-P	120'	MINOR ARTERIAL	4
Q-Q	75'-108'	MINOR ARTERIAL	4
R-R	75'	MINOR ARTERIAL	4
S-S	53'	MINOR ARTERIAL	2
T-T	53'	MINOR ARTERIAL	2

-  SIGNALIZED INTERSECTION
-  ROUNDABOUT

Map 4-1. Road Circulation Diagram



The street improvements below are included in the CIP and described in the remainder of this section:

- County Road 25A
- Parkland Avenue
- Parkland Avenue Overcrossing
- Main Internal Roadways
- Road B
- Highway 113 and County Road 25A Interchange

County Road 25A

County Road 25A (CR 25A) will be the primary entryway to the WRTP. The RTIF Program includes construction of the section of CR 25A from Highway 113 on the west to Harry Lorenzo Avenue on the east. There will be a signalized intersection at Road B and CR 25A that will serve as the main entryway to the WRTP.

Parkland Avenue Overcrossing

The parkland avenue overcrossing over Highway 113 will connect the west side of Highway 113 to the future Parkland Avenue in the WRTP. It will provide access from the WRTP to other future City plan areas west of Highway 113.

Parkland Avenue

Parkland Avenue is the principal arterial street within the WRTP. The CIP includes funding for the extension of Parkland Avenue from Harry Lorenzo Avenue on the east side of the WRTP to the future Parkland Avenue Overcrossing on the west side of the WRTP.

Main Internal Roads

The CIP includes construction of the main internal streets. These streets are designed to connect different parts of the WRTP and provide access into and out of the project.

Road B

Road B is a minor arterial street that serves as the main north to south road linking different areas of the WRTP. All intersections on Road B will be signalized and include enhanced pavement, landscaping, and aesthetic features. Road B includes a mobility hub that will serve as a point of arrival or departure for employees and residents traveling to, from, and within the WRTP by different transportation modes.

Harry Lorenzo Avenue – Half Section North

The CIP includes construction of the western half section of Harry Lorenzo Avenue from the northern WRTP boundary at Farmer’s Central Road to Parkland Avenue. This improvement has been completed and included grading and erosion control, street work and concrete, and streetlights.

Highway 113 and County Road 25A Interchange (Double Roundabout Option)

Interchange improvements at Highway 113 and CR 25A include ramp improvements and the construction of roundabouts on both the east and west sides of the interchange.

Storm Drainage

Map 4-2 displays the planned storm drainage system plan for the WRTP. The CIP includes a total of \$20.8 million in land acquisition, agricultural mitigation and HCP, and construction costs for on-site and off-site storm drainage improvements. On-site improvements consist of a system of collection and conveyance facilities located in and adjacent to collector and arterial streets and in greenbelt corridors. Off-site improvements consist of upgrades to the recently constructed South Regional Pump Station.

The WRTP does not have an obligation for any additional storm drainage improvements outside of those in the CIP.

Water

Map 4-3 displays the proposed water distribution system for the WRTP, and **Map 4-4** shows the proposed recycled water distribution system. The CIP includes a total of \$5.2 million in construction costs for water improvements, including recycled water improvements. Water facilities consist of the pipelines needed to connect to existing transmission mains and distribute water throughout the WRTP. The water pipelines will be located within the right-of-way of public streets, and the recycled water pipelines will be located within the right-of-way of public streets and greenways.

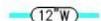
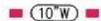
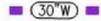
An additional \$10.6 million in Citywide water improvements will be payable through the City MPFP Fee Program.

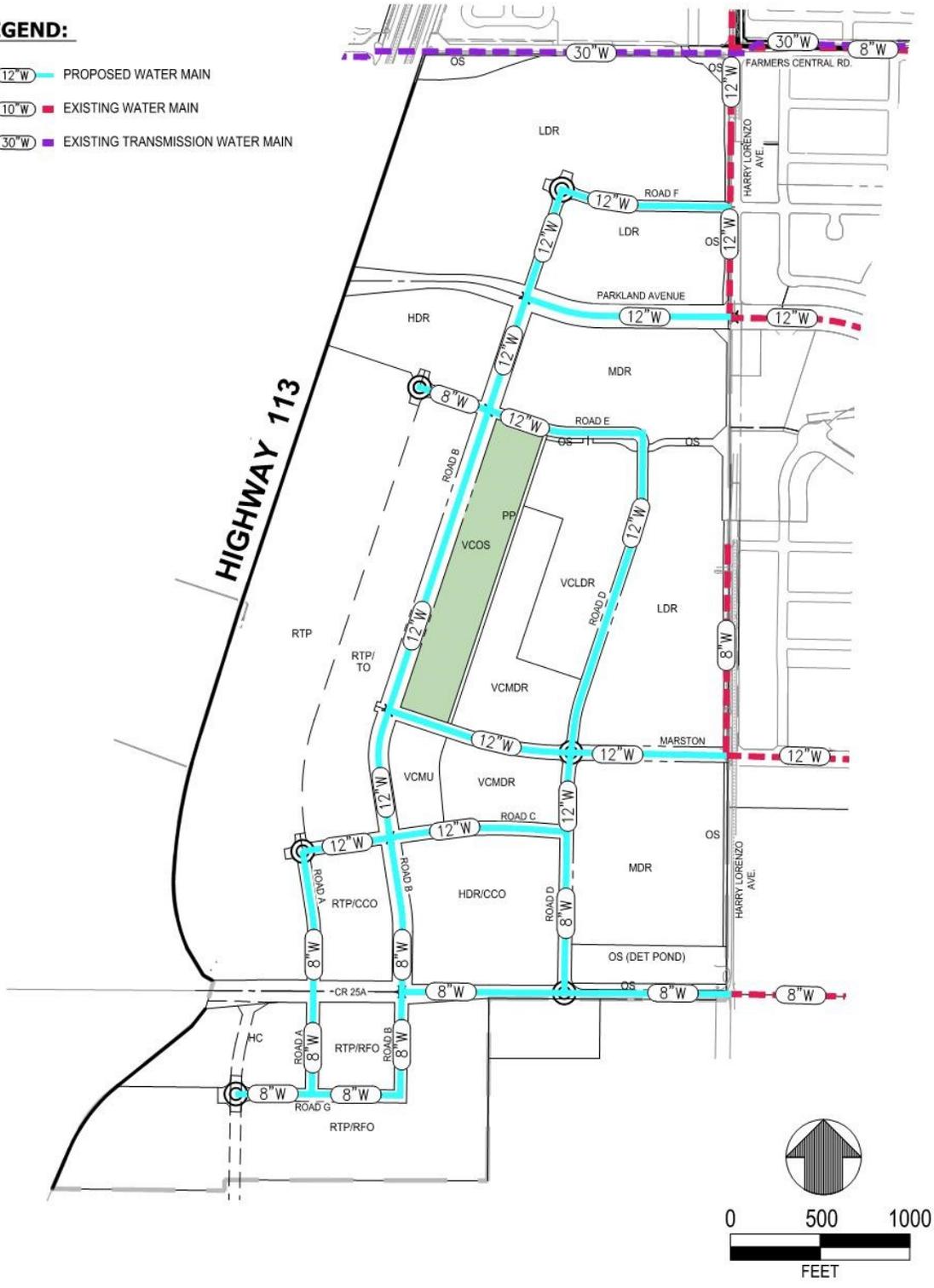
Sewer

Map 4-5 displays the proposed wastewater system for the WRTP. The CIP includes a total of \$3.5 million in construction costs for on-site and off-site sewer improvements. On-site improvements consist of a system of collection and conveyance facilities located in and adjacent to internal streets. Off-site improvements include upgrades to the existing SLSP pump station.

Map 4-3. Proposed Water Distribution System

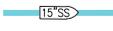
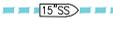
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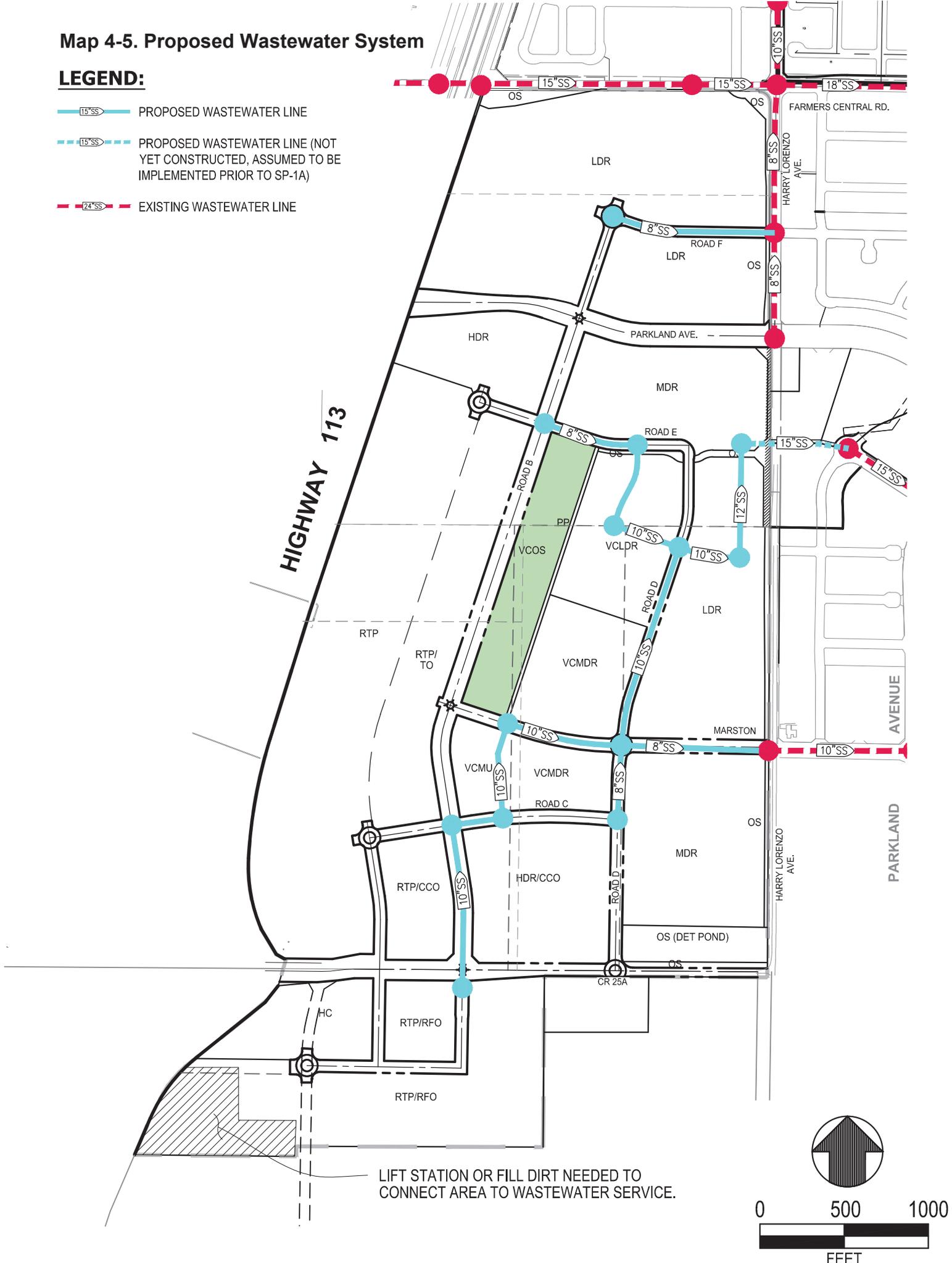
-  12" W PROPOSED WATER MAIN
-  10" W EXISTING WATER MAIN
-  30" W EXISTING TRANSMISSION WATER MAIN



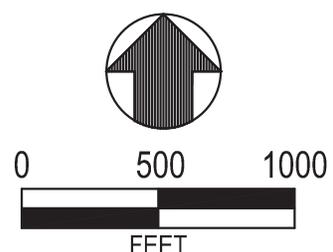
Map 4-5. Proposed Wastewater System

LEGEND:

-  15"SS PROPOSED WASTEWATER LINE
-  15"SS PROPOSED WASTEWATER LINE (NOT YET CONSTRUCTED, ASSUMED TO BE IMPLEMENTED PRIOR TO SP-1A)
-  24"SS EXISTING WASTEWATER LINE



LIFT STATION OR FILL DIRT NEEDED TO CONNECT AREA TO WASTEWATER SERVICE.



An additional \$16.0 million in Citywide sewer improvements will be payable through the City MPFP Fee Program.

Public Facilities

As shown in **Table 4-1**, the WRTP has a public facilities cost obligation of approximately \$98.6 million. The WRTP CIP contains an estimated \$30.1 million of parks and open space. The WRTP will have an additional \$68.5 million obligation for other City, County, and school district improvements. All costs are expressed in 2023 dollars. The following types of public facilities will be required to serve the WRTP:

- Parks and Open Space
- Schools
- Libraries
- Fire
- Police
- General Government
- Public Transit
- Affordable Housing
- Agricultural Mitigation
- County Facilities
- Habitat

Parks and Open Space

Map 4-6 shows the proposed parks and open space sites in the WRTP. The CIP includes a total of \$30.1 million in land acquisition, agricultural mitigation and HCP, and construction costs for parks and open space. The planned parks and open space system includes parks, open space, and green belts throughout the WRTP. The land use plan includes an 11.6-acre neighborhood park to serve the WRTP and 6 acres of open space and greenbelts.

An additional \$14.3 million in Citywide parks and recreation improvements will be payable through the City MPFP Fee Program.

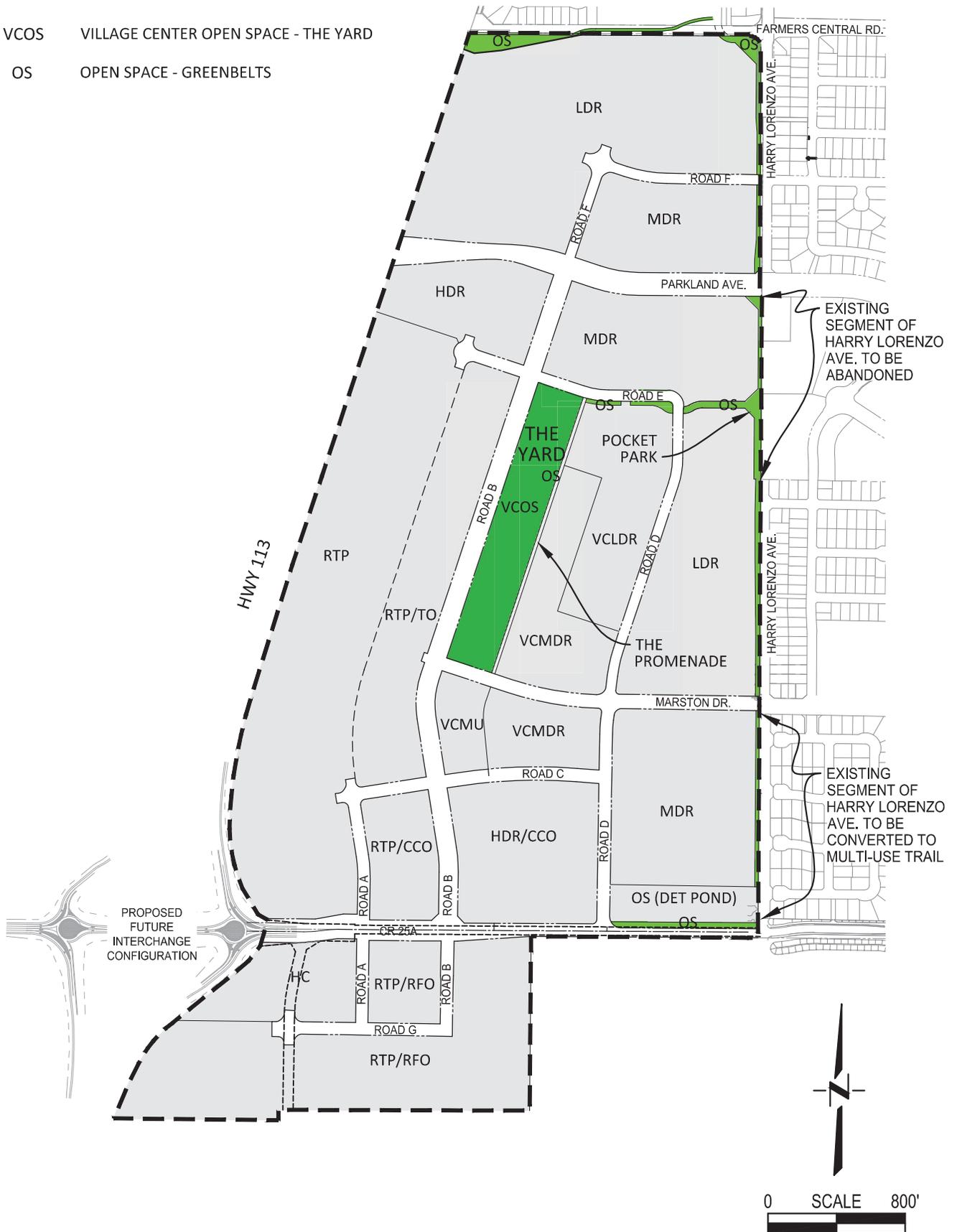
Schools

Map 4-7 shows a possible WRTP school site. It is currently undecided if a school will be constructed in the WRTP. However, land has been zoned and reserved for a 12-acre school campus if the Woodland Joint Unified School District (WJUSD) determines that there is a need for a school in the plan area. Public school students from the WRTP may be served by existing or new schools in neighboring areas of Woodland. The CIP does not contain any school costs, as schools will be constructed by the WJUSD.

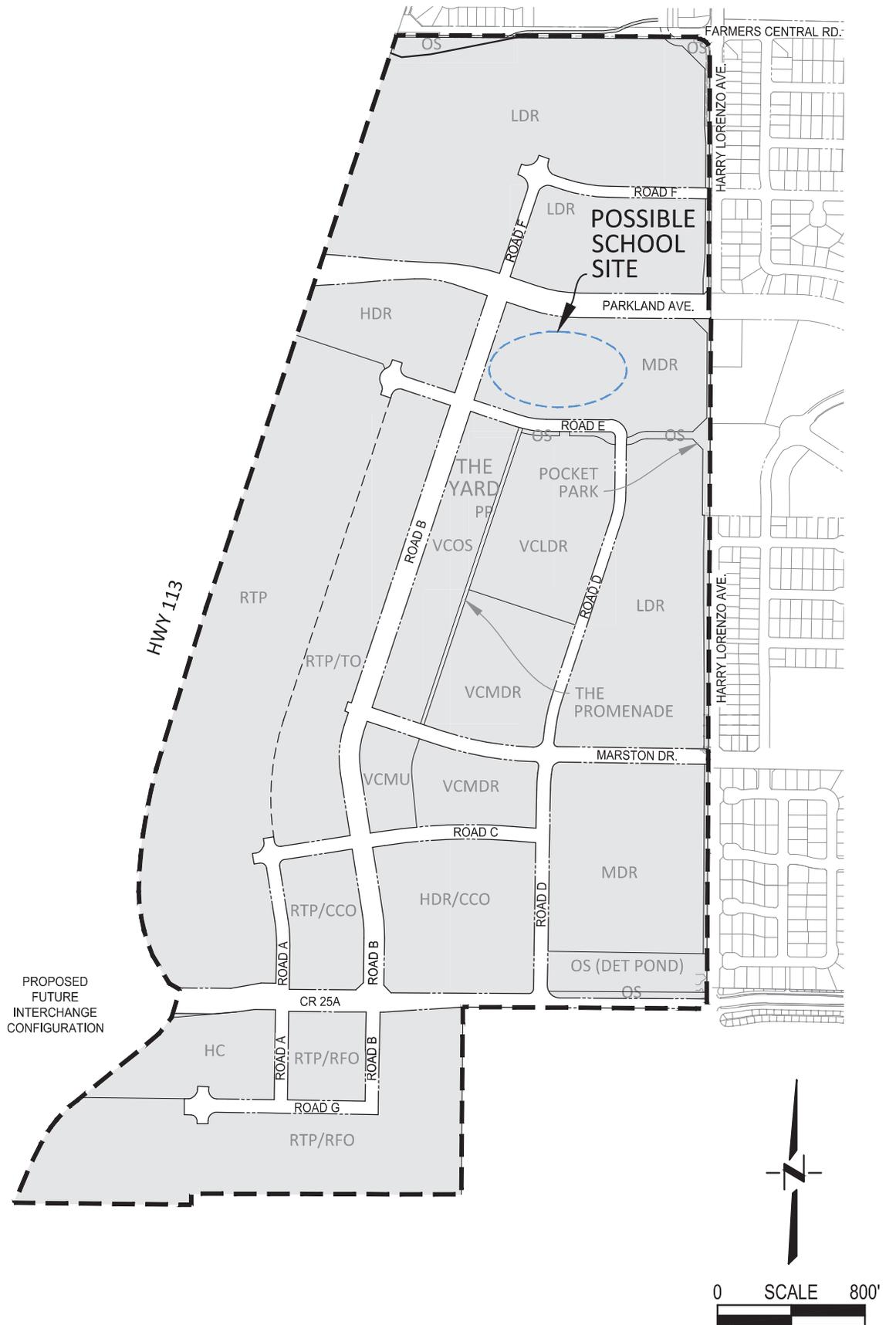
Map 4-6. Proposed Parks/Open Space Sites

LEGEND

- VCOS VILLAGE CENTER OPEN SPACE - THE YARD
- OS OPEN SPACE - GREENBELTS



Map 4-7. Possible School Site



The WRTP is anticipated to contribute an estimated \$13.5 million to school construction costs through the payment of WJUSD school development fees.

Libraries

New or expanded library facilities may be needed to serve the residents of the WRTP, but a library is not planned in the WRTP. The residents of the WRTP will be served by the existing public library in the City or by potential new facilities. The CIP does not contain any library costs, as required new library facilities will be constructed by the City. The WRTP will contribute an estimated \$1.0 million to library improvement costs through the payment of City MPFP fees.

Fire

Fire facilities will be needed to serve the residents of the WRTP, but a fire station is not planned in the WRTP. The City plans to relocate the existing Fire Station Three from its current location two miles north of the WRTP to a site one-half mile north of the WRTP, so that this station can provide service with the required response times to the WRTP. The CIP does not contain fire facility costs, as required new fire facilities will be constructed by the City. The WRTP will contribute an estimated \$9.9 million to fire facilities and equipment costs through the payment of City MPFP fees.

Police

Expanded police facilities and equipment will be needed to provide adequate law enforcement services to the residents of the WRTP, but a police station is not planned in the WRTP. The Woodland Police Department plans to add additional police officers to serve the new City residents, resulting in the need for additional equipment and vehicles. However, the City has determined that its existing police station is sufficiently sized to provide required police service levels through buildout of the WRTP. The CIP does not include police improvement costs, as the City will acquire the required new equipment and vehicles and make any facility improvements. The WRTP will contribute an estimated \$2.7 million to police improvement costs through the payment of City MPFP fees.

General Government Facilities

Improvements to general government facilities, such as City Hall and other City buildings may be needed to serve the new residents and employees of the WRTP. The CIP does not include general government facilities costs, as any required improvements will be constructed by the City. The WRTP will contribute an estimated \$3.3 million to general government facilities costs through the payment of City MPFP fees.

Public Transit

A public transit system will be needed to serve the new residents and employees of the WRTP. Transit improvement costs for the mobility hub are included with the Streets costs (see prior discussion in Road B section). The City may require additional start-up capital investment for vehicles and operations and maintenance costs. The City has established a Public Transit fee to fund these additional costs. The WRTP will contribute an estimated \$471,000 to transit improvements through the payment of the City Public Transit Fee.

Affordable Housing

As stated in the WRTP Affordable Housing Plan, ten percent of all units developed in the WRTP shall be made available to low income residents, as defined in the City's Affordable Housing Ordinance. The developers may pay in-lieu fees established for WRTP market rate residential development to satisfy the affordable housing obligation. In addition, five acres of land shall be dedicated in the plan area for the construction of at least 160 low-income housing units. The WRTP will contribute to the construction of affordable housing through a combination of affordable housing in-lieu fees and land dedication.

Agricultural Mitigation

Development of the WRTP will result in the loss of agricultural land. The City of Woodland and Yolo County have implemented agricultural land conservation policies and mitigation programs to permanently protect agricultural land in Yolo County. The adopted conservation and mitigation programs allow for the recordation of agricultural conservation easements and/or payment of an in-lieu mitigation fee at a ratio of 1 to 3 acres of agricultural land for each acre of development (i.e. loss of prime agricultural land). The WRTP will contribute toward establishing agricultural conversation easements in Yolo County through payment of City and County agricultural land mitigation fees and/or the recordation of easements.

County Facilities

New or expanded County facilities will be needed to provide services to the residents and employees of the WRTP. The County has a County Facilities and Services Authorization development impact fee program to fund the construction of County facilities. The WRTP is estimated to contribute an estimated \$6.3 million toward County facilities construction through the payment of the County FSA fees.

Habitat

Development of the WRTP will result in the loss of plant and wildlife habitats. The WRTP is within the Yolo Habitat Conservation Plan/Natural Community Conservation Plan (HCP/NCCP) coverage area, which encompasses the whole of Yolo County. The HCP/NCCP was adopted in 2018 to mitigate the impacts of development on habitats within Yolo County. A fee program is established under the HCP/NCCP to mitigate for the loss of land cover types, covered species habitat, and other biological values on a per acre basis. The WRTP is estimated to contribute \$4.9 million in development-based fees to fund mitigation that will offset those losses listed above.

5. Financing Strategy and Funding Sources

Overview

The backbone infrastructure and public facilities required to serve development in the WRTP will be funded using a combination of public and private funding sources. **Table 5-1** summarizes the estimated costs and anticipated funding sources for the WRTP backbone infrastructure and public facilities. The funding sources and amounts are shown separately for improvements included in the WRTP CIP and other improvements funded through existing development impact fee programs. The funding for each of these types of improvements is discussed in the remainder of this chapter.

WRTP CIP Improvements

Developers will construct and privately finance the construction costs for most of the WRTP backbone infrastructure and public facilities included in the WRTP CIP. Specific requirements for developer construction of backbone infrastructure and public facilities will be identified in tentative map conditions and DA requirements. The developers will be required to submit an Infrastructure Phasing Plan to the City with the submission of the first tentative map for the property to be developed. The Infrastructure Phasing Plan will include the required infrastructure that the developer must construct.

For developer constructed improvements that are included in the proposed WRTP Infrastructure Fee Program (RTIF Program) the developer may receive credits or reimbursements from the proposed RTIF Program, subject to the credit and reimbursement requirements in the DA and the Master Reimbursement Agreement.

Some of the CIP improvements are not included in the RTIF Program and either will be constructed and privately funded by developers or will be constructed by the City. The Spring Lake Specific Plan area (SLSP) and future specific plan areas 1B and 1C (SP-1B and SP-1C) will contribute to the funding of some of the street improvements.

Additionally, the financing strategy includes formation of one or more land-secured bond financing districts (e.g., Mello-Roos CFD or Assessment District), which may be used to fund a portion of the total backbone infrastructure and public facilities needed at the outset of development.

Table 5-1
Woodland Research and Technology Park Financing Plan
Estimated Sources and Uses of Funding (2023\$)

Item	Total	WRTP CIP							Existing Fee Programs				Subtotal	
		RTIF Program	Developer	City	SP-1B/1C	SLSP	Other	Subtotal	City MPFP Fees	Other City Fees	WJUSD	County		
Backbone Infrastructure														
Streets [1]	\$100,727,322	\$48,702,000	\$9,377,000	\$3,428,000	\$6,524,000	\$3,015,000	-	\$71,046,000	\$29,681,322	-	-	-	-	\$29,681,322
Storm Drainage	\$20,841,000	\$11,338,000	\$2,012,000	-	-	-	\$7,491,000	\$20,841,000	-	-	-	-	-	-
Water	\$15,792,537	\$4,343,000	\$375,000	\$510,000	-	-	-	\$5,228,000	\$10,564,537	-	-	-	-	\$10,564,537
Sewer	\$19,547,446	\$1,879,000	\$1,131,000	\$539,000	-	-	-	\$3,549,000	\$15,998,446	-	-	-	-	\$15,998,446
Subtotal Backbone Infrastructure	\$156,908,304	\$66,262,000	\$12,895,000	\$4,477,000	\$6,524,000	\$3,015,000	\$7,491,000	\$100,664,000	\$56,244,304	-	-	-	-	\$56,244,304
Public Facilities														
Parks and Open Space	\$44,413,170	\$28,946,000	-	-	-	\$1,173,000	-	\$30,119,000	\$14,294,170	-	-	-	-	\$14,294,170
Schools	\$13,452,711	-	-	-	-	-	-	-	-	-	\$13,452,711	-	-	\$13,452,711
Libraries	\$967,434	-	-	-	-	-	-	-	\$967,434	-	-	-	-	\$967,434
Fire	\$9,856,258	-	-	-	-	-	-	-	\$9,856,258	-	-	-	-	\$9,856,258
Police	\$2,720,698	-	-	-	-	-	-	-	\$2,720,698	-	-	-	-	\$2,720,698
General Government	\$3,329,707	-	-	-	-	-	-	-	\$3,329,707	-	-	-	-	\$3,329,707
Public Transit	\$471,196	-	-	-	-	-	-	-	-	\$471,196	-	-	-	\$471,196
Affordable Housing	\$7,782,340	-	-	-	-	-	-	-	-	\$7,782,340	-	-	-	\$7,782,340
Agricultural Mitigation	\$4,499,785	-	-	-	-	-	-	-	-	\$4,499,785	-	-	-	\$4,499,785
County Facilities	\$6,263,783	-	-	-	-	-	-	-	-	-	-	\$6,263,783	-	\$6,263,783
Habitat Conservation Plan	\$4,860,368	-	-	-	-	-	-	-	-	-	-	\$4,860,368	-	\$4,860,368
Total Public Facilities	\$98,617,450	\$28,946,000	-	-	-	\$1,173,000	-	\$30,119,000	\$31,168,267	\$12,753,321	\$13,452,711	\$11,124,150	\$68,498,450	
Total	\$255,525,754	\$95,208,000	\$12,895,000	\$4,477,000	\$6,524,000	\$4,188,000	\$7,491,000	\$130,783,000	\$87,412,572	\$12,753,321	\$13,452,711	\$11,124,150	\$124,742,754	

Source: Cunningham Engineering; City of Woodland; EPS.

[1] WRTP CIP Streets costs differ from costs on Table A-1 because this table includes a \$1.45 million interchange contingency that is not included in the CIP costs shown in Table A-1.

Existing Fee Programs

The WRTP will have an obligation to contribute to construction of other backbone infrastructure and public facilities not included in the CIP. For these public improvements, the developers will pay applicable City, County and WJUSD development impact fees.

Detailed Sources of Funding

The following sections detail the currently available sources identified to fund WRTP Facilities:

- RTIF Program
- Existing City and Other Agency Fee Programs
- Other Funding Sources

As shown in **Table 5-1**, approximately \$95.2 million will be funded through the proposed RTIF Program, approximately \$124.7 million will be funded by the existing fee programs, and approximately \$35.6 million will be funded from other funding sources.

RTIF Program

Detailed further in **Chapter 6**, the proposed RTIF Program will help fund those backbone infrastructure and public facilities costs that are not funded by existing fee programs or other funding sources identified in the sections to follow. Facilities included in the RTIF Program include those facilities with plan-wide benefits (i.e., serve multiple individual subdivisions), the costs of which should be distributed among RTIF land uses and ownership interests.

The RTIF Program will be a City-implemented, plan area-specific development fee program applicable only to new RTIF development. The RTIF Program will fund the following types of improvements:

- Streets
- Storm Drainage
- Water
- Sewer
- Parks and Open Space

The estimated buildout fee revenue generated by the RTIF Program is detailed in **Appendix B**.

Existing City and Other Agency Fee Programs

The WRTP will be subject to all applicable City and other agency development impact fees in place at the time of acceptance of the building permit application. The WRTP will contribute to the construction of City, County, and regional backbone infrastructure and public facilities through payment of existing development impact fees. The existing fee programs in which the WRTP will participate are summarized below and were further detailed in the previous chapter:

- City MPFP Fee Program (Streets, Water, Sewer, Parks, Fire, Police, General Government)
- City Transit Fee Program
- City Affordable Housing Fee Program
- City Agricultural Mitigation Fee Program
- Yolo County Facilities and Services Authorization Fee Program

The estimated WRTP buildout fee revenue generated by these fee programs is summarized in **Table 5-1** and detailed in **Appendix B**.

Other Funding Sources

Other funding sources anticipated to fund a portion of required backbone infrastructure and public facilities are summarized below.

Other Development Projects

Other plan areas will participate in funding facilities for which they have a shared benefit with the WRTP. Specifically, future plan areas SP-1B and SP-1C will have a funding share for construction of the Parkland Avenue overcrossing, and the SLSP will have a funding share for the Highway 113/County Road 25A interchange.

Private Developers

Certain facilities will be the responsibility of individual project developers to fund. The WRTP developers will be responsible for funding a portion of the streets, storm drainage, water, and sewer costs associated with the main internal roads and Road B.

City

The City will contribute to the funding of several projects. Specifically, the City will help fund the Highway 113/County Road 25A interchange with MPFP funding and the existing sewer lift station upgrades with sewer enterprise funding and will fully fund the water system for the parks and open space.

Other

Offsite storm drainage improvements will benefit the WRTP and other areas of the City. The RTIF Program includes some funding for these improvements, but the source of the remaining required funding (approximately \$7.5 million) has not been decided. It is anticipated that a portion of the funding will come from citywide development fees.

Land-Secured Financing

This Financing Plan includes the potential use of land-secured financing for a portion of the backbone infrastructure and public facilities costs. Although this Financing Plan identifies sources of funding for all the included backbone infrastructure and public facilities, major facility oversizing and substantial up-front capital outlays may be required for certain projects. Land-secured financing, in the form of either a Mello-Roos CFD or an Assessment District, may be used to provide debt financing for some of these oversized Facilities:

- **Mello-Roos CFD.** The Mello-Roos Community Facilities Act of 1982 enables public agencies to form CFDs and levy a special tax on property owners in those CFDs. These special taxes may be used to pay debt service on CFD bonds or to finance public improvements directly on a pay-as-you-go (PAYGO) basis.
- **Assessment Districts.** California statutes give local governments the authority to levy several special assessments for specific public improvements such as streets, storm drains, sewers, streetlights, curbs, gutters, and sidewalks. The agency creates a special Assessment District that defines both the area to benefit from the improvements and the properties that will pay for the improvements.

A CFD is the most likely form of land-secured financing to be used to mitigate up-front costs of construction or acquisition of backbone infrastructure and public facilities in the Project, and it is anticipated that Project developers may request that the City form a CFD on all or a portion of the Project.

The proceeds from a CFD bond sale can be used for direct funding of improvements, to acquire facilities constructed by the developer, to reimburse developers for advance-funding improvements, or to pay certain development fees. The annual special tax can be used toward bond debt service or to build or reimburse for infrastructure as needed. The proceeds of the Mello-Roos special tax can be used for direct funding of facilities or to service bond debt.

Tables 5-2 and 5-3 show a preliminary estimate of the Mello-Roos CFD bonding capacity of the Project, based on assumptions regarding tax rates, reserve fund requirements, and interest rates. Based on current assumptions, which are estimates for purposes of example in this document, the Project is estimated to have capacity to bond for approximately \$86.4 million, of which \$71.4 million could be available to fund Project infrastructure costs. Actual tax rates and related bond capacity will be established at the time of formation of the CFD. **Table 5-4** shows an overall estimated value to lien ratio of 20:1 at buildout.

CFD Boundaries and Improvement Areas

The City anticipates one or more property owners may wish to use land-secured financing to help fund a portion of WRTP infrastructure and public facility costs. In anticipation of these requests, this Financing Plan sets forth City preferences regarding future infrastructure CFD formations in the WRTP. A current trend for land-secured infrastructure financing for larger master-planned projects is to include phases of development (or even sub-phases) as Improvement Areas either on a stand-alone basis or within one "project-wide CFD." The City has indicated a preference for one project-wide CFD that could accommodate a number of separate Improvement Areas within that project-wide CFD. The way this structure is anticipated for WRTP is that an initial phase of the Project will be included in the formation of the projectwide CFD, as Improvement Area No. 1, with remaining phases identified in the formation documents and CFD boundary maps as a "future annexation area."

Under the Mello-Roos Act, each Improvement Area is its own geographical area which can be stand-alone or more typically within the framework of a larger, project-wide CFD that will serve as the umbrella CFD for the Project. What this means is that special taxes imposed within each Improvement Area will be the sole source of repayment for debt service for CFD Bonds issued under the authorization for that Improvement Area. Parcels and landowners in other Improvement Areas of the CFD will not be obligated to bond repayment obligations of other Improvement Areas. This CFD bond structure is favored by bond underwriters and investors and is commonly used throughout the state.

Under the proposed structure, Improvement Area No. 1 would be the initial Improvement Area under a projectwide WRTP CFD. Future areas desiring to use land secured financing may elect to annex into the existing Improvement Area No. 1 or may elect to form a new Improvement Area No. 2. The City retains discretion regarding whether to permit annexations into prior Improvement Areas. One important consideration for City policymakers is whether the proposed annexation might lengthen the duration of the special tax levy for the initial property owners within the initial Improvement Area.

**Table 5-2
Woodland Research and Technology Park Financing Plan
Estimated Bond Sizing (2023\$)**

Item	Assumption	Estimated Bond Sizing
Maximum Special Taxes Available for Debt Service		
Estimated Annual Maximum Special Taxes [1]		\$6,337,090
Less Estimated Administration Costs	4.00%	(\$254,000)
Less Delinquency Coverage	10.00%	(\$634,000)
Adjustment for Rounding		\$910
Estimated Gross Debt Service (Rounded)		\$5,450,000
Total Bond Size		
Total Bond Size without Tax Escalation		\$75,019,000
Adjustment for Rounding		\$81,000
Total Bond Size (Rounded)		\$75,100,000
Increase for Annual Escalation [2]	15%	\$11,265,000
Total Bond Size (Rounded)		\$86,365,000
Estimated Bond Proceeds		
Total Bond Size (Rounded)		\$86,365,000
Less Capitalized Interest	12 months	(\$5,182,000)
Less Bond Reserve Fund	1-yr. debt service	(\$5,450,000)
Less Issuance Cost	5.00%	(\$4,318,000)
Estimated Bond Proceeds		\$71,415,000
Assumptions [3]		
Interest Rate	6.00%	
Term	30 years	
Annual Escalation	2%	

Source: City of Woodland; EPS.

[1] See Table 5-3.

[2] Assumes special taxes are escalated 2.0% annually for 30 years, which increases total bond size by approximately 15% to 20%. This estimate uses 15% to be conservative.

[3] Estimated bond sizing based on conservative assumptions. The interest rate will be determined at the time of the bond sale. This analysis is based on an assumed bond term of 30 years.

Table 5-3
Woodland Research and Technology Park Financing Plan
Estimated Bond Proceeds (2023\$)

Item	Units/ Acres	Prelim. Max. Special Tax Rate	Maximum Special Tax		Bond Size [1]		Bond Proceeds	
			Amount	% of Total	Amount	Per Unit/Acre	Amount	Per Unit/Acre
<i>Formula</i>	<i>A</i>	<i>B</i>	<i>C = A * B</i>	<i>D = C / Total Max Tax</i>	<i>E = D x total bond</i>	<i>F = E / A</i>	<i>G = D x bond proceeds</i>	<i>H = G / A</i>
Residential	<u>units</u>	<u>per unit</u>				<u>per unit</u>		<u>per unit</u>
Low Density	628	\$3,700	\$2,323,600	37%	\$31,667,171	\$50,425	\$26,185,504	\$41,697
Medium Density	524	\$3,200	\$1,676,800	26%	\$22,852,261	\$43,611	\$18,896,476	\$36,062
High Density	518	\$2,000	\$1,036,000	16%	\$14,119,121	\$27,257	\$11,675,065	\$22,539
Subtotal Residential Land Uses	1,670		\$5,036,400	79%	\$68,638,553		\$56,757,046	
Nonresidential		<u>per acre</u>						<u>per acre</u>
Village Center/Community Commercial	16.43	\$10,000	\$164,260	3%	\$2,238,617	\$136,285	\$1,851,106	\$112,694
Business Park	113.64	\$10,000	\$1,136,430	18%	\$15,487,831	\$136,285	\$12,806,848	\$112,694
Subtotal Nonresidential	130.07		\$1,300,690	21%	\$17,726,447		\$14,657,954	\$112,694
Total [2]			\$6,337,090	100.00%	\$86,365,000		\$71,415,000	

Source: City of Woodland; EPS.

[1] Assumes special taxes are escalated 2.0% annually for 30 years, which increases total Bond Size by approximately 20%.

[2] See Table 5-2 for total bond size and total bond proceeds.

Table 5-4
Woodland Research and Technology Park Financing Plan
Project Buildout Value-to-Lien Ratio (2023\$)

Item	Residential			Commercial		Total
	Low Density	Medium Density	High Density	Village Center/ Com. Commercial	Business Park	
Estimated Project Buildout Value						
Dwelling Units / Building Square Feet	628	524	518	310,948	1,980,115	
Sales Price per Unit/Square Foot	\$675,000	\$550,000	\$375,000	\$350	\$350	
Total Project Buildout Value	\$423,900,000	\$288,200,000	\$194,250,000	\$108,831,800	\$693,040,250	\$1,708,222,050
Estimated Bond Size						\$86,365,000
Estimated Buildout Value-to-Lien Ratio						20:1

Source: City of Woodland; EPS.

Finally, although this Financing Plan includes placeholder special tax estimates for nonresidential land uses, for purposes of stimulating economic development and encouraging nonresidential development in the Project, the City may wish to exclude nonresidential development from the CFD or not have nonresidential development annex into the CFD. This Financing Plan is identifying this City preference and does not preclude inclusion or exclusion of nonresidential property from the proposed infrastructure CFD.

Phasing and the Financing Strategy

Phasing of public facility construction is an important component of the overall financing strategy. The ability to sequence public facilities will depend on the type of facility and the pace of new development. When possible, construction of public facilities will be sequenced over time as needed to serve new development. The sequencing of public facility costs will help ensure that adequate monies are available from the various financing sources to fund the public facility improvements.

Completion of backbone infrastructure and other public facilities will be phased to serve logical increments of development, based on the demand for such facilities as the WRTP builds out. The timing and amount of development in each increment will depend on many factors, such as market demand. In the normal course of the development approval process, the City will condition the WRTP's tentative maps with backbone infrastructure and other public facility requirements.

The Financing Plan is designed to be flexible enough to accommodate faster or slower growth of WRTP development in response to the market for housing and nonresidential development.

The developers of the WRTP will be responsible for advance funding and constructing all of the backbone infrastructure and public facilities needed to serve the WRTP, unless the City and WRTP proponents agree otherwise to City construction of specific improvements. Subject to the City's fee credit and reimbursement policies and the terms of the Master Reimbursement Agreement entered into by the City and all landowners/developers, some or all of this private funding will be reimbursed to the landowners/developers over time as the City is able to issue public debt through the CFD, issue credits due for landowner/developer proportionate share of fees and collect fees from other developers that will provide reimbursements. The timeframe for reimbursement is unknown and could be a considerable period of time depending on market conditions and the actual absorption of the development projects. There is no guarantee the initial developers will be fully reimbursed for the costs to oversize facilities for later development projects.

6. Woodland Research and Technology Park Plan-Area Fee Program

This Financing Plan proposes adoption of a new plan area development impact fee program, the Woodland Research and Technology Park Infrastructure Fee Program (RTIF Program), that will fund the backbone infrastructure and public facilities needed to serve the WRTP. The proposed RTIF Program is designed to fund construction of backbone infrastructure and public facilities necessary to accommodate new residents and employees generated by WRTP development after taking into consideration a variety of other funding sources for the improvements. The costs to be funded through the RTIF Program were detailed in **Chapter 4**.

The Draft WRTP Nexus Study was prepared to establish the RTIF Program and the legally required nexus findings for the City to implement the RTIF Program and to set the initial development impact fees (RTIF or fees). The Nexus Study also details key components of the implementation and administration of the proposed RTIF Program. The RTIF Program may be updated periodically to reflect changes in land uses, facilities requirements, improvement costs, and other fee program information over time.

The key features of the RTIF program are summarized in this chapter.

RTIF Program Overview

The RTIF Program will be a City-implemented plan-area-specific development impact fee program applicable only to WRTP land uses. It will be used as a mechanism to fund backbone infrastructure and public facilities needed to serve the projected new residents and employees that are not funded by other sources. The RTIF Program will contain the following fee components:

- Streets
- Parks
- Storm Drainage
- Water
- Sewer
- Administration

Table 6-1 summarizes the proposed RTIF by residential and commercial land use for each of the fee components. For each land use, the administration component fee is calculated as four percent of the sum of all other fees and will fund administration of the RTIF Program. All RTIF components will be charged and collected per residential dwelling unit or commercial acre and are consistent with the requirements of California Assembly Bill (AB) 1600 legislation, as codified by the Mitigation Fee Act (California Government Code sections 66000 et. seq.).

While the commercial fee categories (Village Commercial/Community Commercial and Business Park) are the same for all RTIF components, there are different residential fee categories for different fee components to address provisions of California Assembly Bill 602 (AB 602), enacted in 2021. AB 602 modified the Mitigation Fee Act and stipulates that a jurisdiction must calculate residential fees proportionately to the square footage of the dwelling units, with the exception of water and sewer fees and any other fees for which specific findings can be made as to why the fees should not be enacted on a square foot basis.

In response to the provisions of AB 602, some of the RTIF components must have fees that differ by dwelling unit size while other components do not need to have different fees for different unit sizes. The proposed residential fee land uses for all fee components are summarized below:

Streets and Parks Residential Fee Components

- Single Family <1,000 sq. ft.
- Single Family 1,000 – 1,999 sq. ft.
- Single Family 2,000 – 2,999 sq. ft.
- Single Family >2,999 sq. ft.
- Multifamily <1,000 sq. ft.
- Multifamily ≥1,000 sq. ft.

Storm Drainage, Water, and Sewer Residential Fee Components

- Low Density
- Medium Density
- High Density

The proposed fees are based on the best land use information, backbone infrastructure and public facilities costs estimates, funding source information, and administrative cost estimates available at this time. The land uses, cost estimates, and funding sources are detailed in prior chapters of this report and are also included in the Nexus Study.

**Table 6-1
Woodland Research and Technology Park Financing Plan
RTIF Program Fee Summary (2023\$)**

Land Use Category	Streets	Parks	Storm Drainage	Water	Sewer	Subtotal	Admin.	Total
<i>Percentage</i>							4.00%	
Residential by Land Use	<i>per unit</i>							
Low Density (LDR)	-	-	\$6,952	\$2,907	\$1,176	\$11,036	\$441	\$11,477
Medium Density (MDR)	-	-	\$4,742	\$2,475	\$910	\$8,127	\$325	\$8,452
High Density (HDR)	-	-	\$3,085	\$1,759	\$819	\$5,663	\$227	\$5,890
Residential by Square Foot Range	<i>per unit</i>							
Single Family (LDR and MDR)	<i>per unit</i>							
< 1,000 sq. ft.	\$12,800	\$8,199	-	-	-	\$20,999	\$840	\$21,839
1,000 to 1,999 sq. ft.	\$20,696	\$14,450	-	-	-	\$35,146	\$1,406	\$36,552
2,000 to 2,999 sq. ft.	\$29,904	\$22,380	-	-	-	\$52,284	\$2,091	\$54,375
> 2,999 sq. ft.	\$36,547	\$27,555	-	-	-	\$64,102	\$2,564	\$66,666
Multifamily (HDR)	<i>per unit</i>							
< 1,000 sq. ft.	\$12,256	\$14,450	-	-	-	\$26,705	\$1,068	\$27,773
≥1000 sq. ft.	\$13,606	\$16,533	-	-	-	\$30,139	\$1,206	\$31,345
Residential for Average Unit Size	<i>per unit</i>							
Low Density (2,300 sq. ft.)	\$29,904	\$22,380	\$6,952	\$2,907	\$1,176	\$63,321	\$2,533	\$65,854
Medium Density (1,850 sq. ft.)	\$20,696	\$14,450	\$4,742	\$2,475	\$910	\$43,273	\$1,731	\$45,004
High Density (990 sq. ft.)	\$12,256	\$14,450	\$3,085	\$1,759	\$819	\$32,368	\$1,295	\$33,663
Commercial [1]	<i>per acre</i>							
Village Center/Community Commercial	\$189,072	N/A	\$22,213	\$2,580	\$2,596	\$216,461	\$8,658	\$225,119
Business Park	\$86,378	N/A	\$22,213	\$2,346	\$1,731	\$112,667	\$4,507	\$117,174
Commercial [1]	<i>per bldg. sq. ft.</i>							
Village Center/Community Commercial	\$9.99	N/A	\$1.17	\$0.14	\$0.14	\$11.43	\$0.46	\$11.89
Business Park	\$4.96	N/A	\$1.27	\$0.13	\$0.10	\$6.47	\$0.26	\$6.73

Source: City of Woodland; EPS

[1] Commercial fees will be charged per acre. The amount per bldg. sq. ft. is shown for comparative purposes.

RTIF Program Implementation and Administration

Implementation

The proposed RTIF Program is anticipated to be approved by the City through an ordinance establishing and authorizing collection of the fee and a resolution setting the amount of the fees. The RTIF Program will be independent and separate from all other City, County, other agency, or regional development impact fees that will be applicable to WRTP development.

Note that as part of the tentative map process, the developers will be required to submit proposed development plans. If there has been a change to the proposed land uses or number of dwelling units that results in less RTIF revenue generation than anticipated under the original land uses, then the developer will be required to make a shortfall payment to cover the difference at the time that a final map is filed.

Administration

The City will administer the RTIF Program. The RTIF Program includes a four-percent planning and administration component to fund RTIF Program administration. Administration of the RTIF Program includes, but is not limited to, the following major tasks:

- Tracking all RTIF Program payments and assignment of fee credits and reimbursements for improvements constructed privately by developers.
- Tracking the progress of construction contracts for RTIF Program improvements.
- Performing annual fee adjustments to account for the inflation of infrastructure and public facilities costs.
- Conducting periodic reviews of development projections, facilities requirements, improvement costs, funding sources, and cost allocation methodologies and making necessary adjustments to the RTIF Program.
- Preparing the required annual and five-year reports on the status of the RTIF Program.

The RTIF Program implementation and administration are further detailed in the Nexus Study.

7. Feasibility of the Financing Plan

This chapter quantifies and evaluates the total estimated infrastructure cost burden and total effective annual tax rate anticipated for new WRTP development and makes observations regarding the financial feasibility of the Financing Plan. The financial feasibility is addressed by reviewing a total infrastructure burden analysis, as well as bond issuance guidelines, to ensure the Finance Plan will meet the required financial tests.

Description of Static Feasibility Analyses

This analysis includes the following static methods for evaluating the financial feasibility of the WRTP:

- Total Backbone Infrastructure and Public Facilities Cost Burden.
- Total Taxes and Assessments as a Percentage of Sales Price.

Each of these methods is based on a static financial feasibility evaluation. To be considered financially feasible, the Project should meet each of the static feasibility tests.

It is important to note that these feasibility metrics, described in detail below, should be considered initial diagnostics, offering a general indicator of whether or not a project is likely to meet financial feasibility criteria or whether measures should be taken to improve viability, either through a reduction in cost burdens, identification of other funding sources, or other approaches. None of the indicators, by themselves, should be considered absolute determinations regarding Project feasibility.

Total Backbone Infrastructure and Public Facilities Cost Burden

It is common for developers of major development projects to advance-fund and carry infrastructure costs for some timeframe. The impact of the land developer's cost burden depends on several factors, including the timeframe for the reimbursements and the extent to which full reimbursement is received, either through public funding programs or through adjustments in land sales prices.

The purpose of the total backbone infrastructure and public facilities cost burden feasibility test is to assess the financial feasibility of the WRTP, given all current and proposed fees and the additional burden of WRTP-specific infrastructure costs. This feasibility test assesses the total fee burden on residential dwelling units associated with the proposed backbone infrastructure and public facilities.

The total backbone infrastructure and public facilities cost burden feasibility test provides a performance indicator of a project's feasibility. For each residential land use, the total cost burden per dwelling unit and nonresidential building square foot is expressed as a percent of the finished sales price. Project feasibility is evaluated based on the following general guidelines or benchmarks:

- Burdens below 15 percent generally are considered financially feasible.
- Burdens between 15 and 20 percent may be feasible depending on the specific circumstances of the project.
- Burdens above 20 percent suggest a project may not be financially feasible unless other components of the project pro forma are particularly advantageous to the developer, thus allowing the project to bear unusually high backbone infrastructure and public facilities costs.¹

These static feasibility benchmarks are based on EPS's experience conducting financial feasibility analyses for numerous projects throughout the Sacramento Region and Central Valley over the last 3 decades. This feasibility diagnostic is merely a tool that can be used—along with other tools—as a general measure of financial feasibility. This measure should not automatically be interpreted to mean that if one land use type exceeds the threshold, the project definitely is infeasible. In certain circumstances, there are ways in which a development project can mitigate against a high cost burden. In addition, the backbone infrastructure and public facilities costs will be fine-tuned and possibly reduced as engineering studies are completed closer to actual construction.

¹ Such other components may include extraordinarily low land basis (e.g., land has been in the family for a long time, land acquired during severe real estate market downturn, etc.), development phasing (e.g., fast early absorption ahead of a major infrastructure cost such as a new water treatment plant), or low or no environmental mitigation requirements (e.g., through avoidance or on-site preservation).

As summarized in **Table 7-1** (and further detailed in **Appendix B**), the total backbone infrastructure and public facilities cost burden accounts for between approximately 19.2 percent and 19.8 percent of the estimated sales price of residential units in the WRTP. Cost burdens of this magnitude are at the upper end of the diagnostic range indicating that the Project may be financially feasible if home sales values do not taper or if infrastructure costs don't continue to escalate rapidly. Other factors such as the magnitude of advance funding requirements within each development phase, anticipated reimbursement timeframes, and development absorption would also factor into the feasibility of individual development phases. Although fee burdens for new development in Woodland plan areas typically are close to 20 percent, the City generally experiences strong building permit activity except under the most difficult economic circumstances.

The infrastructure cost burden could change for several reasons, including a re-allocation of costs among land uses and cost reductions resulting from fine-tuning the estimates as engineering studies are completed and the WRTP development becomes closer to implementation. The cost burden estimates will continue to be examined and may be refined as the WRTP development is implemented.

Total Taxes and Assessments as a Percentage of Sales Price

The Total Taxes and Assessments as a Percentage of Sales Price feasibility test often is referred to as a "two-percent test." This test provides another measure of the financial feasibility of a project that is used by land developers, builders, and municipal governments to evaluate development projects. The Total Taxes and Assessments as a Percentage of Sales Price test provides a general rule for the feasibility of proposed annual special taxes and assessments. In general, if the sum of property taxes, other ad valorem taxes, and all annual special taxes and assessments is less than 2 percent of the average finished home sales price, then the burden of annual taxes and assessments is considered financially feasible. In the Sacramento Region, jurisdictions and developers typically target total taxes and assessments at levels no greater than approximately 1.6 percent to 1.8 percent of the finished home sales price.

Table 7-2 shows the estimated taxes and assessments as a percentage of home sales prices for three different proposed Project land uses. The total annual amount includes the following taxes and assessments:

- Property taxes
- Other general ad valorem taxes (e.g., school/other General Obligation bonds)
- Services CFD taxes
- Infrastructure CFD taxes (proposed in this Finance Plan)

Table 7-1
Woodland Research and Technology Park Financing Plan
Summary of Infrastructure Burden per Unit or Building Square Foot (2023\$)

Item	Fee per Unit			Fee per Building Square Foot	
	Low Density	Medium Density	High Density	Village Center/ Com. Commercial	Business Park
Sales Price per Dwelling Unit/Sq. Ft.	\$675,000	\$550,000	\$375,000	\$350	\$350
City Building Permit Processing Fees	\$3,861	\$3,608	\$1,274	\$1.44	\$1.83
City Development Impact Fees					
General City	\$1,005	\$1,005	\$755	\$0.67	\$0.79
Fire	\$3,406	\$3,406	\$2,042	\$1.99	\$2.15
Library	\$628	\$628	\$471	\$0.00	\$0.00
Police	\$823	\$823	\$616	\$0.55	\$0.65
Wastewater	\$7,524	\$7,524	\$5,642	\$3.07	\$1.74
Parks & Recreation	\$9,279	\$9,279	\$6,959	\$0.00	\$0.00
Roads	\$9,915	\$9,915	\$4,063	\$12.31	\$6.23
Water	\$6,174	\$6,174	\$977	\$0.42	\$0.46
Administration	\$291	\$291	\$154	\$0.14	\$0.09
Affordable Housing - Proposed	\$6,900	\$4,625	\$1,980	\$0.00	\$0.00
Agricultural Mitigation	\$2,500	\$1,250	\$625	\$0.79	\$0.86
Public Transit Fee (due at final map)	\$311	\$311	\$218	\$0.00	\$0.00
Total Citywide Development Impact Fees	\$48,756	\$45,231	\$24,502	\$19.94	\$12.97
Other Agency Fees					
County Facilities and Services Authorization (FSA)	\$3,118	\$3,118	\$2,312	\$0.50	\$0.67
School District Fees	\$9,384	\$7,548	\$4,039	\$0.66	\$0.66
Yolo County HCP Fee	\$2,700	\$1,350	\$675	\$0.86	\$0.93
Total Other Agency Fees	\$15,202	\$12,016	\$7,026	\$2.02	\$2.26
RTIF - Proposed	\$65,854	\$45,004	\$33,663	\$11.89	\$6.73
Total Fees	\$133,673	\$105,859	\$66,465	\$35.30	\$23.78
Percentage of Sales Price	19.8%	19.2%	17.7%	10.1%	6.8%

Source: City of Woodland; Yolo County; WJUSD; EPS

Table 7-2
Woodland Research and Technology Park Financing Plan
Test of 2% Sales Price - Residential Market-Rate Units (2023\$)

Item	Pct.	Residential			Commercial	
		LDR	MDR	HDR	Village Center/ Com. Commercial	Business Park
Assumptions		<i>price per unit</i>			<i>price per bldg. sq. ft.</i>	
Sales Price		\$675,000	\$550,000	\$375,000	\$350	\$350
FAR					0.43	0.40
Maintenance CFD Category (assumes SLSP rates)		Avg of R-5 and R-8	R-15	R-25	Non-Res.	Non-Res.
Property Taxes		<i>per unit</i>			<i>per bldg. sq. ft.</i>	
General Property Tax [2]	1.0000%	\$6,750	\$5,500	\$3,750	\$3.50	\$3.50
WJUSD	0.0190%	\$128	\$105	\$71	\$0.07	\$0.07
Yuba CCD 2006	0.0240%	\$162	\$132	\$90	\$0.08	\$0.08
Yuba CCD 2016	0.0081%	\$55	\$45	\$31	\$0.03	\$0.03
Total Ad Valorem Taxes Range	1.0512%	\$7,095	\$5,781	\$3,942	\$3.68	\$3.68
Estimated Special Annual Taxes/Assessments						
Maintenance CFD [1]		\$1,122	\$669	\$599	\$0.20	\$0.20
Sports Park Maintenance CFD [2]		\$218	\$180	\$180	\$0.03	\$0.04
Total Estimated Special Annual Taxes/Assessments		\$1,340	\$850	\$779	\$0.23	\$0.24
Estimated WRTP Infrastructure CFD [3]		\$3,700	\$3,200	\$2,000	\$0.53	\$0.57
Total Annual Taxes and Assessments		\$12,135	\$9,831	\$6,721	\$4.44	\$4.49
Taxes & Assessments as % of Sales Price		1.80%	1.79%	1.79%	1.27%	1.28%

Source: City of Woodland; EPS.

[1] Maintenance CFD residential rates set equal to SLSP rates; nonresidential rates estimated by EPS. Per the City, the following SLSP rates are assumed for the WRTP land use categories:

LDR: average of SLSP R-5 and R-8 rates

MDR: SLSP R-15 rate

HDR: SLSP R-25 rate

[2] Sports Park maintenance CFD rates set equal to SLSP rates. Assumed Spring Lake categories are the same as those shown above in footnote [1].

[3] Residential rates set so that taxes and assessments=1.80% of sales price. Nonresidential rates set at \$10,000 per acre.

It is proposed that development in the WRTP participate in two services CFDs to pay for required services and ongoing maintenance of public improvements. One of the two districts would be a general maintenance CFD to provide funding for services, operations, and maintenance specifically required for WRTP development. In addition, the WRTP will be required to participate in the Citywide Sports Park maintenance CFD. When combined with the potential implementation of infrastructure special taxes that range from \$2,000 to \$3,700 per unit, the WRTP total special taxes and assessments would equal approximately 1.80% of the home sales prices.

8. Financing Plan Implementation

This Financing Plan establishes a detailed and specific financing strategy for the WRTP. This chapter identifies the key administration and implementation actions and policies that will be required to carry out the financing strategy.

Implementation of the Financing Plan, along with EIR mitigation measures, DA provisions, and Master Reimbursement Agreement provisions will ensure that the public improvements needed to serve the WRTP are constructed consistent with the Specific Plan requirements. The City will implement the Financing Plan, which will include the following near-term and ongoing actions related specifically to the WRTP:

- Approval of the Specific Plan.
- Approval of the DAs and the Master Reimbursement Agreement between the developers and the City.
- Adoption of the Nexus Study and establishment of the RTIF Program.
- Coordinating closely with all appropriate City departments and other service providers to implement the Financing Plan.
- Working with State and Federal agencies to secure State and Federal funding, where available and applicable.
- Coordinating the infrastructure construction program and funding sources with the developers.
- Periodically reviewing and updating the CIP.
- Monitoring identified revenue sources.
- Estimating fee program cash flows.
- Forming CFD(s) or other financing districts and administering subsequent bond sales and tax collection.
- Accounting for RTIF payments, credits, and reimbursements.
- Updating and adjusting the RTIF Program as new infrastructure cost, land use, and revenue information become available.

Updates

It is anticipated that as the Financing Plan is implemented, there may be changes to anticipated development, public facilities costs, and funding sources. Changes should be evaluated in the context of the overall financing strategy framework, ensuring that required funding is available when needed. The costs and funding sources for the Project may need to be adjusted periodically to reflect changes in the Specific Plan.

Such changes over time may be updated or reflected in the documents and City actions taken to implement various financing mechanisms identified in this document rather than revisiting or updating the Financing Plan itself. The City has the discretion to determine whether the Financing Plan itself should be updated or whether applicable updates should be made to the implementing documents themselves.



APPENDICES:

- Appendix A: Summary of WRTP Capital Improvement Program
- Appendix B: Development Impact Fee Analysis
- Appendix C: Infrastructure Cost Burden Comparison

APPENDIX A:
Summary of WRTP
Capital Improvement Program



Table A-1 Backbone Infrastructure Cost Estimates (2 pages)A-1

Table A-1
Woodland Research and Technology Park Financing Plan
Backbone Infrastructure Cost Estimates (2023\$)

Item	Assumption	Total Estimated Costs							RTIF						
		Streets [1]	Storm Drainage	Water	Sewer	Parks and Open Space	Planning and Admin.	Total	Streets [1]	Storm Drainage	Water	Sewer	Parks and Open Space	Planning and Admin.	Total
County Road 25A (Hwy 113 to Harry Lorenzo and Parkland to Miekle)															
Construction		\$4,173,000	\$249,000	\$374,000	\$0	\$0	\$0	\$4,796,000	\$4,173,000	\$249,000	\$374,000	\$0	\$0	\$0	\$4,796,000
Contingency	10%	\$418,000	\$25,000	\$37,000	\$0	\$0	\$0	\$480,000	\$418,000	\$25,000	\$37,000	\$0	\$0	\$0	\$480,000
Soft Cost	15%	\$626,000	\$37,000	\$56,000	\$0	\$0	\$0	\$719,000	\$626,000	\$37,000	\$56,000	\$0	\$0	\$0	\$719,000
Subtotal County Road 25A		\$5,217,000	\$311,000	\$467,000	\$0	\$0	\$0	\$5,995,000	\$5,217,000	\$311,000	\$467,000	\$0	\$0	\$0	\$5,995,000
Parkland Avenue															
Construction		\$5,451,000	\$652,000	\$369,000	\$0	\$0	\$0	\$6,472,000	\$5,451,000	\$652,000	\$369,000	\$0	\$0	\$0	\$6,472,000
Contingency	10%	\$545,000	\$65,000	\$37,000	\$0	\$0	\$0	\$647,000	\$545,000	\$65,000	\$37,000	\$0	\$0	\$0	\$647,000
Soft Cost	15%	\$818,000	\$98,000	\$55,000	\$0	\$0	\$0	\$971,000	\$818,000	\$98,000	\$55,000	\$0	\$0	\$0	\$971,000
Subtotal Parkland Avenue		\$6,814,000	\$815,000	\$461,000	\$0	\$0	\$0	\$8,090,000	\$6,814,000	\$815,000	\$461,000	\$0	\$0	\$0	\$8,090,000
Main Internal Roadways															
Construction		\$12,008,000	\$3,723,000	\$1,914,000	\$1,579,000	\$0	\$0	\$19,224,000	\$5,246,000	\$2,113,000	\$1,614,000	\$1,041,000	\$0	\$0	\$10,014,000
Contingency	10%	\$1,201,000	\$372,000	\$191,000	\$158,000	\$0	\$0	\$1,922,000	\$525,000	\$211,000	\$161,000	\$104,000	\$0	\$0	\$1,001,000
Soft Cost	15%	\$1,801,000	\$558,000	\$287,000	\$237,000	\$0	\$0	\$2,883,000	\$787,000	\$317,000	\$242,000	\$156,000	\$0	\$0	\$1,502,000
Subtotal Main Internal Roadways		\$15,010,000	\$4,653,000	\$2,392,000	\$1,974,000	\$0	\$0	\$24,029,000	\$6,558,000	\$2,641,000	\$2,017,000	\$1,301,000	\$0	\$0	\$12,517,000
Road B															
Mobility Hub (includes 25% contingency)		\$862,000	\$0	\$0	\$0	\$0	\$0	\$862,000	\$862,000	\$0	\$0	\$0	\$0	\$0	\$862,000
Other Construction		\$10,048,000	\$839,000	\$1,118,000	\$467,000	\$0	\$0	\$12,472,000	\$10,048,000	\$839,000	\$1,118,000	\$101,000	\$0	\$0	\$12,106,000
Contingency (on Other Construction)	10%	\$1,005,000	\$84,000	\$112,000	\$47,000	\$0	\$0	\$1,248,000	\$1,005,000	\$84,000	\$112,000	\$10,000	\$0	\$0	\$1,211,000
Soft Cost	15%	\$1,637,000	\$126,000	\$168,000	\$70,000	\$0	\$0	\$2,001,000	\$1,637,000	\$126,000	\$168,000	\$15,000	\$0	\$0	\$1,946,000
Subtotal Road B		\$13,552,000	\$1,049,000	\$1,398,000	\$584,000	\$0	\$0	\$16,583,000	\$13,552,000	\$1,049,000	\$1,398,000	\$126,000	\$0	\$0	\$16,125,000
Harry Lorenzo - Half Section North [2]		\$387,000	\$0	\$0	\$0	\$0	\$0	\$387,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Hwy 113/CR 25A Interchange															
Right of Way and Utilities (no soft costs/cont.)		\$250,000	\$0	\$0	\$0	\$0	\$0	\$250,000	\$132,000	\$0	\$0	\$0	\$0	\$0	\$132,000
Construction (incl. 20% contingency)		\$10,307,000	\$0	\$0	\$0	\$0	\$0	\$10,307,000	\$5,463,000	\$0	\$0	\$0	\$0	\$0	\$5,463,000
Construction Management	15%	\$1,546,000	\$0	\$0	\$0	\$0	\$0	\$1,546,000	\$819,000	\$0	\$0	\$0	\$0	\$0	\$819,000
Soft Cost	15.56%	\$1,604,000	\$0	\$0	\$0	\$0	\$0	\$1,604,000	\$850,000	\$0	\$0	\$0	\$0	\$0	\$850,000
Subtotal SR 113/CR 25A Interchange		\$13,707,000	\$0	\$0	\$0	\$0	\$0	\$13,707,000	\$7,264,000	\$0	\$0	\$0	\$0	\$0	\$7,264,000
Parkland Avenue Overcrossing															
Construction		\$7,863,000	\$0	\$0	\$0	\$0	\$0	\$7,863,000	\$3,590,000	\$0	\$0	\$0	\$0	\$0	\$3,590,000
Construction Management	25%	\$1,966,000	\$0	\$0	\$0	\$0	\$0	\$1,966,000	\$898,000	\$0	\$0	\$0	\$0	\$0	\$898,000
Soft Cost	15%	\$1,180,000	\$0	\$0	\$0	\$0	\$0	\$1,180,000	\$539,000	\$0	\$0	\$0	\$0	\$0	\$539,000
Subtotal Parkland Avenue Overcrossing		\$11,009,000	\$0	\$0	\$0	\$0	\$0	\$11,009,000	\$5,027,000	\$0	\$0	\$0	\$0	\$0	\$5,027,000
Sewer Lift Station Upgrades															
Construction		\$0	\$0	\$0	\$734,000	\$0	\$0	\$734,000	\$0	\$0	\$0	\$335,000	\$0	\$0	\$335,000
Offsite Contingency	20%	\$0	\$0	\$0	\$147,000	\$0	\$0	\$147,000	\$0	\$0	\$0	\$67,000	\$0	\$0	\$67,000
Soft Cost	15%	\$0	\$0	\$0	\$110,000	\$0	\$0	\$110,000	\$0	\$0	\$0	\$50,000	\$0	\$0	\$50,000
Subtotal Sewer Lift Station Upgrades		\$0	\$0	\$0	\$991,000	\$0	\$0	\$991,000	\$0	\$0	\$0	\$452,000	\$0	\$0	\$452,000
Greenbelts, Parks, and Ponds															
Construction		\$0	\$3,632,000	\$408,000	\$0	\$22,249,000	\$0	\$26,289,000	\$0	\$3,632,000	\$0	\$0	\$21,310,000	\$0	\$24,942,000
Contingency	10%	\$0	\$363,000	\$41,000	\$0	\$2,225,000	\$0	\$2,629,000	\$0	\$363,000	\$0	\$0	\$2,131,000	\$0	\$2,494,000
Soft Cost	15%	\$0	\$545,000	\$61,000	\$0	\$3,337,000	\$0	\$3,943,000	\$0	\$545,000	\$0	\$0	\$3,197,000	\$0	\$3,742,000
Subtotal Greenbelts, Parks, and Ponds		\$0	\$4,540,000	\$510,000	\$0	\$27,811,000	\$0	\$32,861,000	\$0	\$4,540,000	\$0	\$0	\$26,638,000	\$0	\$31,178,000
Off-Site Storm Drainage		\$0	\$8,650,000	\$0	\$0	\$0	\$0	\$8,650,000	\$0	\$1,159,000	\$0	\$0	\$0	\$0	\$1,159,000
Land Allocations		\$2,908,000	\$648,000	\$0	\$0	\$2,035,000	\$0	\$5,591,000	\$1,828,000	\$648,000	\$0	\$0	\$2,035,000	\$0	\$4,511,000
Ag Mitigation and HCP Cost		\$992,000	\$175,000	\$0	\$0	\$273,000	\$0	\$1,440,000	\$992,000	\$175,000	\$0	\$0	\$273,000	\$0	\$1,440,000
Subtotal Costs		\$69,596,000	\$20,841,000	\$5,228,000	\$3,549,000	\$30,119,000	\$0	\$129,333,000	\$47,252,000	\$11,338,000	\$4,343,000	\$1,879,000	\$28,946,000	\$0	\$93,758,000
Planning and Administration [3]	0%	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Total Costs		\$69,596,000	\$20,841,000	\$5,228,000	\$3,549,000	\$30,119,000	\$0	\$129,333,000	\$47,252,000	\$11,338,000	\$4,343,000	\$1,879,000	\$28,946,000	\$0	\$93,758,000

Source: Cunningham Engineering

[1] Includes Grading & Erosion Control, Street Work & Concrete, Dry Utilities, Streetlights, Mobility Hub, and ROW acquisition.

[2] Improvements completed.

[3] Planning and Administration costs from CIP are excluded because they are accounted for in the RTIF Program Administration fee component.

Table A-1
Woodland Research and Technology Park Financing Plan
Backbone Infrastructure Cost Estimates (2023\$)

Item	Assumption	Developer				City				SP-1B/1C	SLSP		Other	
		Streets [1]	Storm Drainage	Water	Sewer	Total	Streets [1]	Water	Sewer	Total	Streets [1]	Streets [1]	Parks and Open Space	Total
County Road 25A (Hwy 113 to Harry Lorenzo and Parkland to Miekle)														
Construction		\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Contingency	10%	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Soft Cost	15%	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Subtotal County Road 25A		\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Parkland Avenue														
Construction		\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Contingency	10%	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Soft Cost	15%	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Subtotal Parkland Avenue		\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Main Internal Roadways														
Construction		\$6,762,000	\$1,610,000	\$300,000	\$538,000	\$9,210,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Contingency	10%	\$676,000	\$161,000	\$30,000	\$54,000	\$921,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Soft Cost	15%	\$1,014,000	\$241,000	\$45,000	\$81,000	\$1,381,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Subtotal Main Internal Roadways		\$8,452,000	\$2,012,000	\$375,000	\$673,000	\$11,512,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Road B														
Mobility Hub (includes 25% contingency)		\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Other Construction		\$0	\$0	\$0	\$366,000	\$366,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Contingency (on Other Construction)	10%	\$0	\$0	\$0	\$37,000	\$37,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Soft Cost	15%	\$0	\$0	\$0	\$55,000	\$55,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Subtotal Road B		\$0	\$0	\$0	\$458,000	\$458,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Harry Lorenzo - Half Section North [2]		\$387,000	\$0	\$0	\$0	\$387,000	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Hwy 113/CR 25A Interchange														
Right of Way and Utilities (no soft costs/cont.)		\$0	\$0	\$0	\$0	\$0	\$63,000	\$0	\$0	\$63,000	\$0	\$55,000	\$55,000	\$0
Construction (incl. 20% contingency)		\$0	\$0	\$0	\$0	\$0	\$2,577,000	\$0	\$0	\$2,577,000	\$0	\$2,267,000	\$2,267,000	\$0
Construction Management	15%	\$0	\$0	\$0	\$0	\$0	\$387,000	\$0	\$0	\$387,000	\$0	\$340,000	\$340,000	\$0
Soft Cost	15.56%	\$0	\$0	\$0	\$0	\$0	\$401,000	\$0	\$0	\$401,000	\$0	\$353,000	\$353,000	\$0
Subtotal SR 113/CR 25A Interchange		\$0	\$0	\$0	\$0	\$0	\$3,428,000	\$0	\$0	\$3,428,000	\$0	\$3,015,000	\$3,015,000	\$0
Parkland Avenue Overcrossing														
Construction		\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$4,273,000	\$0	\$0	\$0
Construction Management	25%	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$1,068,000	\$0	\$0	\$0
Soft Cost	15%	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$641,000	\$0	\$0	\$0
Subtotal Parkland Avenue Overcrossing		\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$5,982,000	\$0	\$0	\$0
Sewer Lift Station Upgrades														
Construction		\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$399,000	\$399,000	\$0	\$0	\$0	\$0
Offsite Contingency	20%	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$60,000	\$60,000	\$0	\$0	\$0	\$0
Soft Cost	15%	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$80,000	\$80,000	\$0	\$0	\$0	\$0
Subtotal Sewer Lift Station Upgrades		\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$539,000	\$539,000	\$0	\$0	\$0	\$0
Greenbelts, Parks, and Ponds														
Construction		\$0	\$0	\$0	\$0	\$0	\$0	\$408,000	\$0	\$408,000	\$0	\$0	\$939,000	\$939,000
Contingency	10%	\$0	\$0	\$0	\$0	\$0	\$0	\$41,000	\$0	\$41,000	\$0	\$0	\$94,000	\$94,000
Soft Cost	15%	\$0	\$0	\$0	\$0	\$0	\$0	\$61,000	\$0	\$61,000	\$0	\$0	\$140,000	\$140,000
Subtotal Greenbelts, Parks, and Ponds		\$0	\$0	\$0	\$0	\$0	\$0	\$510,000	\$0	\$510,000	\$0	\$0	\$1,173,000	\$1,173,000
Off-Site Storm Drainage		\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$7,491,000
Land Allocations		\$538,000	\$0	\$0	\$0	\$538,000	\$0	\$0	\$0	\$0	\$542,000	\$0	\$0	\$0
Ag Mitigation and HCP Cost		\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Subtotal Costs		\$9,377,000	\$2,012,000	\$375,000	\$1,131,000	\$12,895,000	\$3,428,000	\$510,000	\$539,000	\$4,477,000	\$6,524,000	\$3,015,000	\$1,173,000	\$4,188,000
Planning and Administration [3]	0%	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0	\$0
Total Costs		\$9,377,000	\$2,012,000	\$375,000	\$1,131,000	\$12,895,000	\$3,428,000	\$510,000	\$539,000	\$4,477,000	\$6,524,000	\$3,015,000	\$1,173,000	\$4,188,000

Source: Cunningham Engineering

[1] Includes Grading & Erosion Control, Street Work & Concrete, Dry Utilities, Streetlights, Mobility Hub, and ROW acquisition.

[2] Improvements completed.

[3] Planning and Administration costs from CIP are excluded because they are accounted for in the RTIF Program Administration fee component.



APPENDIX B: Development Impact Fee Analysis

Table B-1	Single-Family Development Fees	B-1
Table B-2	Multifamily Development Fees	B-2
Table B-3	Nonresidential Development Fees (2 pages)	B-3
Table B-4	Estimated WRTP Fee Revenue for Backbone Infrastructure and Public Facilities.....	B-5

Table B-1
Woodland Research and Technology Park Financing Plan
Single Family Development Fees (2023\$)

Development Impact Fees	Description/Formula	Fee per Unit	
		Low Density	Medium Density
Assumptions			
Building Valuation Group		R-3	R-3
Construction Type		VB	VB
Valuation per sq. ft. (VB construction type Feb. 2023)		\$167.37	\$167.37
Garage Valuation per sq. ft. (VB construction type Feb. 2023)		\$66.48	\$66.48
Square Feet		2,300	1,850
Garage Square Feet		450	400
Units per Acre		6	12
Building Valuation per Unit		\$384,951	\$309,635
Garage Valuation per Unit		\$29,916	\$26,592
Total Unit Valuation		\$414,867	\$336,227
City Building Permit Processing Fees (effective July 1, 2022)			
		<i>Fee per Unit</i>	
Building Inspection and Plan Review Fee [1]		\$1,641	\$1,402
Green Building Plan Review		\$276	\$276
Permit Processing Charge/Application		\$8	\$8
Seismic/Strong Motion	\$0.00013 of total valuation	\$54	\$44
California Building Standards Commission Fee	\$1.00 per \$25,000 value	\$17	\$14
Electrical Permit Processing		\$58	\$58
Electrical Inspection	10% of building fee; \$200 min.	\$200	\$200
Plumbing Permit Processing		\$58	\$58
Plumbing Inspection	20% of building fee; \$400 min.	\$400	\$400
Mechanical Permit Processing		\$58	\$58
Mechanical Inspection	10% of building fee; \$200 min.	\$200	\$200
Solar Plan Review and Inspection		\$280	\$280
Energy Compliance Inspections		\$55	\$55
Fire Sprinklers Plan Check and Review		\$556	\$556
Total Processing Fees		\$3,861	\$3,608
City Development Impact Fees (effective Jan. 1, 2023)			
General City		\$1,005	\$1,005
Fire		\$3,406	\$3,406
Library		\$628	\$628
Police		\$823	\$823
Wastewater		\$7,524	\$7,524
Parks & Recreation		\$9,279	\$9,279
Roads		\$9,915	\$9,915
Water		\$6,174	\$6,174
Administration		\$291	\$291
Affordable Housing - Proposed	Fee per sq. ft.: LDR: \$3; MDR: \$2.50	\$6,900	\$4,625
Agricultural Mitigation [2]	\$15,000 per acre	\$2,500	\$1,250
Public Transit Fee (due at final map)		\$311	\$311
Total City Development Impact Fees		\$48,756	\$45,231
Other Agency Fees			
County Facilities and Services Authorization (effective 9/13/16)		\$3,118	\$3,118
School District Fees [3]	\$4.08 per sq. ft.	\$9,384	\$7,548
Yolo County HCP Fee (effective 3/27/23)	\$16,202 per acre	\$2,700	\$1,350
Total Other Agency Fees		\$15,202	\$12,016
RTIF - Proposed		\$65,854	\$45,004
Total Fees per Unit		\$133,673	\$105,859

Source: City of Woodland; Yolo County; WJUSD; EPS

[1] First 1,000 sq. ft.: \$946
1,000-2,000 sq. ft.: \$53.60 for each 100 sq. ft. or fraction thereof
2,000-3,000 sq. ft.: \$53.10 for each 100 sq. ft. or fraction thereof

[2] Estimated amount. Different requirements for different development and mitigation land.

[3] Fee for all areas of Woodland except Spring Lake. Spring Lake rate is \$6.13 per sq. ft.

Table B-2
Woodland Research and Technology Park Financing Plan
Multifamily Development Fees (2023\$)

Development Impact Fees	Description/Formula	Multifamily (High Density)	
		Per Permit	Per Unit
Assumptions			
Building Valuation Group		R-2	
Valuation per sq. ft. (VA construction type)		\$117.40	
Square Feet per Unit		990	
Units per Acre		24	
Acres per Permit		1	
Building Valuation per Unit		\$116,226	
Total Building Valuation per Permit		\$2,789,424	
City Building Permit Processing Fees			
Building Inspection/Building Permit Fee [1]		\$15,491	\$645
Plan Review	65% of bldg. insp. fee	\$10,069	\$420
Permit Processing Charge/Application	\$58 per permit	\$58	\$2
Energy Plan Review	5% of plan review	\$503	\$21
Disability Plan Review	5% of plan review	\$503	\$21
Seismic/Strong Motion	\$0.00028 of valuation	\$781	\$33
California Building Standards Commission Fee	\$1.00 per \$25,000 value	\$112	\$5
Electrical Permit Processing	\$58 per permit	\$58	\$2
Electrical Inspection	\$0.012 per sq. ft.	\$285	\$12
Plumbing Permit Processing	\$58 per permit	\$58	\$2
Plumbing Inspection	\$0.02 per sq. ft.	\$475	\$20
Mechanical Permit Processing	\$58 per permit	\$58	\$2
Mechanical Inspection	\$0.012 per sq. ft.	\$285	\$12
Solar Plan Review and Inspection	\$600 per permit	\$600	\$25
Energy Compliance Inspections	\$9 per unit	\$216	\$9
Fire Sprinklers Plan Check and Review [2]	\$1,017 per permit	\$1,017	\$42
Total Processing Fees		\$30,570	\$1,274
City Development Impact Fees (effective Jan. 1, 2023)			
General City			\$755
Fire			\$2,042
Library			\$471
Police			\$616
Wastewater			\$5,642
Parks & Recreation			\$6,959
Roads			\$4,063
Administration			\$154
Water (including 0.75% administration)			\$977
Affordable Housing - Proposed	\$2.00 per sq. ft.		\$1,980
Agricultural Mitigation [3]	\$15,000 per acre		\$625
Public Transit Fee (due at final map) [4]			\$218
Total Citywide Development Impact Fees			\$24,502
Other Agency Fees			
County Facilities and Services Authorization (effective 9/13/16)			\$2,312
School District Fees [5]	\$4.08 per sq. ft.		\$4,039
Yolo County HCP Fee (effective 3/27/23)	\$16,202 per acre		\$675
Total Other Agency Fees			\$7,026
RTIF - Proposed			\$33,663
Total Fees			\$66,465

Source: City of Woodland; Yolo County; WJUSD; EPS

[1] First \$1 million of valuation: \$6,730.50. Remaining valuation: \$4.38 for each \$1,000 or fraction thereof.

[2] \$242 + \$0.05 per bldg. sq. ft. for first 10,000 square feet + \$0.02 per bldg. sq. ft. for remaining square feet.

[3] Estimated amount. Different requirements for different development and mitigation land.

[4] 70% of single family fee.

[5] Fee for all areas of Woodland except Spring Lake. Spring Lake rate is \$6.13 per sq. ft.

**Table B-3
Woodland Research and Technology Park Financing Plan
Nonresidential Development Fees (2023\$)**

Item	Description/Formula	Fee per Permit	
		Village Center/ Com. Commercial	Business Park
Assumptions			
Building Valuation Group		M	B
Construction Type		IIIA	IIIA
Valuation per sq. ft. (ICC February 2023)		\$164.83	\$213.00
Acres		5.00	5.00
FAR		0.43	0.40
Square Feet		94,651	87,120
Total Project Valuation		\$15,601,351	\$18,556,554
Water Meters		2	2
Water Meter Size		2"	2"
City Building Permit Processing Fees (effective July 1, 2022)			
Building Inspection/Building Permit Fee	[1]	\$72,431	\$85,571
Plan Review	65% of bldg. insp. fee	\$47,080	\$55,621
Permit Processing Charge/Application	\$58 per permit	\$58	\$58
Energy Plan Review	5% of plan review	\$2,354	\$2,781
Disability Plan Review	5% of plan review	\$2,354	\$2,781
Seismic/Strong Motion	\$0.00028 of valuation	\$4,368	\$5,196
California Building Standards Commission	\$1.00 per \$25,000 value	\$625	\$743
Electrical Permit Processing	\$58 per permit	\$58	\$58
Electrical Inspection	\$0.012 per sq. ft.	\$1,136	\$1,045
Plumbing Permit Processing	\$58 per permit	\$59	\$58
Plumbing Inspection	\$0.02 per sq. ft.	\$1,893	\$1,742
Mechanical Permit Processing	\$58 per permit	\$59	\$58
Mechanical Inspection	\$0.012 per sq. ft.	\$1,136	\$1,045
Solar Plan Review and Inspection	\$600 per permit	\$600	\$600
Energy Compliance Inspections	\$108 per permit	\$108	\$108
Fire Sprinklers Plan Check and Review	[2] per sq. ft.	\$2,435	\$2,284
Total Processing Fees		\$136,753	\$159,750
City Development Impact Fees (effective Jan. 1, 2023)			
<i>Land Use</i>			
General City	per 1,000 sq. ft.		
Fire	per 1,000 sq. ft.		
Library	per 1,000 sq. ft.		
Police	per 1,000 sq. ft.		
Wastewater	per 1,000 sq. ft.		
Parks & Recreation	per 1,000 sq. ft.		
Roads	per 1,000 sq. ft.		
Administration	per 1,000 sq. ft.		
Water (including 0.75% administration)	\$19,757 per 2" meter	\$39,810	\$39,810
Agricultural Mitigation [3]	\$15,000 per acre		
Total Citywide Development Impact Fees			
Other Agency Fees			
County Facilities and Services Authorization (eff. 9/13/16)	per 1,000 sq. ft.		
School District Fees	\$0.66 per sq. ft.		
Yolo County HCP Fee (effective 3/27/23)	\$16,202 per acre		
Total Other Agency Fees			
RTIF - Proposed			
Total Fees			

Source: City of Woodland; Yolo County; WJUSD; EPS

[1] First \$1 million of valuation: \$6,730.50.

Remaining valuation: \$4.38 for each \$1,000 or fraction thereof.

[2] \$242 plus \$0.05 for the first 10,000 sq. ft. plus \$0.02 for each additional sq. ft. thereafter.

[3] Estimated amount. Different requirements for different development and mitigation land.

**Table B-3
Woodland Research and Technology Park Financing Plan
Nonresidential Development Fees (2023\$)**

Item	Description/Formula	Fee per Bldg. Sq. Ft.	
		Village Center/ Com. Commercial	Business Park
Assumptions			
Building Valuation Group		M	B
Construction Type		IIIA	IIIA
Valuation per sq. ft. (ICC February 2023)		\$164.83	\$213.00
Acres		5.00	5.00
FAR		0.43	0.40
Square Feet		94,651	87,120
Total Project Valuation		\$15,601,351	\$18,556,554
Water Meters		2	2
Water Meter Size		2"	2"
City Building Permit Processing Fees (effective July 1, 2022)			
Building Inspection/Building Permit Fee	[1]	\$0.77	\$0.98
Plan Review	65% of bldg. insp. fee	\$0.50	\$0.64
Permit Processing Charge/Application	\$58 per permit	\$0.00	\$0.00
Energy Plan Review	5% of plan review	\$0.02	\$0.03
Disability Plan Review	5% of plan review	\$0.02	\$0.03
Seismic/Strong Motion	\$0.00028 of valuation	\$0.05	\$0.06
California Building Standards Commission	\$1.00 per \$25,000 value	\$0.01	\$0.01
Electrical Permit Processing	\$58 per permit	\$0.00	\$0.00
Electrical Inspection	\$0.012 per sq. ft.	\$0.01	\$0.01
Plumbing Permit Processing	\$58 per permit	\$0.00	\$0.00
Plumbing Inspection	\$0.02 per sq. ft.	\$0.02	\$0.02
Mechanical Permit Processing	\$58 per permit	\$0.00	\$0.00
Mechanical Inspection	\$0.012 per sq. ft.	\$0.01	\$0.01
Solar Plan Review and Inspection	\$600 per permit	\$0.01	\$0.01
Energy Compliance Inspections	\$108 per permit	\$0.00	\$0.00
Fire Sprinklers Plan Check and Review	[2] per sq. ft.	\$0.03	\$0.03
Total Processing Fees		\$1.44	\$1.83
City Development Impact Fees (effective Jan. 1, 2023)			
<i>Land Use</i>		<i>Retail</i>	<i>Office</i>
General City	per 1,000 sq. ft.	\$0.67	\$0.79
Fire	per 1,000 sq. ft.	\$1.99	\$2.15
Library	per 1,000 sq. ft.	\$0.00	\$0.00
Police	per 1,000 sq. ft.	\$0.55	\$0.65
Wastewater	per 1,000 sq. ft.	\$3.07	\$1.74
Parks & Recreation	per 1,000 sq. ft.	\$0.00	\$0.00
Roads	per 1,000 sq. ft.	\$12.31	\$6.23
Administration	per 1,000 sq. ft.	\$0.14	\$0.09
Water (including 0.75% administration)	\$19,757 per 2" meter	\$0.42	\$0.46
Agricultural Mitigation [3]	\$15,000 per acre	\$0.79	\$0.86
Total Citywide Development Impact Fees		\$19.94	\$12.97
Other Agency Fees			
County Facilities and Services Authorization (eff. 9/13/16)	per 1,000 sq. ft.	\$0.50	\$0.67
School District Fees	\$0.66 per sq. ft.	\$0.66	\$0.66
Yolo County HCP Fee (effective 3/27/23)	\$16,202 per acre	\$0.86	\$0.93
Total Other Agency Fees		\$2.02	\$2.26
RTIF - Proposed		\$11.89	\$6.73
Total Fees		\$35.30	\$23.78

Source: City of Woodland; Yolo County; WJUSD; EPS

[1] First \$1 million of valuation: \$6,730.50.

Remaining valuation: \$4.38 for each \$1,000 or fraction thereof.

[2] \$242 plus \$0.05 for the first 10,000 sq. ft. plus \$0.02 for each additional sq. ft. thereafter.

[3] Estimated amount. Different requirements for different development and mitigation land.

Table B-4
Woodland Research and Technology Park Financing Plan
Estimated W RTP Fee Revenue for Backbone Infrastructure and Public Facilities (2023\$)

Item	Residential			Commercial		Total
	Low Density	Medium Density	High Density	Village Center/Com. Commercial	Business Park	
Dwelling Units/Building Square Feet	628	524	518	310,948	1,980,115	
Building Permit Processing Fees	\$2,424,854	\$1,890,754	\$659,800	\$449,262	\$3,630,892	\$9,055,563
City Development Impact Fees						
General City	\$631,140	\$526,620	\$391,090	\$208,646	\$1,572,211	\$3,329,707
Fire	\$2,138,968	\$1,784,744	\$1,057,756	\$617,543	\$4,257,247	\$9,856,258
Library	\$394,384	\$329,072	\$243,978	\$0	\$0	\$967,434
Police	\$516,844	\$431,252	\$319,088	\$170,400	\$1,283,115	\$2,720,698
Wastewater	\$4,725,072	\$3,942,576	\$2,922,556	\$954,921	\$3,453,321	\$15,998,446
Parks & Recreation	\$5,827,212	\$4,862,196	\$3,604,762	\$0	\$0	\$14,294,170
Roads	\$6,226,620	\$5,195,460	\$2,104,634	\$3,828,392	\$12,326,216	\$29,681,322
Water	\$3,877,272	\$3,235,176	\$506,086	\$130,785	\$904,834	\$8,654,153
Administration	\$182,748	\$152,484	\$79,772	\$43,222	\$172,270	\$630,496
Affordable Housing - Proposed	\$4,333,200	\$2,423,500	\$1,025,640	\$0	\$0	\$7,782,340
Agricultural Mitigation	\$1,570,000	\$655,000	\$323,750	\$246,390	\$1,704,645	\$4,499,785
Public Transit	\$195,308	\$162,964	\$112,924	\$0	\$0	\$471,196
Total Citywide Development Impact Fees	\$30,618,768	\$23,701,044	\$12,692,036	\$6,200,298	\$25,673,858	\$98,886,004
Other Agency Fees						
County Facilities and Services Authorization	\$1,958,104	\$1,633,832	\$1,197,616	\$155,474	\$1,318,757	\$6,263,783
School District Fees	\$5,893,152	\$3,955,152	\$2,092,306	\$205,226	\$1,306,876	\$13,452,711
Yolo County HCP Fee	\$1,695,809	\$707,487	\$349,693	\$266,134	\$1,841,244	\$4,860,368
Total Other Agency Fees	\$9,547,065	\$6,296,471	\$3,639,615	\$626,834	\$4,466,876	\$24,576,862
RTIF - Proposed						
Streets	\$17,619,713	\$11,811,748	\$6,348,539	\$3,105,698	\$9,816,301	\$48,702,000
Storm Drainage	\$4,366,048	\$2,484,704	\$1,598,075	\$364,864	\$2,524,309	\$11,338,000
Water	\$1,825,876	\$1,296,889	\$911,283	\$42,385	\$266,566	\$4,343,000
Sewer	\$738,832	\$476,823	\$424,029	\$42,641	\$196,674	\$1,879,000
Parks and Open Space	\$13,055,403	\$8,404,261	\$7,484,867	\$0	\$0	\$28,944,531
Total RTIF	\$37,605,873	\$24,474,425	\$16,766,793	\$3,555,589	\$12,803,851	\$95,206,531
Total Fee Revenue	\$80,196,561	\$56,362,695	\$33,758,244	\$10,831,983	\$46,575,477	\$227,724,959

Source: City of Woodland; Yolo County; WJUSD; EPS



APPENDIX C:

Infrastructure Cost Burden Comparison

Table C-1	Fee Comparison—Low-Density Single-Family Development.....	C-1
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Table C-3	Fee Comparison—High Density Dwelling Unit	C-3
Table C-4	Fee Comparison—Commercial Development	C-4
Table C-5	Spring Lake Single Family Development Fees	C-5
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Table C-9	Northlake Residential Development Impact Fees	C-10
Table C-10	Northlake Commercial Fees.....	C-11

Table C-1
Woodland Research and Technology Park Financing Plan
Fee Comparison - Low-Density Single-Family Development (2023\$)

Item	Fee per Low-Density Single-Family Dwelling Unit			
	Woodland		Sacramento	
	WRTP	SLSP	Panhandle (Traditional Unit)	Northlake [1]
<i>Source</i>	<i>Table 7-1</i>	<i>Table C-5</i>	<i>Table C-8</i>	<i>Table C-9</i>
Building Permit Processing Fees	\$3,861	\$3,861	\$8,185	\$8,308
City Fees	\$48,756	\$54,396	\$23,939	\$23,939
Other Agency Fees	\$15,202	\$19,917	\$32,183	\$39,433
Plan Area Fees	\$65,854	\$56,510	\$23,605	\$5,333
Total Fees	\$133,673	\$134,685	\$87,911	\$77,012

Source: City of Woodland; City of Sacramento; other public agencies; EPS

[1] The Northlake plan area fees include only certain public facility obligations not covered through existing City fees. Because the project was developed by a single master developer, costs for project-related backbone infrastructure were not included.

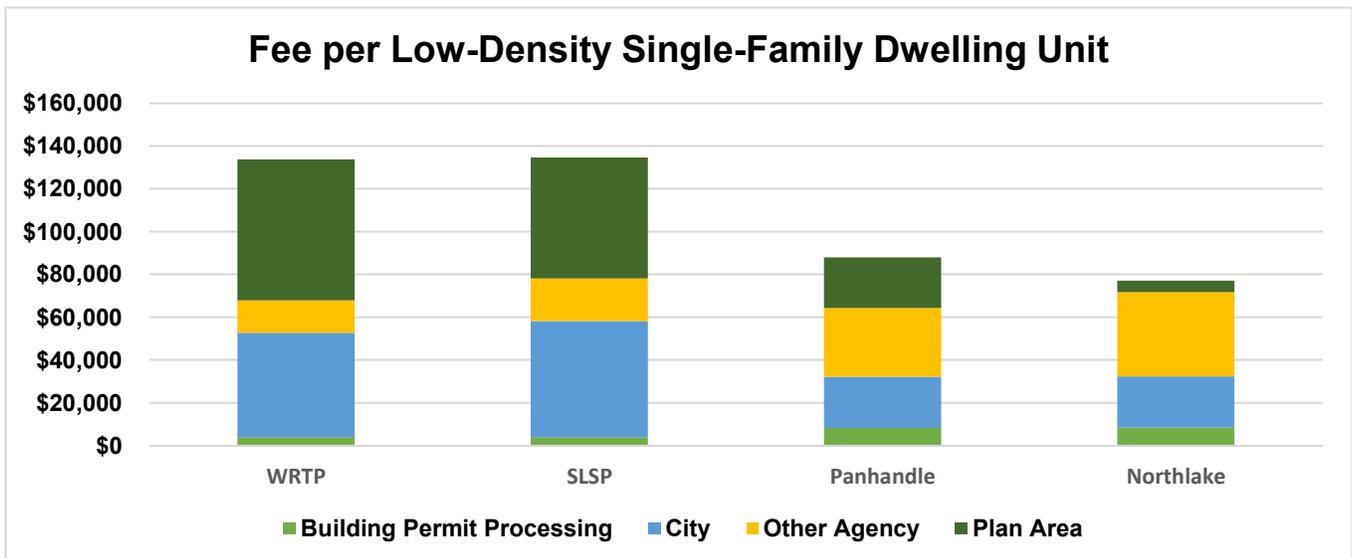
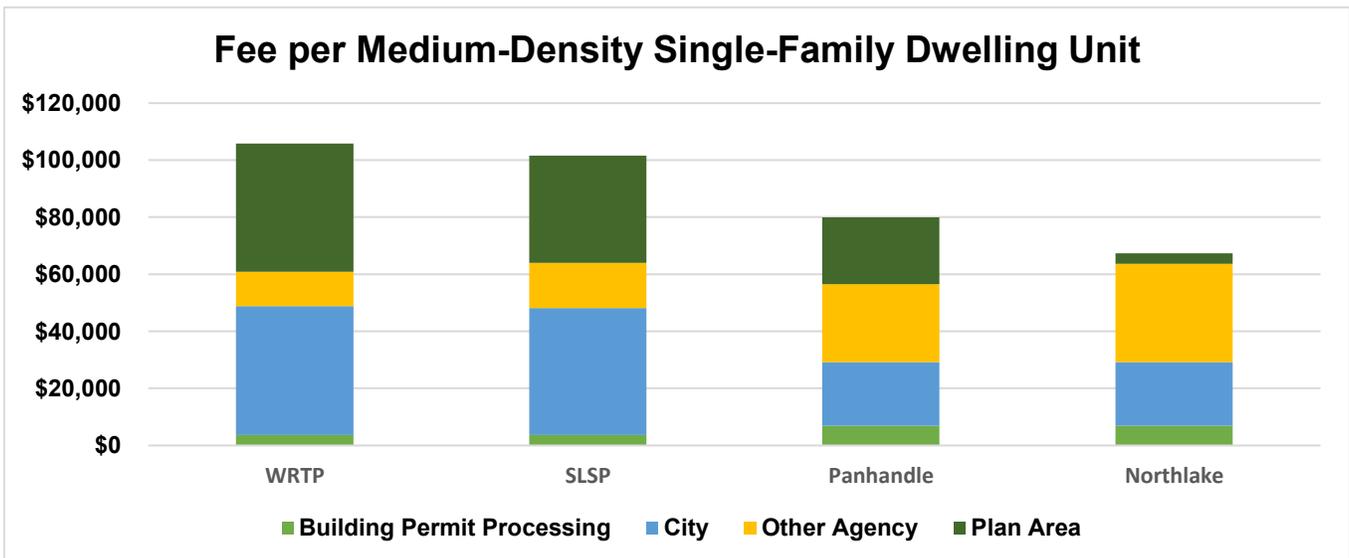


Table C-2
Woodland Research and Technology Park Financing Plan
Fee Comparison - Medium Density Single Family Development (2023\$)

Item	Fee per Medium-Density Single-Family Dwelling Unit			
	Woodland		Sacramento	
	WRTP	SLSP	Panhandle (Village Unit)	Northlake
Source	Table 7-1	Table C-5	Table C-8	Table C-9
Building Permit Processing Fees	\$3,608	\$3,608	\$6,864	\$6,864
City Fees	\$45,231	\$44,539	\$22,337	\$22,337
Other Agency Fees	\$12,016	\$15,809	\$27,331	\$34,432
Plan Area Fees	\$45,004	\$37,637	\$23,446	\$3,742
Total Fees	\$105,859	\$101,593	\$79,978	\$67,375

Source: City of Woodland; City of Sacramento; other public agencies; EPS

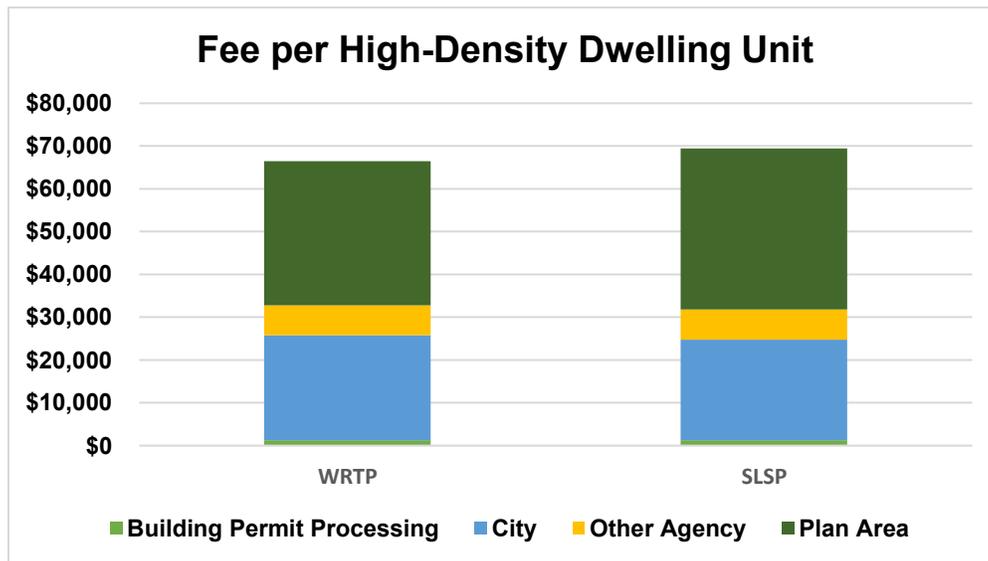
[1] The Northlake plan area fees include only certain public facility obligations not covered through existing City fees. Because the project was developed by a single master developer, costs for project-related backbone infrastructure were not included.



**Table C-3
Woodland Research and Technology Park Financing Plan
Fee Comparison - High Density Dwelling Unit (2023\$)**

Item	Fee per High-Density Dwelling Unit	
	Woodland	
	WRTP	SLSP
<i>Source</i>	<i>Table 7-1</i>	<i>Table C-6</i>
Building Permit Processing Fees	\$1,274	\$1,274
City Fees	\$24,502	\$23,473
Other Agency Fees	\$7,026	\$7,026
Plan Area Fees	\$33,663	\$37,637
Total Fees	\$66,465	\$69,410

Source: City of Woodland; City of Sacramento; other public agencies; EPS



**Table C-4
Woodland Research and Technology Park Financing Plan
Fee Comparison - Commercial Development (2023\$)**

Item	Fee per Commercial Bldg. Sq. Ft.		
	Woodland		Sacramento
	WRTP	Spring Lake Specific Plan	Northlake
<i>Source</i>	<i>Table 7-1</i>	<i>Table C-7</i>	<i>Table C-10</i>
Building Permit Processing Fees	\$1.44	\$1.44	\$3.79
City Fees	\$19.94	\$19.94	\$4.47
Other Agency Fees	\$2.02	\$2.02	\$8.66
Plan Area Fees	\$11.89	\$43.45	\$4.58
Total Fees	\$35.30	\$66.85	\$21.50

Source: City of Woodland; City of Sacramento; other public agencies; EPS

[1] The Northlake plan area fees include only certain public facility obligations not covered through existing City fees. Because the project was developed by a single master developer, costs for project-related backbone infrastructure were not included.

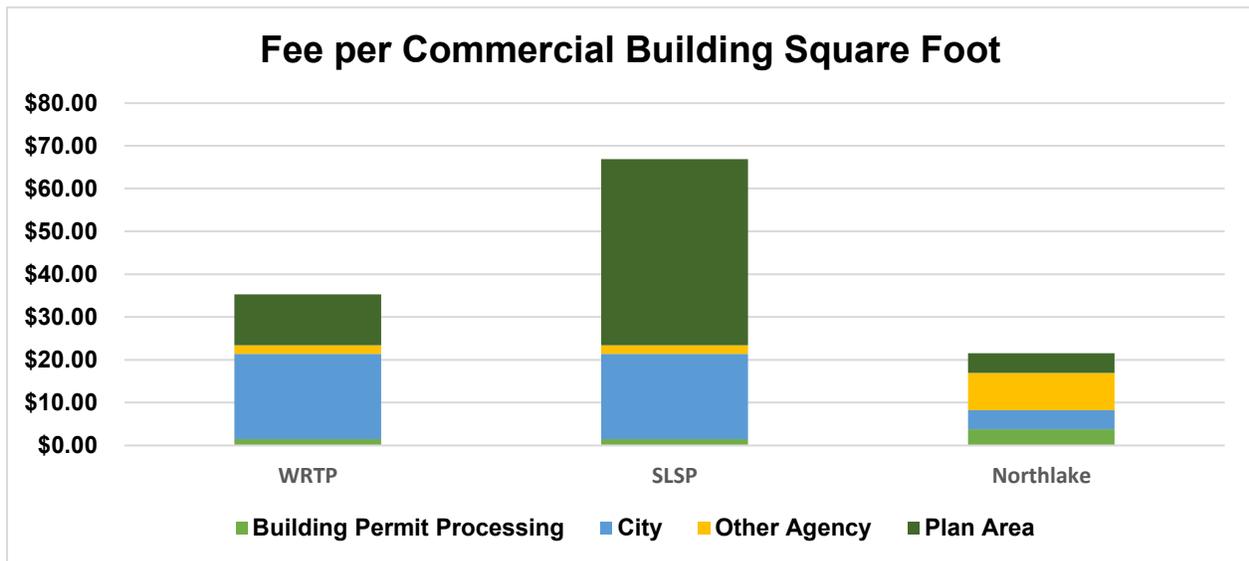


Table C-5
Woodland Research and Technology Park Financing Plan
Spring Lake Single Family Development Fees (2023\$)

Item	Fee per Unit		Description/Formula
	Low Density	Medium Density	
Assumptions			
Building Valuation Group	R-3	R-3	
Construction Type	VB	VB	
Valuation per sq. ft. (VB construction type Feb. 2023)	\$167.37	\$167.37	
Garage Valuation per sq. ft. (VB construction type Feb. 2023)	\$66.48	\$66.48	
Square Feet	2,300	1,850	
Garage Square Feet	450	400	
Units per Acre	6	12	
Building Valuation per Unit	\$384,951	\$309,635	
Garage Valuation per Unit	\$29,916	\$26,592	
Total Unit Valuation	\$414,867	\$336,227	
City Building Permit Processing Fees (effective July 1, 2022)			
	<i>Fee per Unit</i>		
Building Inspection and Plan Review Fee [1]	\$1,641	\$1,402	
Green Building Plan Review	\$276	\$276	
Permit Processing Charge/Application	\$8	\$8	
Seismic/Strong Motion	\$54	\$44	\$0.00013 of total valuation
California Building Standards Commission Fee	\$17	\$14	\$1.00 per \$25,000 value
Electrical Permit Processing	\$58	\$58	
Electrical Inspection	\$200	\$200	10% of building fee; \$200 min.
Plumbing Permit Processing	\$58	\$58	
Plumbing Inspection	\$400	\$400	20% of building fee; \$400 min.
Mechanical Permit Processing	\$58	\$58	
Mechanical Inspection	\$200	\$200	10% of building fee; \$200 min.
Solar Plan Review and Inspection	\$280	\$280	
Energy Compliance Inspections	\$55	\$55	
Fire Sprinklers Plan Check and Review	\$556	\$556	
Total Processing Fees	\$3,861	\$3,608	
City Development Impact Fees (effective Jan. 1, 2023)			
General City	\$1,005	\$1,005	
Fire	\$3,406	\$3,406	
Library	\$628	\$628	
Police	\$823	\$823	
Wastewater	\$7,524	\$7,524	
Parks & Recreation	\$5,105	\$5,105	
Roads	\$9,915	\$9,915	
Water	\$6,174	\$6,174	
Administration	\$259	\$259	
Affordable Housing [2] [3]	\$8,629	\$3,200	
Agricultural Mitigation [4]	\$2,500	\$1,250	\$15,000 per acre
Fiscal Deficit Fee	\$1,500	\$1,050	
Fire Operations & Maintenance Fund	\$771	\$540	
General Purpose Fee [2] [3]	\$5,790	\$3,402	
Habitat Education (due at final map)	\$56	\$40	
Public Transit Fee (due at final map)	\$311	\$218	
Total City Development Impact Fees	\$54,396	\$44,539	
Other Agency Fees			
County Facilities and Services Authorization (effective 9/13/16)	\$3,118	\$3,118	
School District Fees [5]	\$14,099	\$11,341	\$6.13 per sq. ft.
Yolo County HCP Fee (effective 3/27/23)	\$2,700	\$1,350	\$16,202 per acre
Total Other Agency Fees	\$19,917	\$15,809	
SLIF			
Roadway	\$24,503	\$16,173	multifamily rate for medium density
Water	\$2,777	\$1,794	multifamily rate for medium density
Sewer	\$3,579	\$2,739	multifamily rate for medium density
Drainage	\$12,702	\$8,383	multifamily rate for medium density
Fire Station	\$1,021	\$674	multifamily rate for medium density
Parks	\$10,084	\$6,656	multifamily rate for medium density
On-going Admin Costs	\$1,844	\$1,218	multifamily rate for medium density
Subtotal SLIF	\$56,510	\$37,637	
Total Fees per Unit	\$134,685	\$101,593	

Source: City of Woodland; Yolo County; WJUSD; EPS

- [1] First 1,000 sq. ft.: \$946
1,000-2,000 sq. ft.: \$53.60 for each 100 sq. ft. or fraction thereof
2,000-3,000 sq. ft.: \$53.10 for each 100 sq. ft. or fraction thereof
- [2] LDR fees are based on Pioneer Village development project.
- [3] MDR fees are based on Beeghly multifamily development project. Affordable housing assuming a 10 percent requirement.
- [4] Estimated amount. Different requirements for different development and mitigation land.
- [5] School fee set equal to Spring Lake rate. Rate for all other areas of Woodland is \$4.08 per sq. ft.

Table C-6
Woodland Research and Technology Park Financing Plan
Spring Lake Multifamily Development Fees (2023\$)

Item	Multifamily		Description/Formula
	Per Permit	Per Unit	
Assumptions			
Building Valuation Group		R-2	
Valuation per sq. ft. (VA construction type)	\$117.40		
Square Feet per Unit	990		
Units per Acre	24		
Acres per Permit	1		
Building Valuation per Unit	\$116,226		
Total Building Valuation per Permit	\$2,789,424		
City Building Permit Processing Fees			
Building Inspection/Building Permit Fee [1]	\$15,491	\$645	
Plan Review	\$10,069	\$420	65% of bldg. insp. fee
Permit Processing Charge/Application	\$58	\$2	\$58 per permit
Energy Plan Review	\$503	\$21	5% of plan review
Disability Plan Review	\$503	\$21	5% of plan review
Seismic/Strong Motion	\$781	\$33	\$0.00028 of valuation
California Building Standards Commission Fee	\$112	\$5	\$1.00 per \$25,000 value
Electrical Permit Processing	\$58	\$2	\$58 per permit
Electrical Inspection	\$285	\$12	\$0.012 per sq. ft.
Plumbing Permit Processing	\$58	\$2	\$58 per permit
Plumbing Inspection	\$475	\$20	\$0.02 per sq. ft.
Mechanical Permit Processing	\$58	\$2	\$58 per permit
Mechanical Inspection	\$285	\$12	\$0.012 per sq. ft.
Solar Plan Review and Inspection	\$600	\$25	\$600 per permit
Energy Compliance Inspections	\$216	\$9	\$9 per unit
Fire Sprinklers Plan Check and Review [2]	\$1,017	\$42	\$1,017 per permit
Total Processing Fees	\$30,570	\$1,274	
City Development Impact Fees (effective Jan. 1, 2023)			
General City		\$755	
Fire		\$2,042	
Library		\$471	
Police		\$616	
Wastewater		\$5,642	
Parks & Recreation		\$3,828	
Roads		\$4,063	
Administration		\$131	
Water (including 0.75% administration) [3]		\$977	
Affordable Housing		\$2,475	
Agricultural Mitigation [4]	\$625		\$15,000 per acre
Fiscal Defecit Fee	\$1,050		
Fire Operations & Maintenance Fund		\$540	
Habitat Education		\$40	
Public Transit Fee (due at final map) [5]		\$218	
Total Citywide Development Impact Fees		\$23,473	
Other Agency Fees			
County Facilities and Services Authorization (effective 9/13/16)	\$2,312		
School District Fees	\$4,039		\$4.08 per sq. ft.
Yolo County HCP Fee (effective 3/27/23)	\$675		\$16,202 per acre
Total Other Agency Fees	\$7,026		
SLIF			
Roadway		\$16,173	
Water		\$1,794	
Sewer		\$2,739	
Drainage		\$8,383	
Fire Station		\$674	
Parks		\$6,656	
On-going Admin Costs		\$1,218	
Subtotal SLIF		\$37,637	
Total Fees per Unit		\$69,410	

Source: City of Woodland; Yolo County; WJUSD; EPS

- [1] First \$1 million of valuation: \$6,730.50. Remaining valuation: \$4.38 for each \$1,000 or fraction thereof.
[2] \$242 + \$0.05 per bldg. sq. ft. for first 10,000 square feet + \$0.02 per bldg. sq. ft. for remaining square feet.
[3] City water is based on Vista Del Robles with 2 4" and 2-2" water mains for 168 units.
[4] Estimated amount. Different requirements for different development and mitigation land.
[5] 70% of single family fee.

Table C-7
Woodland Research and Technology Park Financing Plan
Spring Lake Commercial Fees (2023\$)

Item	Commercial		Description/Formula
	Fee per Permit	Fee per Bldg. Sq. Ft.	
Assumptions			
Building Valuation Group		M	
Construction Type		IIIA	
Valuation per sq. ft. (ICC February 2023)	\$164.83		
Acres	5.00		
FAR	0.43		
Square Feet	94,651		
Total Project Valuation	\$15,601,351		
Water Meters	2		
Water Meter Size	2"		
City Building Permit Processing Fees (effective July 1, 2022)			
Building Inspection/Building Permit Fee [1]	\$72,431	\$0.77	
Plan Review	\$47,080	\$0.50	65% of bldg. insp. fee
Permit Processing Charge/Application	\$58	\$0.00	\$58 per permit
Energy Plan Review	\$2,354	\$0.02	5% of plan review
Disability Plan Review	\$2,354	\$0.02	5% of plan review
Seismic/Strong Motion	\$4,368	\$0.05	\$0.00028 of valuation
California Building Standards Commission	\$625	\$0.01	\$1.00 per \$25,000 value
Electrical Permit Processing	\$58	\$0.00	\$58 per permit
Electrical Inspection	\$1,136	\$0.01	\$0.012 per sq. ft.
Plumbing Permit Processing	\$60	\$0.00	\$58 per permit
Plumbing Inspection	\$1,893	\$0.02	\$0.02 per sq. ft.
Mechanical Permit Processing	\$60	\$0.00	\$58 per permit
Mechanical Inspection	\$1,136	\$0.01	\$0.012 per sq. ft.
Solar Plan Review and Inspection	\$600	\$0.01	\$600 per permit
Energy Compliance Inspections	\$108	\$0.00	\$108 per permit
Fire Sprinklers Plan Check and Review [2]	\$2,435	\$0.03	per sq. ft.
Total Processing Fees	\$136,755	\$1.44	
City Development Impact Fees (effective Jan. 1, 2023)			
<i>Land Use</i>		<i>Retail</i>	
General City		\$0.67	\$671 per 1,000 sq. ft.
Fire		\$1.99	\$1,986 per 1,000 sq. ft.
Library		\$0.00	\$0 per 1,000 sq. ft.
Police		\$0.55	\$548 per 1,000 sq. ft.
Wastewater		\$3.07	\$3,071 per 1,000 sq. ft.
Parks & Recreation		\$0.00	\$0 per 1,000 sq. ft.
Roads		\$12.31	\$12,312 per 1,000 sq. ft.
Administration		\$0.14	\$139 per 1,000 sq. ft.
Water (including 0.75% administration)	\$39,810	\$0.42	\$19,757 per 2" meter
Agricultural Mitigation [3]		\$0.79	\$15,000 per acre
Total Citywide Development Impact Fees		\$19.94	
Other Agency Fees			
County Facilities and Services Authorization (eff. 9/13/16)		\$0.50	\$500 per 1,000 sq. ft.
School District Fees		\$0.66	\$0.66 per sq. ft.
Yolo County HCP Fee (effective 3/27/23)		\$0.86	\$16,202 per acre
Total Other Agency Fees		\$2.02	
SLIF			
Roadway		\$18.86	
Water		\$2.10	
Sewer		\$2.70	
Drainage		\$9.79	
Fire Station		\$0.78	
Park		\$7.77	
On-going Admin Costs		\$1.45	
Subtotal SLIF		\$43.45	
Total		\$66.85	

Source: City of Woodland; Yolo County; WJUSD; EPS

[1] First \$1 million of valuation: \$6,730.50.

Remaining valuation: \$4.38 for each \$1,000 or fraction thereof.

[2] \$242 plus \$0.05 for the first 10,000 sq. ft. plus \$0.02 for each additional sq. ft. thereafter.

[3] Estimated amount. Different requirements for different development and mitigation land.

**Table C-8
Woodland Research and Technology Park Financing Plan
Panhandle Residential Development Impact Fees (2023\$)**

Item	Residential			Notes
	Estates	Traditional	Village	
Assumptions				
Net Acres	75.7	147.7	60.5	
Number of Units	340	869	453	
Unit Size/Bldg. Sq. Ft.	2,500	2,300	1,850	
Garage Square Feet	500	500	450	
Units per Acre	4.5	5.9	7.5	
Valuation per Bldg. Sq. Ft.- Living Area (VB)	\$167.37	\$167.37	\$167.37	
Valuation per Bldg. Sq. Ft. - Garage (U)	\$66.48	\$66.48	\$66.48	
Building Valuation - Living Area and Garage	\$451,665	\$418,191	\$339,551	
2002 Building plus Equipment Valuations				
Valuation per Bldg. Sq. Ft.- Living Area	\$92.40	\$92.40	\$92.40	
Valuation per Bldg. Sq. Ft. - Garage	\$24.30	\$24.30	\$24.30	
Air Conditioning - Living Area	\$3.50	\$3.50	\$3.50	
Sprinklers - Living Area and Garage	\$2.60	\$2.60	\$2.60	
Building and Equipment Valuation	\$259,700	\$224,670	\$181,875	Used for calculation of Construction Excise Tax
<hr/>				
<i>Current as of</i>	<i>Apr-23</i>	<i>Apr-23</i>	<i>Apr-23</i>	
Processing Fees				
	<i>per unit</i>	<i>per unit</i>	<i>per unit</i>	
Administrative Processing Fee	\$164	\$164	\$164	\$164 per hour, assumes 1 hour review
Building Permit	\$3,465	\$3,238	\$2,704	\$1,078 + \$0.006787 for each dollar over \$100,000 of bldg. valuation
Technology Surcharge	\$277	\$259	\$216	8% of Building Permit
Plan Review Fee	\$728	\$680	\$568	42% of Building Permit Fee; 50% of amount for Master Plan projects.
Technology Surcharge	\$58	\$54	\$45	8% of Plan Review Fee
Planning Review Fee	\$109	\$102	\$85	15% of Plan Review Fee
Planning Inspection Fee	\$336	\$336	\$336	Flat rate; charged when Planning Division performs inspections.
Seismic/Strong Motion	\$59	\$54	\$44	\$0.00013 per \$1 of bldg. valuation
General Plan Recovery Fee	\$1,174	\$1,087	\$883	\$2.60 per \$1,000 of bldg. valuation
Green Building/CBSC Fee	\$19	\$17	\$14	\$1 per \$25,000 of bldg. valuation or fraction, thereof
Construction Excise Tax	\$2,078	\$1,797	\$1,455	0.008 * valuation of 2002 bldg. + equipment valuation
Fire Inspection Fee	\$279	\$260	\$214	\$0.093 x gross square feet
Fire Review Fee	\$136	\$136	\$136	\$136 per hour, assumes 1 hour review
Subtotal Processing Fees	\$8,882	\$8,185	\$6,864	
City Development Impact Fees				
Adjusted Transportation Development Impact Fee (TDIF)	\$2,188	\$2,188	\$2,188	Net of alternative modes credits
Water Development Fee	\$3,609	\$3,609	\$3,609	Assumes 1" domestic meter
Water Easement Tap Installation Fee	\$1,822	\$1,822	\$1,822	Assumes 1" domestic meter
Water Meter Installation	\$672	\$672	\$672	Assumes 1" domestic meter
Residential Construction Water Use Fee	\$201	\$201	\$201	Flat rate per residential lot
City Business Operations Tax	\$181	\$167	\$136	\$0.0004 per \$1 of bldg. valuation; Max. \$5,000/year/contractor
Erosion and Sediment Control (ESC)	\$70	\$70	\$70	Flat rate per residential lot

**Table C-8
Woodland Research and Technology Park Financing Plan
Panhandle Residential Development Impact Fees (2023\$)**

Item	Residential			Notes
	Estates	Traditional	Village	
Neighborhood and Community Parks	\$4,504	\$4,504	\$4,504	\$4,504 per unit for units 2,000 sq. ft. or larger
Citywide Parks/Facilities	\$2,293	\$2,293	\$2,293	\$2,293 per unit for units 2,000 sq. ft. or larger
Mixed Income Housing Ordinance/Housing Trust Fund	\$8,725	\$8,027	\$6,457	\$3.49 per unit sq. ft.
Residential Construction Tax	\$385	\$385	\$385	Flat rate for dwelling units with 3 or more bedrooms
Subtotal City Development Impact Fees	\$24,650	\$23,939	\$22,337	
Other Agency Fees				
Twin Rivers and Robla Elementary School District Fees	\$11,150	\$10,258	\$8,251	\$4.46 per living area sq. ft. (effective 3/29/2023)
Natomas Basin Habitat Conservation Plan Fee	\$6,638	\$5,068	\$3,982	\$29,815 per acre; assumes land dedication in-lieu (\$15,750/acre), so excludes land acquisition portion of total fee (\$45,565 per acre).
Sacramento Area Flood Control Agency Dev. Impact Fee	\$5,250	\$4,830	\$3,885	\$2.10 per habitable area sq. ft.(effective 1/4/2020).
Sacramento Countywide Transportation Mitigation Fee	\$1,458	\$1,458	\$1,458	\$1,429 mitigation fee + 2% admin fee (7/19/2022)
Air Quality Mitigation Fee	\$290	\$290	\$290	Estimated \$220.18 per unit, plus 15% admin fee (\$37.86)
Sacramento Area Sewer District (Expansion)	\$4,978	\$3,800	\$2,986	\$22,360 per net acre for Expansion Area
Regional SAN (New)	\$6,479	\$6,479	\$6,479	\$6,479 per ESD for New Growth Area
Subtotal Other Agency Fees	\$36,243	\$32,183	\$27,331	
Panhandle Plan Area Fees				
Panhandle DA Fee Component				
Regional Park Land Acquisition	\$2,388	\$2,388	\$2,388	
Transit	\$587	\$587	\$587	
Fire Facilities	\$596	\$596	\$596	
Community Center	\$368	\$368	\$368	
Library	\$936	\$936	\$936	
Subtotal Panhandle DA Fee Component	\$4,875	\$4,875	\$4,875	
Panhandle Impact Fee Component				
Roadways	\$8,736	\$8,736	\$8,736	
Sanitary Sewer	\$234	\$178	\$175	
Storm Drainage (Including Land Acquisition)	\$11,131	\$8,497	\$8,346	
Trails	\$631	\$631	\$631	
Subtotal Panhandle Impact Fee Component	\$20,732	\$18,042	\$17,888	
Panhandle Administration Fee (3%)	\$768	\$688	\$683	
Subtotal Panhandle Plan Area Fees	\$26,375	\$23,605	\$23,446	
Total Fees	\$96,150	\$87,911	\$79,978	

Source: City of Sacramento; various public agencies; EPS.

[1] From the FEIR and escalated by percentage change in SF CPI from April 2018 through April 2022 (14.63%) See Table A-1.
Fee in the FEIR was \$220.18 per unit plus 15% administration.

Table C-9
Woodland Research and Technology Park Financing Plan
Northlake Residential Development Impact Fees (2023\$)

Item	Residential		Notes
	Low Density	Medium Density	
Assumptions			
Net Acres	147.7	60.5	
Number of Units	869	453	
Unit Size/Bldg. Sq. Ft.	2,300	1,850	
Garage Square Feet	500	450	
Units per Acre	5.9	7.5	
Valuation per Bldg. Sq. Ft.- Living Area (VB)	\$167.37	\$167.37	
Valuation per Bldg. Sq. Ft. - Garage (U)	\$66.48	\$66.48	
Building Valuation - Living Area and Garage	\$418,191	\$339,551	
2002 Building plus Equipment Valuations			
Valuation per Bldg. Sq. Ft.- Living Area	\$92.40	\$92.40	
Valuation per Bldg. Sq. Ft. - Garage	\$24.30	\$24.30	
Air Conditioning - Living Area	\$3.50	\$3.50	
Sprinklers - Living Area and Garage	\$2.60	\$2.60	
Building and Equipment Valuation	\$240,000	\$181,875	Used for calculation of Construction Excise Tax
Processing Fees			
	<i>per unit</i>	<i>per unit</i>	
Administrative Processing Fee	\$164	\$164	\$164 per hour, assumes 1 hour review
Building Permit	\$3,238	\$2,704	\$1,078 + \$0.006787 for each dollar over \$100,000 of bldg. valuation
Technology Surcharge	\$259	\$216	8% of Building Permit
Plan Review Fee	\$680	\$568	42% of Building Permit Fee; 50% of amount for Master Plan projects.
Technology Surcharge	\$54	\$45	8% of Plan Review Fee
Planning Review Fee	\$102	\$85	15% of Plan Review Fee
Planning Inspection Fee	\$336	\$336	Flat rate; charged when Planning Division performs inspections.
Seismic/Strong Motion	\$54	\$44	\$0.00013 per \$1 of bldg. valuation
General Plan Recovery Fee	\$1,087	\$883	\$2.60 per \$1,000 of bldg. valuation
Green Building/CBSC Fee	\$17	\$14	\$1 per \$25,000 of bldg. valuation or fraction, thereof
Construction Excise Tax	\$1,920	\$1,455	0.008 * valuation of 2002 bldg. + equipment valuation
Fire Inspection Fee	\$260	\$214	\$0.093 x gross square feet
Fire Review Fee	\$136	\$136	\$136 per hour, assumes 1 hour review
Subtotal Processing Fees	\$8,308	\$6,864	
City Development Impact Fees			
Adjusted Transportation Development Impact Fee (TDIF)	\$2,188	\$2,188	Net of alternative modes credits
Water Development Fee	\$3,609	\$3,609	Assumes 1" domestic meter
Water Easement Tap Installation Fee	\$1,822	\$1,822	Assumes 1" domestic meter
Water Meter Installation	\$672	\$672	Assumes 1" domestic meter
Residential Construction Water Use Fee	\$201	\$201	Flat rate per residential lot
City Business Operations Tax	\$167	\$136	\$0.0004 per \$1 of bldg. valuation; Max. \$5,000/year/contractor
Erosion and Sediment Control (ESC)	\$70	\$70	Flat rate per residential lot
Neighborhood and Community Parks	\$4,504	\$4,504	\$4,504 per unit for units 2,000 sq. ft. or larger
Citywide Parks/Facilities	\$2,293	\$2,293	\$2,293 per unit for units 2,000 sq. ft. or larger
Mixed Income Housing Ordinance/Housing Trust Fund	\$8,027	\$6,457	\$3.49 per unit sq. ft.
Residential Construction Tax	\$385	\$385	Flat rate for dwelling units with 3 or more bedrooms
Subtotal City Development Impact Fees	\$23,939	\$22,337	
Other Agency Fees			
Twin Rivers USD	\$11,017	\$8,862	\$4.79 per living area sq. ft. (effective 3/29/2023)
Rio Linda Supplemental School Fee [1]	\$6,781	\$6,781	\$5,180 escalated to 2023\$
Natomas Basin Habitat Conservation Plan Fee	\$5,068	\$3,982	\$29,815 per acre; assumes land dedication in-lieu (\$15,750/acre), so excludes land acquisition portion of total fee (\$45,565 per acre).
Sacramento Area Flood Control Agency Dev. Impact Fee	\$4,830	\$3,885	\$2.10 per habitable area sq. ft.(effective 1/4/2020).
Sacramento Countywide Transportation Mitigation Fee	\$1,458	\$1,458	\$1,429 mitigation fee + 2% admin fee
Sacramento Area Sewer District (Expansion)	\$3,800	\$2,986	\$22,360 per net acre for Expansion Area
Regional SAN	\$6,479	\$6,479	\$6,479 per ESD for New Growth Area
Subtotal Other Agency Fees	\$39,433	\$34,432	
North Lake (Greenbriar) Impact Fee [2]			
Transit Fee	\$985	\$820	
Regional Park Fee	\$2,766	\$1,760	
Fire Facilities Fee	\$691	\$496	
Community Center Fee	\$891	\$666	
Subtotal Greenbriar Impact Fee	\$5,333	\$3,742	
Total Fees	\$77,012	\$67,375	

Source: City of Sacramento; various public agencies; EPS.

[1] EPS obtained the project-specific supplemental school fee from the Greenbriar Specific Plan Draft Proposed Public Facilities Financing Plan (PFFP) prepared April 26, 2017 and escalated these fees using the average of the 20-City and San Francisco Construction Cost Indices from March 2016 to March 2023 as published by Engineering-News Record.

[2] The Northlake plan area fees include only certain public facility obligations not covered through existing City fees. Because the project was developed by a single master developer, costs for project-related backbone infrastructure were not included.

Table C-10
Woodland Research and Technology Park Financing Plan
Northlake Commercial Fees (2023\$)

Item	Commercial		Notes
	Fee per Permit	Fee per Bldg. Sq. Ft	
Assumptions			
Building Valuation Group		M	
Construction Type		IIIA	
Valuation per sq. ft. (ICC February 2023)	\$164.83		
Acres	5.00		
FAR	0.43		
Square Feet	94,651		
Total Project Valuation	\$15,601,351		
Water Meters	2		
Water Meter Size	2"		
<hr/>			
<i>Current as of</i>	<i>Apr-23</i>	<i>Apr-23</i>	
Processing Fees			
Administrative Processing Fee	\$164.00	\$0.00	\$164 per hour, assumes 1 hour review
Building Permit	\$85,448	\$0.90	\$20,761 + \$0.0051333 for each dollar over \$3,000,000 of bldg. valuation
Technology Surcharge	\$6,835.80	\$0.07	8% of Building Permit
Plan Review Fee	\$69,896	\$0.74	\$16,970 + \$0.0042 for each dollar over \$3,000,000
Technology Surcharge	\$5,591.65	\$0.06	8% of Plan Review Fee
Planning Review Fee	\$10,484.35	\$0.11	15% of Plan Review Fee
Planning Inspection Fee	\$1,344	\$0.01	Flat rate; charged when Planning Division performs inspections.
Seismic/Strong Motion	\$4,368	\$0.05	\$0.00028 per \$1 of bldg. valuation
General Plan Recovery Fee	\$40,564	\$0.43	\$2.60 per \$1,000 of bldg. valuation
Green Building/CBSC Fee	\$624	\$0.01	\$1 per \$25,000 of bldg. valuation
Construction Excise Tax	\$124,811	\$1.32	0.008 * valuation of 2002 bldg.
Fire Inspection Fee	\$8,803	\$0.09	\$0.093 x gross square feet
Fire Review Fee	\$136	\$0.00	\$136 per hour, assumes 1 hour review
Subtotal Processing Fees		\$3.79	
City Development Impact Fees			
Adjusted Transportation Development Impact Fee (TDIF)		\$0.85	
Water Development Fee	\$28,942	\$0.31	Assumes 2 2" domestic meters
Water Easement Tap Installation Fee	\$5,146	\$0.05	Assumes 2 2" domestic meter
Water Meter Installation	\$2,056	\$0.02	Assumes 2 2" domestic meter
City Business Operations Tax	\$5,000	\$0.05	\$0.0004 per \$1 of bldg. valuation; Max. \$5,000/year/contractor
Erosion and Sediment Control (ESC)	\$1,000	\$0.01	\$1,000 per permit for building sq. ft. of 50,000 or more
Neighborhood and Community Parks		\$0.39	\$0.39 per sq. ft.
Citywide Parks/Facilities		\$0.16	\$0.16 per sq. ft.
Mixed Income Housing Ordinance/Housing Trust Fund		\$2.62	\$2.62 per unit sq. ft.
Subtotal City Development Impact Fees		\$4.47	
Other Agency Fees			
Twin Rivers USD		\$0.78	\$0.78 per sq. ft. (effective 3/29/2023)
Natomas Basin Habitat Conservation Plan Fee		\$1.57	\$29,815 per acre; assumes land dedication in-lieu (\$15,750/acre), so excludes land acquisition portion of total fee (\$45,565 per acre).
Sacramento Area Flood Control Agency Dev. Impact Fee		\$1.60	\$1.60 per habitable area sq. ft.(effective 1/4/2020).
Sacramento Countywide Transportation Mitigation Fee		\$3.17	\$2,146 per 1000 sq. ft + 2% admin fee
Sacramento Area Sewer District (Expansion)		\$1.18	\$22,360 per net acre for Expansion Area
Regional SAN		\$0.36	\$3,602 per ESD for New Growth Area
Subtotal Other Agency Fees		\$8.66	
North Lake (Greenbriar) Impact Fee [1]			
	<i>per acre</i>		
Transit Fee	\$52,327	\$2.76	
Regional Park Fee	\$18,641	\$0.98	
Fire Facilities Fee	\$5,254	\$0.28	
Community Center Fee	\$10,488	\$0.55	
Subtotal Greenbriar Impact Fee	\$86,710	\$4.58	
Total Fees	\$574,632	\$21.50	

Source: City of Sacramento; various public agencies; EPS.

[1] Shopping Center fees. The Northlake plan area fees include only certain public facility obligations not covered through existing City fees. Because the project was developed by a single master developer, costs for project-related backbone infrastructure were not included.

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Mitigation Monitoring and Reporting Program Woodland Research & Technology Park Specific Plan

State Clearinghouse #2017062042

August 2023



CITY OF
WOODLAND
CALIFORNIA



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MITIGATION MONITORING AND REPORTING PROGRAM

CALIFORNIA ENVIRONMENTAL QUALITY ACT REQUIREMENT

Where a California Environmental Quality Act (CEQA) document has identified significant environmental effects, Public Resources Code Section 21081.6 requires adoption of a “reporting or monitoring program for the changes to the project which it has adopted or made a condition of a project approval to mitigate or avoid significant effects on the environment.”

This Environmental Mitigation Monitoring and Reporting Program (MMRP) has been prepared to provide for the monitoring of mitigation measures required of the Woodland Research & Technology Park (WRTP) Specific Plan (proposed project), as set forth in the Final Environmental Impact Report (EIR).

The City of Woodland (City) is the Lead Agency that must adopt the MMRP for development and operation of the project. This report will be kept on file with the City of Woodland at the Woodland Community Development Department, 300 First Street, Woodland, CA 95695.

The CEQA Statutes and Guidelines provide direction for clarifying and managing the complex relationships between a lead agency and other agencies with implementing and monitoring mitigation measures. In accordance with CEQA Guidelines Section 15097(d), “each agency has the discretion to choose its own approach to monitoring or reporting; and each agency has its own special expertise.” This discretion will be exercised by implementing agencies at the time they undertake any of portion of the project, as identified in the EIR.

PURPOSE OF MITIGATION MONITORING AND REPORTING PROGRAM

The intent of the MMRP is to ensure the effective implementation and enforcement of adopted mitigation measures. The MMRP is intended to be used by City staff and others responsible for project implementation.

This document identifies the individual mitigation measures, the party responsible for monitoring implementation of the measure, the timing of implementation, and space to confirm implementation of the mitigation measures.

ROLES AND RESPONSIBILITIES

The City of Woodland will oversee monitoring and documenting the implementation of mitigation measures. The City or its construction contractor is responsible for fully understanding and effectively implementing all of the mitigation measures contained within this MMRP. Certain mitigation measures also will require that the applicant coordinate or consult with one or more other public agencies in implementing mitigation measures specified herein.

CHANGES TO MITIGATION MEASURES

Any substantive change in the MMRP is required to be reported in writing. Modifications to the mitigation measures may be made by the City, subject to one of the following findings, and documented by evidence included in the public record:

- a. The mitigation measure included in the Final EIR and the MMRP is no longer required because the significant environmental impact identified in the Final EIR has been found not to exist, or to occur at a level which makes

the impact less than significant as a result of changes in the project, changes in environment conditions, or other factors.

OR,

- b. The modified or substitute mitigation measure provides a level of environmental protection equal to, or greater than that afforded by the mitigation measure included in the Final EIR and the MMRP; and,
- c. The modified or substitute mitigation measure or measures do not have significant adverse effects on the environment in addition to, or greater than those which were considered by the responsible hearing parties in their decisions on the Final EIR and the proposed project; and,
- d. The modified or substitute mitigation measures are feasible, and the City, through measures included in the MMRP or other City procedures, can ensure implementation.

SUPPORT DOCUMENTATION

Findings and related documentation supporting the findings involving modifications to mitigation measures shall be maintained in the project file with this MMRP and shall be made available to the public upon request.

This MMRP will be kept on file at:

City of Woodland
Woodland Community Development Department
300 First Street
Woodland, CA 95695
(530) 661-5820

MITIGATION MONITORING AND REPORTING PROGRAM FOR THE WOODLAND RESEARCH AND TECHNOLOGY PARK SPECIFIC PLAN

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
<p>3.3 Air Quality IMPACT 3.3-1. Conflict with or Obstruct Implementation of the Applicable Air Quality Plan.</p>	<p>Mitigation Measure 3.3-1a – Implement Mitigation Measures 3.3-2a through 3.3-2d</p>	<p>Implementation: Project applicant(s) and construction contractor(s). Timing: Prior to construction activities and prior to issuance of a building permit (only for Mitigation Measure 3.3-2d).</p>		
<p>3.3 Air Quality IMPACT 3.3-2. Result in a Cumulatively Considerable Net Increase of Criteria Air Pollutant and Precursor Emissions.</p>	<p>Mitigation Measure 3.3-2a – Implement Construction Best Management Practices. New development shall incorporate the following construction best management practices, those included in an updated set of mitigation recommendations prepared by the YSAQMD [Yolo Solano Air Quality Management District], or those determined by the City to be as effective:</p> <ul style="list-style-type: none"> a. Water all active construction areas at least twice daily. b. Haul trucks shall maintain at least two feet of freeboard. c. Cover all trucks hauling soil, sand, and other loose materials. d. Apply non-toxic binders (e.g., latex acrylic copolymer) to exposed areas after cut-and-fill operations and hydroseed area. e. Apply chemical soil stabilizers on inactive construction areas (disturbed lands within construction projects that are unused for at least four consecutive days). f. Plant vegetative ground cover in disturbed areas as soon as possible. g. Cover inactive storage piles. h. Sweep streets if visible soil material is carried out from the construction site. i. Treat accesses to a distance of 100 feet from the paved road with a 6-inch layer of gravel. j. Limit all idling of vehicles and equipment that use gasoline or diesel fuel to five minutes maximum. 	<p>Implementation: Project applicant(s) and construction contractor(s). Timing: During construction activities.</p>		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
	k. Use alternative power source, such as electricity, for construction equipment or use reformulated and emulsified fuels, incorporate catalyst and filtration technologies, and generally modernize the equipment fleet with cleaner and newer engines.			
<p>3.3 Air Quality IMPACT 3.3-2. (Continued) Result in a Cumulatively Considerable Net Increase of Criteria Air Pollutant and Precursor Emissions.</p>	<p>Mitigation Measure 3.3-2b: Construction-Related Mobile Emissions Reductions for NO_x and PM₁₀ Emissions. Construction contractors shall adhere to the following requirements:</p> <ul style="list-style-type: none"> a. Maintain all construction equipment properly according to manufacturer’s specifications. b. Fuel all off-road and portable diesel-powered equipment with CARB [California Air Resources Board]-certified motor vehicle diesel fuel (non-taxed version suitable for use off-road). c. For all off-road heavy-duty equipment greater than 50 horsepower, utilize equipment that meet or exceed CARB Tier 4 Final standards for off-road heavy-duty diesel engines. d. Requiring off-road construction equipment to be zero-emission, where available, and all diesel-fueled off-road construction equipment, to be equipped with CARB Tier 4 Final engines, and including this requirement in applicable bid documents, purchase orders, and contracts, with successful contractors demonstrating the ability to supply the compliant construction equipment for use prior to any ground-disturbing and construction activities. e. Prohibiting off-road diesel-powered equipment from being in the “on” position for more than 10 hours per day. f. Requiring on-road heavy-duty haul trucks to be model year 2010 or newer if diesel-fueled. g. Providing electrical hook ups to the power grid, rather than use of diesel-fueled generators, for electric construction tools, such as saws, drills and compressors, and using electric tools whenever feasible. h. Limiting the amount of daily grading disturbance area. Prohibiting grading on days with an Air Quality Index forecast of greater than 100 for particulates or ozone for the project area. i. Forbidding trucks from idling for more than two minutes and requiring operators to turn off engines when not in use. j. Keeping onsite and furnishing to the lead agency or other regulators upon request, all equipment maintenance records and data sheets, including design specifications and emission control tier classifications. k. Conducting an on-site inspection to verify compliance with construction mitigation and to identify other opportunities to further reduce construction impacts. 	<p>Implementation: Project applicant(s) and construction contractor(s). Timing: During construction activities.</p>		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
	1. Providing meal options onsite or shuttles between the facility and nearby meal destinations for construction employees.			
3.3 Air Quality IMPACT 3.3-2. (Continued) Result in a Cumulatively Considerable Net Increase of Criteria Air Pollutant and Precursor Emissions.	Mitigation Measure 3.3-2c: Require the Use of Ultra-Low VOC (10 g/L [grams per liter] or less) Architectural Coatings for Construction-related Application. Construction contractors shall be required to use architectural coatings that are ultra-low VOC (10 g/L or less) in all possible applications. These products are identified by manufacturers as “super-compliant.” For construction-related applications, the product manufacturer, product name, product code, and intended use shall be identified on the construction design drawings for approval prior to the issuance of a building permit.	Implementation: Project applicant(s) and construction contractor(s). Timing: During construction activities.		
3.3 Air Quality IMPACT 3.3-2. (Continued) Result in a Cumulatively Considerable Net Increase of Criteria Air Pollutant and Precursor Emissions.	Mitigation Measure 3.3-2d: Ban Wood-burning Stoves and Fireplaces in New Development. Wood burning or pellet stoves and fireplaces shall not be permitted. Natural gas or propane fired fireplaces shall be clearly delineated on plans submitted to obtain building permits.	Implementation: Leaseholder(s) and developer(s). Timing: Demonstrate compliance prior to issuance of building permit.		
3.3 Air Quality IMPACT 3.3-3. Expose Sensitive Receptors to Substantial Pollutant Concentrations.	Mitigation Measure 3.3-3a: Implement Mitigation Measure 3.3-2b - Construction-Related Mobile Emissions Reductions for NO_x and PM₁₀ emissions.	Implementation: Project applicant(s) and contractor(s). Timing: During construction activities.		
3.3 Air Quality IMPACT 3.3-3. (Continued) Expose Sensitive Receptors to Substantial Pollutant Concentrations.	Mitigation Measure 3.3-3b: Implement Guidelines in the California Air Resources Board’s Air Quality and Land Use Handbook: A Community Health Perspective, and subsequent Technical Advisory. New development that would result in substantial TAC [toxic air contaminant] emissions directly or indirectly (e.g., industrial sources) or that would expose sensitive receptors to substantial TAC concentrations (e.g., residential land uses located near existing TAC sources) shall implement CARB’s [2005] Air Quality and Land Use Handbook: A Community Health Perspective (Handbook) guidance concerning land use compatibility	Implementation: Leaseholder(s) and developer(s). Timing: Demonstrate compliance prior to issuance of a grading permit.		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
	with regard to sources of TAC emissions, or CARB guidance as it may be updated in the future.			
<p>3.3 Air Quality IMPACT 3.3-3. (Continued) Expose Sensitive Receptors to Substantial Pollutant Concentrations.</p>	<p>Mitigation Measure 3.3-3c: Conduct Project-Level Analysis and Implement Mitigation for Sources of TACs.</p> <p>For projects with the potential to generate substantial TAC emissions or expose sensitive receptors to substantial TAC pollutant concentrations, the City will require a site-specific analysis for construction and/or operational activities, and appropriate mitigation, as necessary, to ensure that sensitive receptors are not exposed to substantial pollutant concentrations. In communication with the YSAQMD, the City will require, if necessary, a site-specific analysis for operational activities to determine whether health risks attributable to future proposed projects in relation to proposed, planned, and/or existing sensitive receptors would exceed applicable thresholds of significance. Site-specific analysis may include screen level analysis, dispersion modeling, and/or a health risk assessment, consistent with applicable guidance from the YSAQMD. Analyses shall take into account regulatory requirements for proposed uses.</p> <p>The City will require the project applicant(s) to identify and implement feasible mitigation measures to reduce any potentially significant effect and communicate with the YSAQMD to identify measures to reduce exposure of sensitive receptors to substantial pollutant concentrations to levels consistent with thresholds recommended by the YSAQMD applicable at the time the project is proposed. If the YSAQMD does not have applicable thresholds at the time of this analysis, the thresholds will be a probability of contracting cancer for the Maximally Exposed Individual equal to 10.0 in a million or more attributable to the project, or a non-cancer risk of 1.0 Hazard Index (chronic or acute) or more attributable to the project. If the project would exceed applicable thresholds recommended by the YSAQMD or the substitute thresholds outlined above, mitigation will be required to reduce the impact to a less-than-significant level. Agreed upon feasible mitigation actions shall be documented as a project condition of approval.</p> <p>If the results of analysis for the operational activities of any future development project within the WRTP Specific Plan Area determine that the performance standard for this mitigation would be exceeded, actions shall be taken to reduce potential operational impacts which may include, but not necessarily be limited to:</p> <ul style="list-style-type: none"> • locating air intakes and designing windows to reduce particulate matter exposure by, for example, not allowing windows facing the source to open; • providing electrification hook-ups for TRUs to avoid diesel-fueled TRUs continuing to operate at loading docks during loading and unloading operations; • requiring the TAC-generating activity (e.g., loading docks) be located away from sensitive receptors; 	<p>Implementation: <i>For Construction:</i> Project applicant(s) and contractor(s). <i>For Operations:</i> Leaseholder(s) and developer(s).</p> <p>Timing: <i>For Construction:</i> Prior to and during construction. <i>For Operations:</i> Demonstrate compliance with design features prior to issuance of a grading permit. Demonstrate compliance with operational ongoing operational actions prior to issuance of a building permit and maintain and provide records of compliance for operational-related measures throughout operations.</p>		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
	<ul style="list-style-type: none"> • incorporating exhaust emission controls on mobile and/or stationary sources (e.g., filters, oxidizers); • develop and implement a dock management system at the time of occupancy to minimize on-site idling below regulatory limits; • require all on-site user owned and operated trucks with transportation refrigeration units to be capable of plugging into power at loading docks and require plug-in when at the loading dock; • utilize on-site cargo and material handling equipment that is the lowest emitting equipment available at the time of occupancy; • evaluate the potential to electrify a portion of entirety of an on-site user-owned and operated truck fleet; • evaluate the potential to consolidate delivery or haul truck trips to increase the load and decrease vehicle trips; • provide building air filtration units with a Minimum Efficiency Reporting Value (MERV) that is adequate to address adjacent sensitive land uses according to performance standards of this mitigation measure; • ensure adequate distance between existing and planned sensitive receptors and gasoline dispensing facilities, based on the proposed size and design of any gasoline-dispensing facilities; • utilize vegetated buffers between substantial TAC-generating source locations and sensitive receptors. <p>If analysis demonstrates that construction activities associated with development of on-site WRTP Specific Plan land uses or off-site improvement components would exceed the performance standards identified in this mitigation measure, actions shall be taken to reduce potential construction-related impacts which may include, but not necessarily be limited to:</p> <ul style="list-style-type: none"> • installing diesel particulate filters or implementing other CARB-verified diesel emission control strategies on all construction equipment to reduce diesel PM emissions; • using equipment during time when receptors are not present (e.g., when school is not in session or during non-school hours, or when office buildings are unoccupied); • establishing staging areas for the construction equipment that are as far as possible from sensitive receptors; • rerouting construction trucks away from congested streets or sensitive receptor areas; • communicating requirements through daily kick-off meetings and signage that off-road diesel equipment operators shut down their engines rather than idle for more than five minutes; 			

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
	<ul style="list-style-type: none"> • documenting that all off-road equipment is compliant with the CARB in-use off-road diesel vehicle regulation; • establishing an electrical supply to the construction site and use electric-powered equipment instead of diesel-powered equipment or generators, where feasible; • using haul trucks with on-road engines instead of off-road engines; • equipping nearby buildings with High Efficiency Particle Arresting (HEPA) filters systems at all mechanical air intake points to the building to reduce the levels of diesel PM that enter buildings; • planning construction phasing so that future construction activities continue to move further away from occupied land uses; and • planning construction phasing to complete mass site grading, which typically generates the largest portion of diesel PM emissions, prior to occupancy of the project site. 			
<p>3.3 Air Quality IMPACT 3.3-4. Generation of Other Emissions (Such as Those Leading to Odors) Adversely Affecting a Substantial Number of People.</p>	<p>Mitigation Measure 3.3-4: Reduce Exposure of Sensitive Receptors to Odorous Emissions. The City of Woodland shall require, as part of plans for development within the WRTP Specific Plan Area, the implementation of strategies to avoid exposure of sensitive receptors to objectionable odors:</p> <ol style="list-style-type: none"> a. Project applicant(s) for residential development in areas adjacent to ongoing agricultural operations shall include a disclosure clause advising buyers and tenants of the potential adverse odor impacts in the deeds to all residential properties. Residential subdivisions shall provide notification to buyers in writing of odors associated with existing dairies, agricultural burning, and decay of agricultural waste. b. For existing odor-producing sources, sensitive receptors shall be sited as far away as possible from the existing sources. c. For new project-generated odor-producing sources, sensitive receptors shall be sited as far away as possible from the new sources. d. Apply SMAQMD [Sacramento Metropolitan Air Quality Management District] Recommended Odor Screening Distances in the siting of land uses. e. As an alternative to these buffer distances, indoor air filtration systems could be implemented to reduce exposure to odors. For odor-producing sources, activities would be maintained within an enclosed space and appropriate air filtration systems would be implemented to reduce odors expelled from the building. For developments that would host sensitive receptors, design would include air site layout, landscaping, and indoor air filtration systems to minimize exposure to odors. 	<p>Implementation: Project applicant(s). Timing: Demonstrate compliance prior to issuance of a building permit.</p>		
<p>3.4 Biological Resources</p>	<p>Mitigation for impacts to nesting common raptors is included under Mitigation Measure 3.4-2a (Avoid Direct Loss of Protected Bird Nests).</p>	<p>Implementation:</p>		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
<p>IMPACT 3.4-1. Impacts on Special-Status Species: Swainson’s Hawk, White-tailed Kite, and Burrowing Owl.</p>	<p>Mitigation Measure 3.4-1a: Minimize Take and Adverse Effects on Habitat of Swainson’s Hawk and White-Tailed Kite</p> <p>a. In accordance with AMM [Avoidance and Minimization Measure] 16 of the Yolo HCP/NCCP [Habitat Conservation Plan / Natural Community Conservation Plan], the City will require project proponent/s to retain a qualified biologist to conduct species-specific surveys and identify any nesting habitat present within 1,320 feet of the footprint of a proposed project, prior to any ground disturbing activities necessary to implement proposed development and infrastructure projects. Adjacent parcels under different land ownership will be surveyed only if access is granted or if the parcels are visible from authorized areas.</p> <p>b. If a construction project cannot avoid potential nest trees (as determined by the qualified biologist) by 1,320 feet, the City will require project proponent/s to retain a qualified biologist to conduct preconstruction surveys for active nests consistent with guidelines provided by the Swainson’s Hawk Technical Advisory Committee (2000), between March 1 and August 30, within 15 days prior to the beginning of the construction activity. The results of the survey will be submitted to the Yolo Habitat Conservancy and CDFW [California Department of Fish and Wildlife]. If active nests are found during preconstruction surveys, a 1,320-foot initial temporary nest disturbance buffer shall be established. If project-related activities within the temporary nest disturbance buffer are determined to be necessary during the nesting season, then the qualified biologist will monitor the nest and will, along with the City, consult with CDFW to determine the best course of action necessary to avoid nest abandonment or take of individuals. Work may be allowed only to proceed within the temporary nest disturbance buffer if Swainson’s hawk or white-tailed kite are not exhibiting agitated behavior, such as defensive flights at intruders, getting up from a brooding position, or flying off the nest, and only with the agreement of CDFW. The designated on-site biologist/monitor shall be on-site daily while construction-related activities are taking place within the 1,320-foot buffer and shall have the authority to stop work if raptors are exhibiting agitated behavior. Up to 20 Swainson’s hawk nest trees (documented nesting within the last 5 years) may be removed during the permit term, but they must be removed when not occupied by Swainson’s hawks.</p> <p>c. For covered activities that involve pruning or removal of a potential Swainson’s hawk or white-tailed kite nest tree, the City will require project proponent/s to conduct preconstruction surveys that are consistent with the guidelines provided by the Swainson’s Hawk Technical Advisory Committee (2000). If active nests are found during preconstruction surveys, no tree pruning or removal of the nest tree will occur during the period between March 1 and August 30 within 1,320 feet of an active nest, unless a qualified biologist determines that the young have fledged and the nest is no longer active.</p>	<p>Project applicant(s) and contractor(s).</p> <p>Timing: Prior to and during construction activities.</p>		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed																
<p>3.4 Biological Resources IMPACT 3.4-1. (Continued) Impacts on Special-Status Species: Swainson’s Hawk, White-tailed Kite, and Burrowing Owl.</p>	<p>Mitigation Measure 3.4-1b: Comply with Yolo HCP/NCCP Requirements for Compensation for Loss of Swainson’s Hawk Foraging Habitat</p> <p>Before any ground-disturbing activities, the City will require project proponent/s to identify and quantify (in acres) Swainson’s hawk habitat (as defined in the Yolo County HCP/NCCP Appendix A, Covered Species Accounts [Yolo HCP/NCCP 2018]) in and within 1,320 feet of a project footprint. The City will require project proponent/s to submit the Yolo HCP/NCCP <i>Application Form</i> for non-member agency projects and <i>Member Agency Reporting Form</i> for member agency projects, as applicable, and will pay applicable fees to the Yolo Habitat Conservancy as specified in the appropriate form.</p>	<p>Implementation: Project applicant(s) and contractor(s). Timing: Prior to and during construction activities.</p>																		
<p>3.4 Biological Resources IMPACT 3.4-1. (Continued) Impacts on Special-Status Species: Swainson’s Hawk, White-tailed Kite, and Burrowing Owl.</p>	<p>Mitigation Measure 3.4-1c: Minimize Take and Adverse Effects on Western Burrowing Owl</p> <p>Suitable habitat for the western burrowing owl is present within the WRTP Specific Plan Area and the Caltrans off-site improvement area. There is no suitable habitat for burrowing owl in the South Regional Pond off-site improvement area. In accordance with AMM18 of the Yolo HCP/NCCP, the City will require project proponent/s to retain a qualified biologist to conduct species-specific surveys within 30-days but no less than 14 days prior to any ground disturbing activities necessary to implement proposed development and infrastructure projects, consistent with Appendix L of the Yolo HCP/NCCP, which follows CDFW guidelines.</p> <p>If burrowing owls are identified during the species-specific pre-project survey, the City will require project proponent/s to minimize activities that will affect occupied habitat, as follows. Occupied habitat is considered fully avoided if the project footprint does not impinge on a non-disturbance buffer around the suitable burrow. For occupied burrowing owl nest burrows, this non-disturbance buffer could range from 150 to 1,500 feet (Table 4-2 of the Yolo HCP/NCCP, Recommended Restricted Activity Dates and Setback Distances by Level of Disturbance for Burrowing Owls), depending on the time of year and the level of disturbance, based on current guidelines. A copy of this table is provided below in Section 3.4 of this EIR, as Table 3.4-8.</p> <p>Table 3.4-8. Recommended Restricted Activity Dates and Setback Distances by Level of Disturbance for Burrowing Owls (Yolo HCP/NCCP 2018)</p> <table border="1" data-bbox="394 1219 1344 1409"> <thead> <tr> <th data-bbox="394 1219 856 1284">Time of Year</th> <th data-bbox="856 1219 1020 1284">Low (Feet)</th> <th data-bbox="1020 1219 1184 1284">Medium (Feet)</th> <th data-bbox="1184 1219 1344 1284">High (Feet)</th> </tr> </thead> <tbody> <tr> <td data-bbox="394 1284 856 1325">April 1 – April 15</td> <td data-bbox="856 1284 1020 1325">600</td> <td data-bbox="1020 1284 1184 1325">1,500</td> <td data-bbox="1184 1284 1344 1325">1,500</td> </tr> <tr> <td data-bbox="394 1325 856 1365">August 16 – October 15</td> <td data-bbox="856 1325 1020 1365">600</td> <td data-bbox="1020 1325 1184 1365">600</td> <td data-bbox="1184 1325 1344 1365">1,500</td> </tr> <tr> <td data-bbox="394 1365 856 1409">October 16 – March 31</td> <td data-bbox="856 1365 1020 1409">150</td> <td data-bbox="1020 1365 1184 1409">300</td> <td data-bbox="1184 1365 1344 1409">1,500</td> </tr> </tbody> </table> <p>Source: Yolo Habitat Conservancy 2018</p>	Time of Year	Low (Feet)	Medium (Feet)	High (Feet)	April 1 – April 15	600	1,500	1,500	August 16 – October 15	600	600	1,500	October 16 – March 31	150	300	1,500	<p>Implementation: Project applicant(s) and contractor(s). Timing: Prior to and during construction activities.</p>		
Time of Year	Low (Feet)	Medium (Feet)	High (Feet)																	
April 1 – April 15	600	1,500	1,500																	
August 16 – October 15	600	600	1,500																	
October 16 – March 31	150	300	1,500																	

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
	<p>The Yolo HCP/NCCP generally defines low, medium, and high levels of disturbances of burrowing owls as follows.</p> <ul style="list-style-type: none"> • Low: Typically 71-80 dB, generally characterized by the presence of passenger vehicles, small gas-powered engines (e.g., lawn mowers, small chain saws, portable generators), and high tension power lines. Includes electric hand tools (except circular saws, impact wrenches and similar). Management and enhancement activities would typically fall under this category. Human activity in the immediate vicinity of burrowing owls would also constitute a low level of disturbance, regardless of the noise levels. • Moderate: Typically 81-90 dB, and would include medium- and large-sized construction equipment, such as backhoes, front end loaders, large pumps and generators, road graders, dozers, dump trucks, drill rigs, and other moderate to large diesel engines. Also includes power saws, large chainsaws, pneumatic drills and impact wrenches, and large gasoline-powered tools. Construction activities would normally fall under this category. • High: Typically 91-100 dB, and is generally characterized by impacting devices, jackhammers, compression (“jake”) brakes on large trucks, and trains. This category includes both vibratory and impact pile drivers (smaller steel or wood piles) such as used to install piles and guard rails, and large pneumatic tools such as chipping machines. It may also include large diesel and gasoline engines, especially if in concert with other impacting devices. Felling of large trees (defined as dominant or subdominant trees in mature forests), truck horns, yarding tower whistles, and muffled or underground explosives are also included. Very few covered activities are expected to fall under this category, but some construction activities may result in this level of disturbance. <p>In accordance with AMM18 of the Yolo HCP/NCCP, the project proponent may qualify for a reduced buffer size, based on existing vegetation, human development, and land use, if agreed upon by CDFW and USFWS (Yolo Habitat Conservancy 2018).</p> <p>If the project does not fully avoid direct and indirect effects on nesting sites (i.e., if the project cannot adhere to the buffers described above), the City will require the project proponent/s to retain a qualified biologist to conduct preconstruction surveys and document the presence or absence of western burrowing owls that could be affected by the covered activity. Prior to any ground disturbance related to covered activities, the qualified biologist will conduct the preconstruction surveys within three days prior to ground disturbance in areas identified in the planning-level surveys carried out in preparation of this EIR as having suitable burrowing owl burrows, consistent with CDFW preconstruction survey guidelines (Appendix L of the Yolo HCP/NCCP, <i>Take Avoidance Surveys</i>) (Yolo Habitat Conservancy 2018). The qualified biologist will</p>			

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
	<p>conduct the preconstruction surveys three days prior to ground disturbance. Time lapses between ground disturbing activities will trigger subsequent surveys prior to ground disturbance. If the biologist finds the site to be occupied by western burrowing owls during the breeding season (February 1 to August 31), the City will require project proponent/s to avoid all nest sites, based on the buffer distances described above, during the remainder of the breeding season or while the nest is occupied by adults or young (occupation includes individuals or family groups that forage on or near the site following fledging; occupancy of burrowing owl habitat during preconstruction surveys is confirmed at a site when at least one burrowing owl or sign (fresh whitewash, fresh pellets, feathers, or nest ornamentation) is observed at or near a burrow entrance). Construction may occur inside of the disturbance buffer during the breeding season if the nest is not disturbed and the project proponent develops an AMM plan that is approved by the Conservancy, CDFW, and USFWS prior to project construction, based on the following criteria:</p> <ul style="list-style-type: none"> • The Conservancy, CDFW, and USFWS approves the AMM plan provided by the project proponent. • A qualified biologist monitors the owls for at least three days prior to construction to determine baseline nesting and foraging behavior (i.e., behavior without construction). • The same qualified biologist monitors the owls during construction and finds no change in owl nesting and foraging behavior in response to construction activities. • If the qualified biologist identifies a change in owl nesting and foraging behavior as a result of construction activities, the qualified biologist will have the authority to stop all construction-related activities within the non-disturbance buffers described above. The qualified biologist will report this information to the Conservancy, CDFW, and USFWS within 24 hours, and the Conservancy will require that these activities immediately cease within the non-disturbance buffer. Construction cannot resume within the buffer until the adults and juveniles from the occupied burrows have moved out of the project site, and the Conservancy, CDFW, and USFWS agree. If monitoring indicates that the nest is abandoned prior to the end of nesting season and the burrow is no longer in use by owls, the project proponent may remove the nondisturbance buffer, only with concurrence from CDFW and USFWS. If the burrow cannot be avoided by construction activity, the biologist will excavate and collapse the burrow in accordance with CDFW's 2012 guidelines to prevent reoccupation after receiving approval from the wildlife agencies. If evidence of western burrowing owl is detected outside the breeding season (December 1 to January 31), the City will require the project proponent/s to establish a non-disturbance buffer around occupied burrows, consistent with Table 4-2 of the Yolo HCP/NCCP (Yolo Habitat Conservancy 2018), as determined by a qualified biologist. Construction activities within the disturbance buffer are allowed if the following criteria are met to prevent owls from abandoning 			

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
	<p>important overwintering sites: A qualified biologist monitors the owls for at least three days prior to construction to determine baseline foraging behavior (i.e., behavior without construction). The same qualified biologist monitors the owls during construction and finds no change in owl foraging behavior in response to construction activities.</p> <ul style="list-style-type: none"> If there is any change in owl roosting and foraging behavior as a result of construction activities, these activities will cease within the buffer. If the owls are gone for at least one week, the project proponent may request approval from the Conservancy, CDFW, and USFWS for a qualified biologist to excavate and collapse usable burrows to prevent owls from reoccupying the site if the burrow cannot be avoided by construction activities. The qualified biologist will install one-way doors for a 48-hour period prior to collapsing any potentially occupied burrows. After all usable burrows are excavated, the buffer will be removed and construction may continue. <p>Monitoring must continue as described above for the nonbreeding season as long as the burrow remains active. A qualified biologist will monitor the site, consistent with the requirements described above, to ensure that buffers are enforced and owls are not disturbed.</p> <p>If burrowing owls are detected during the nonbreeding season, instead of establishing buffers and monitoring for behavior, the qualified biologist in consultation with the Conservancy may determine that passive relocation (i.e., exclusion) of owls is necessary, in which case the project proponent will develop a burrowing owl exclusion plan in consultation with CDFW biologists. Exclusion and burrow closure will not be conducted during the breeding season for any occupied burrow. The methods will be designed, as described in the species monitoring guidelines (California Department of Fish and Game 2012 [see California Department of Fish and Wildlife 2012]) and consistent with the most up-to-date checklist of passive relocation techniques maintained by the Yolo Habitat Conservancy. This may include the installation of one-way doors in burrow entrances by a qualified biologist during the nonbreeding season. These doors will be in place for 48 hours and monitored twice daily to ensure that the owls have left the burrow, after which time the biologist will collapse the burrow to prevent reoccupation. Burrows will be excavated using hand tools. During excavation, an escape route will be maintained at all times. This may include inserting an artificial structure, such as piping, into the burrow to prevent collapsing until the entire burrow can be excavated and it can be determined that no owls are trapped inside the burrow. The Yolo Habitat Conservancy may allow other methods of passive or active relocation, based on best available science, if approved by the wildlife agencies. Artificial burrows will be constructed prior to exclusion and will be created less than 300 feet from the existing burrows on lands that are protected as part of the reserve system.</p>			

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
<p>3.4 Biological Resources IMPACT 3.4-2. Impacts on Special-Status Species: Special-status and Migratory Nesting Birds and Raptors.</p>	<p>Mitigation Measure 3.4-2a: Avoid Direct Loss of Protected Bird Nests While not required as mitigation for a significant impact under CEQA, the following would be required for compliance with the MBTA and California Fish and Game Code:</p> <ul style="list-style-type: none"> • To the extent feasible, the City will require that construction activities be carried out during the nonbreeding season (between September 1 and January 31) for protected bird species in this region to avoid and minimize impacts to common migratory nesting birds. • For any ground disturbance activity necessary to implement proposed development and infrastructure projects that would occur during the nesting season (between February 1 and August 31), the City will require the project applicant to conduct a preconstruction survey. The preconstruction survey shall be conducted by a qualified biologist before any activity occurring within 500 feet of suitable nesting habitat for any protected bird species. The survey shall be timed to maximize the potential to detect nesting birds, and should be repeated within 10 days of the start of project-related activity. • If an active common bird species protected by the Migratory Bird Treaty Act or California Fish and Game Code nest is found, the qualified biologist shall establish a buffer around the nest. No project activity shall commence within the buffer area until a qualified biologist confirms that the nest is no longer active. The size of the buffer shall be determined in consultation with CDFW. Buffer size is anticipated to range from 50 to 500 feet, depending on the nature of the project activity, the extent of existing disturbance in the area, and other relevant circumstances as determined by a qualified biologist in consultation with CDFW. • Monitoring of all protected nests by a qualified biologist during construction activities will be required if the activity has potential to adversely affect the nest. If construction activities cause the nesting bird to vocalize, make defensive flights at intruders, get up from a brooding position, or fly off the nest, then the no-disturbance buffer shall be increased until the agitated behavior ceases. The exclusionary buffer will remain in place until the chicks have fledged or as otherwise determined by a qualified biologist. 	<p>Implementation: Project applicant(s) and contractor(s). Timing: Prior to and during construction activities.</p>		
<p>3.4 Biological Resources IMPACT 3.4-2. (Continued) Impacts on Special-Status Species: Special-status and Migratory Nesting Birds and Raptors.</p>	<p>Mitigation Measure 3.4-2b: Implement Mitigation Measure 3.4-1b</p>	<p>Implementation: Project applicant(s) and contractor(s). Timing: Prior to and during construction activities.</p>		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
<p>3.4 Biological Resources IMPACT 3.4-3. Impacts on Special-Status Species: Loss of Valley Elderberry Longhorn Beetle (VELB) Larvae and Habitat.</p>	<p>Mitigation Measure 3.4-3: Minimize Take and Adverse Effects on Habitat of Valley Elderberry Longhorn Beetle</p> <ul style="list-style-type: none"> In accordance with AMM 12 of the Yolo HCP/NCCP, the City will require project proponent/s to retain a qualified biologist who is familiar with valley elderberry longhorn beetle and evidence of its presence (i.e., exit holes in elderberry shrubs) to map all elderberry shrubs in and within 100 feet of a proposed project footprint with stems that are greater than one inch in diameter at ground level during the project design phase. To avoid take of valley elderberry longhorn beetle fully, the City will require project proponent/s to design projects to avoid mapped elderberry shrubs, if feasible. To avoid effects on shrubs, the City will require that project proponent/s maintain a buffer of at least 100 feet from any elderberry shrubs with stems greater than one inch in diameter at ground level. AMM1 of the Yolo HCP/NCCP, Establish Buffers, describes that a lesser buffer may be approved by the Conservancy, USFWS, and CDFW if they determine that the covered species is avoided to an extent that is consistent with the project purpose. For elderberry shrubs that cannot be avoided with a designated buffer distance as described above, the qualified biologist will quantify the number of stems one inch or greater in diameter to be affected, and the presence or absence of exit holes. The Conservancy will use this information to determine the number of plants or cuttings to plant on a riparian restoration site to help offset the loss, consistent with Section 6.4.2.4.1 Valley Elderberry Longhorn Beetle, of the Yolo HCP/NCCP (Yolo Habitat Conservancy 2018). Additionally, prior to construction, the City will require that the project proponent/s transplant elderberry shrubs identified within the project footprint that cannot be avoided. Transplantation will only occur if a shrub cannot be avoided and, if indirectly affected, the indirect effects would otherwise result in the death of stems or the entire shrub. If the project proponent/s choose/s, in coordination with a qualified biologist and the City, not to transplant the shrub because the activity would not likely result in death of stems of the shrub, then the qualified biologist will monitor the shrub annually for a five-year monitoring period. The monitoring period may be reduced with concurrence from the wildlife agencies if the latest research and best available information at the time indicates that a shorter monitoring period is warranted. If death of stems at least one inch in diameter occurs within the monitoring period, and the qualified biologist determines that the shrub is sufficiently healthy to transplant, the City will require the project proponent/s to transplant the shrub as described in the following paragraph, in coordination with the qualified biologist. If the shrub dies during the monitoring period, or the qualified biologist determines that the shrub is no longer healthy enough to survive transplanting, then the Conservancy will offset the shrub loss consistent with the preceding paragraph. 	<p>Implementation: Project applicant(s) and contractor(s).</p> <p>Timing: Surveys will take place prior to ground disturbing activities. If required, the project proponent will transplant elderberry plants when the plants are dormant, approximately November through the first two weeks of February, after they have lost their leaves.</p>		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
	<ul style="list-style-type: none"> • The City will require project proponent/s to transplant the shrubs into a location in the HCP/NCCP reserve system that has been approved by the Conservancy. Elderberry shrubs outside the project footprint but within the 100-foot buffer will not be transplanted. Transplanting will follow the following measures: <ol style="list-style-type: none"> a. Monitor: A qualified biologist will be on-site for the duration of the transplanting of the elderberry shrubs to ensure the effects on elderberry shrubs are minimized. b. Timing: The project proponent will transplant elderberry plants when the plants are dormant, approximately November through the first two weeks of February, after they have lost their leaves. Transplanting during the non-growing season will reduce shock to the plant and increase transplantation success. c. Transplantation procedure: Cut the plant back three to six feet from the ground or to 50 percent of its height (whichever is taller) by removing branches and stems above this height. Replant the trunk and stems measuring one inch or greater in diameter. Remove leaves that remain on the plants. d. Relocate plant to approved location in the reserve system, and replant as described in Section 6.4.2.4.1, <i>Valley Elderberry Longhorn Beetle</i> of the Yolo HCP/NCCP (Yolo Habitat Conservancy 2018). 			
<p>3.4 Biological Resources IMPACT 3.4-4. Impacts on Special-Status Species: Loss of Bat Roosts, and Special-status Bats.</p>	<p>Mitigation Measure 3.4-4: Avoid Direct Loss of Bat Roosts and Special-status Bats</p> <p>For any project activity necessary to implement proposed development and infrastructure projects that would require removal of roost habitat (i.e. trees or structures) and would occur during the maternity season (between May 1 and August 31), the City will require the project applicant to conduct a preconstruction survey for special-status bats. Camera inspection as well as an emergence (exit survey with night optics) and/or acoustic survey shall be conducted in the summer prior to construction/land disturbance, which provides the best opportunity to determine if roosting bats are present.</p> <p>If bats are found during the preconstruction survey(s), then removal of roost habitat will be delayed until the end of maternity season (August 31) or until the young are capable of flights, as determined by a qualified bat biologist and in consultation with CDFW. Any removal of highly suitable roost habitat should be conducted during the shoulder season, September 1 to October 31, to avoid harm to the species. If a highly suitable roost tree or structure is to be removed, trees and/or structures surrounding the roost habitat should be removed first, allowing any bats that may be present time to leave the area. A qualified monitor shall be present during removal of the habitat tree or structure.</p>	<p>Implementation: Project applicant(s) and contractor(s).</p> <p>Timing: Prior to construction/land disturbance.</p>		
<p>3.4 Biological Resources</p>	<p>Mitigation Measure 3.4-5: Avoid Loss of and Degradation of Federally Protected Waters</p>	<p>Implementation:</p>		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
<p>IMPACT 3.4-5. Impacts on Wetlands: Loss and Degradation of State or Federally Protected Wetlands.</p>	<ul style="list-style-type: none"> • If the implementation of the WRTP Specific Plan would result in ground disturbance on the agricultural or roadside ditches, the City will require project proponent/s to conduct a delineation of waters of the United States according to U.S. Army Corps of Engineers' methods, and to submit the completed delineation to the U.S. Army Corps of Engineers for jurisdictional determination. • If implementation of the WRTP Specific Plan would result in fill of waters of the United States, the City will require that project proponent/s obtain a Section 404 Clean Water Act permit from the U.S. Army Corps of Engineers and water quality certification from the Regional Water Quality Control Board pursuant to Section 401 of the Clean Water Act. • If implementation of the WRTP Specific Plan involves work in areas containing waters disclaimed by the USACE, the City will require that the applicant obtain a Waste Discharge Requirement permit from the Regional Water Quality Control Board pursuant to the Porter Cologne Act. • The City will require that the applicant obtain all needed permits prior to project implementation, to abide by the conditions of the permits, including all mitigation requirements, and to implement all requirements of the permits in the timeframes required therein. 	<p>Project applicant(s) and contractor(s). Timing: Before any ground disturbing activities for any planned project development in areas within the project footprint containing wetland features or other waters of the United States and on an ongoing basis, as required by the permits herein.</p>		
<p>3.6 Cultural and Tribal Cultural Resources IMPACT 3.6-1. Cause a Substantial Adverse Change in the Significance of Archaeological Resources as defined in CEQA Guidelines Section 15064.5.</p>	<p>Mitigation Measure 3.6-1: Treatment of Unanticipated Archaeological Discoveries Project applicants for future projects proposed under the WRTP Specific Plan would be required to implement the following procedures during any ground-disturbing activities:</p> <ol style="list-style-type: none"> a. Prior to ground-disturbing activities necessary to implement proposed development and infrastructure projects, contractors shall receive cultural resource sensitivity training to identify potential archaeological resources and that all work should cease within 150 feet of prehistoric cultural resources that may be discovered during project implementation. b. During ground-disturbing activities necessary to implement proposed development and infrastructure projects, if any prehistoric or historic subsurface resources are discovered, all work within 150 feet of the resources shall be halted and a qualified archaeologist shall be consulted within 24 hours to assess the significance of the find, according to CEQA Guidelines Section 15064.5, and implement, as applicable, CEQA Guidelines Sections 15064.5(d), (e), and (f). <ul style="list-style-type: none"> • The California Office of Historic preservation utilizes the Secretary of the Interior's Standards and Guidelines for Archeology and Historic Preservation as found in Code of Federal Regulations, 36 CFR [Code of Federal Regulations] Part 61. The minimum professional qualifications in archeology are a graduate degree in archeology, anthropology, or closely related field plus: 1. At least one year of full-time professional experience or equivalent specialized training in archeological 	<p>Implementation: Project applicant(s) and contractor(s). Timing: Prior to and during any construction ground disturbing activities.</p>		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
	<p>research, administration or management; 2. At least four months of supervised field and analytic experience in general North American archeology; and 3. Demonstrated ability to carry research to completion. In addition to these minimum qualifications, a professional in prehistoric archeology shall have at least one year of full-time professional experience at a supervisory level in the study of archeological resources of the prehistoric period. A professional in historic archeology shall have at least one year of full-time professional experience at a supervisory level in the study of archeological resources of the historic period.</p> <p>c. If any find is determined to be a unique archaeological resource according to CEQA Guidelines Section 15064.5, representatives from the City and the archaeologist shall meet to determine the appropriate avoidance measures or other appropriate mitigation. Cultural resources shall be recorded on appropriate Department of Parks and Recreation forms, and all significant cultural materials recovered shall be, as necessary and at the discretion of the qualified archaeologist and in consultation with the local Native American community if the discovery is prehistoric in age, subject to scientific analysis, professional curation, and documentation according to professional standards. If it is determined that the proposed development or infrastructure project could damage a historical resource or a unique archaeological resource (as defined pursuant to the CEQA Guidelines), mitigation shall be implemented in accordance with Section 21083.2 of the California Public Resources Code and CEQA Guidelines Section 15126.4, with a preference for preservation in place. Work may proceed on other parts of the project site while mitigation for historical resources or unique archaeological resources is being carried out. Preservation in place may be accomplished by planning construction to avoid the resource; incorporating the resource within open space; capping and covering the resource; or deeding the site into a permanent conservation easement.</p> <p>d. If avoidance is not feasible, the qualified archaeologist shall develop and oversee the execution of a treatment plan. The treatment plan shall include, but shall not be limited to, data recovery procedures based on location and type of archaeological resources discovered and a preparation and submittal of report of findings to the Northwest Information Center of the California Historical Resources Information System. Data recovery shall be designed to recover the significant information the archaeological resource is expected to contain, based on the scientific/historical research questions that are applicable to the resource, what data classes the resource is expected to possess, and how the expected data classes would address the applicable resource questions. Data recovery, in general, should be limited to the portions of the historical property that could be adversely affected by project proponents' actions. Destructive data recovery methods shall not be applied to portions of the archaeological resources if nondestructive methods are practical.</p>			

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
<p>3.6 Cultural and Tribal Cultural Resources IMPACT 3.6-2. Disturb Human Remains, including those Interred Outside of Formal Cemeteries.</p>	<p>Mitigation Measure 3.6-2: Treatment of Human Remains Consistent with Health and Safety Code, Section 7050 through 7052 and Health and Safety Code Section 8010 through 8030, in the event of the accidental discovery or recognition of any human remains in any location other than a dedicated cemetery during construction, the City and contractor/s shall take the following steps:</p> <ul style="list-style-type: none"> (1) No further excavation or disturbance of the project site or any nearby area reasonably suspected to overlie adjacent human remains will occur until: <ul style="list-style-type: none"> (A) the coroner of Yolo County has been contacted to determine that no investigation of the cause of death is required, and (B) if the coroner determines the remains to be Native American: <ul style="list-style-type: none"> 1. the coroner shall contact the Native American Heritage Commission within 24 hours; 2. the Native American Heritage Commission shall identify the person or persons it believes to be the most likely descendant from the deceased Native American; and 3. the most likely descendant may make recommendations to the landowner or the person responsible for the excavation work, for means of treating or disposing of, with appropriate dignity, the human remains and any associated grave goods, as provided in Section 5097.98 of the Public Resources Code; or (2) Where the following conditions occur, the landowner or his or her authorized representative shall rebury the Native American remains and associated grave goods with appropriate dignity on the property in a location not subject to further subsurface disturbance: <ul style="list-style-type: none"> (A) the Native American Heritage Commission is unable to identify a most likely descendant or the most likely descendant fails to make a recommendation within 24 hours after being notified by the commission; (B) the most likely descendant identified fails to make a recommendation; or (C) the landowner or his or her authorized representative rejects the recommendation of the most likely descendant, and mediation by the Native American Heritage Commission fails to provide measures acceptable to the landowner. 	<p>Implementation: Project applicant(s) and contractor(s).</p> <p>Timing: In the event of the accidental discovery or recognition of any human remains in any location other than a dedicated cemetery during construction.</p>		
<p>3.7 Geology, Soils, Minerals and Paleontological Resources</p>	<p>Mitigation Measure 3.7-1: Conduct Construction Personnel Education, Stop Work if Paleontological Resources are Discovered, Assess the Significance of the Find, and Prepare and Implement a Recovery Plan, as Required. To minimize the potential for destruction of, or damage to potentially unique, scientifically important paleontological resources during earth-moving activities, the measures described below shall be implemented by project applicants and contractors for future projects proposed under the WRTP Specific Plan within the Riverbank or</p>	<p>Implementation: Project applicant(s) and contractor(s).</p> <p>Timing: Before and during excavation and other</p>		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
<p>IMPACT 3.7-1. Possible Damage to or Destruction of Unique Paleontological Resources.</p>	<p>Modesto Formations (in the southern portion of the WRTP Specific Plan Area and the proposed South Regional Pond area) before and during construction activities.</p> <ul style="list-style-type: none"> • Prior to the start of earthmoving activities that would disturb 1 acre of land or more within the Riverbank or Modesto Formations (in the southern portion of the WRTP Specific Plan Area and the proposed South Regional Pond area), inform all construction personnel involved with earthmoving activities regarding the possibility of encountering fossils, the appearance and types of fossils likely to be seen during construction, and proper notification procedures should fossils be encountered. This worker training may either be prepared and presented by an experienced field archaeologist at the same time as construction worker education on cultural resources or prepared and presented separately by a qualified paleontologist. • If paleontological resources are discovered during earthmoving activities, the construction crew shall immediately cease work in the vicinity of the find and notify the City of Woodland Community Development Department. Retain a qualified paleontologist to evaluate the resource and prepare a recovery plan. The recovery plan may include, but is not limited to, a field survey, construction monitoring, sampling and data recovery procedures, museum curation for any specimen recovered, and a report of findings. Recommendations in the recovery plan that are determined by the City to be necessary and feasible shall be implemented before construction activities can resume at the site where the paleontological resources were discovered. 	<p>earth disturbing activities.</p>		
<p>3.8 Hazards and Hazardous Materials IMPACT 3.8-1. Create a Significant Hazard to the Public or the Environment through Reasonably Foreseeable Upset and Accident Conditions Involving the Release of Hazardous Materials into the Environment.</p>	<p>Mitigation Measure 3.8-1: Prepare a Remedial Action Plan, and Conduct Phase I and/or II Environmental Site Assessments and Implement Required Measures if Stained or Odiferous Soil is Discovered.</p> <p>To reduce health hazards associated with potential exposure to hazardous substances in the WRTP Specific Plan Area and the off-site South Regional Pond, implement the following measures before the start of ground-disturbing activities in areas of debris piles, pole-mounted transformers, where demolition will occur, and other areas where evidence of hazardous materials contamination is observed or suspected through either obvious or implied evidence (i.e., stained or odorous soil):</p> <ul style="list-style-type: none"> • Prepare a remedial action plan that identifies any necessary remediation activities including excavation and removal of contaminated soils and redistribution of clean fill material at the diesel above-ground storage tank associated with the agricultural well on the East Central Parcel, and other areas within the WRTP Specific Plan Area, if necessary. All above-ground storage tanks shall be removed in accordance with State and local regulations. The remedial action plan shall include measures for the safe transport, use, and disposal of contaminated soil and building debris removed from the project site. During construction, project applicants for future projects proposed under the WRTP Specific Plan and the off-site South Regional Pond shall be required to comply with the remedial action plan and all applicable federal, State, and local laws. 	<p>Implementation: Project applicant(s) and contractor(s). Timing: Before the start of ground-disturbing activities in areas of debris piles, pole-mounted transformers, where demolition will occur, and other areas where evidence of hazardous materials contamination is observed or suspected through either obvious or</p>		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
	<p>The remedial action plan shall outline measures for specific handling and reporting procedures for hazardous materials and disposal of hazardous materials removed from the project site at an appropriate off-site disposal facility.</p> <ul style="list-style-type: none"> • In the event that contaminated groundwater is encountered during site excavation activities, the contractor shall report the contamination to the appropriate regulatory agencies, dewater the excavated area, and treat the contaminated groundwater to remove contaminants before discharge into the sanitary sewer system. • If stained or odiferous soil is discovered during project-related construction activities, project applicants for future projects proposed under the WRTP Specific Plan and the off-site South Regional Pond shall retain a registered environmental assessor to conduct a Phase I ESA, and if necessary, Phase II ESAs and/or other appropriate testing. Recommendations in the Phase I and II ESAs to address any contamination that is found shall be implemented before initiating ground-disturbing activities in these areas. • Notify the appropriate federal, State, and local agencies if evidence of previously undiscovered soil or groundwater contamination (e.g., stained soil, odorous groundwater) or if known or previously undiscovered underground storage tanks are encountered during construction activities. Any contaminated areas shall be remediated in accordance with recommendations made by the EMD, Central Valley RWQCB, DTSC, and/or other appropriate federal, State, or local regulatory agencies. • Retain a licensed contractor to remove all septic systems in accordance with local, State, and federal regulations. • Retain a Cal-OSHA certified Asbestos Consultant before demolition of any buildings in the WRTP Specific Plan Area to investigate whether any asbestos-containing materials or lead-based paints are present, and could become friable or mobile during demolition activities. Provide a copy of the report to YSAQMD. If any materials containing asbestos or lead-based paints are found, they shall be removed by an accredited contractor in accordance with EPA and Cal-OSHA standards as required by YSAQMD. In addition, all activities (construction or demolition) in the vicinity of these materials shall comply with Cal-OSHA asbestos and lead worker construction standards. The materials containing asbestos and lead shall be disposed of properly at an appropriate off-site disposal facility. • Properly close and abandon all on-site groundwater wells in accordance with Yolo County requirements 	implied evidence (i.e., stained or odorous soil).		
3.8 Hazards and Hazardous Materials IMPACT 3.8-2. Emit Hazardous	<p>Mitigation Measure 3.8-2: Notify and Consult with Affected Schools, and Implement a Hazardous Materials Business Plan (if Required).</p> <p>Project applicants for future retail, commercial, or industrial projects proposed under the WRTP Specific Plan and supportive infrastructure improvements that would involve the long-term use of hazardous materials for project operation shall notify the Woodland</p>	Implementation: Project applicant(s) and contractor(s).		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
<p>Emissions or Handle Hazardous or Acutely Hazardous Materials, Substances, or Waste within One-Quarter Mile of an Existing or Proposed School.</p>	<p>Christian School, the Pioneer High School, and the Woodland Joint Unified School District, as appropriate based upon project location relative to school locations, in writing, and shall consult with appropriate school or district personnel about the types of activities that would occur and their estimated timing. Examples of the types of hazardous materials that could be used during proposed operational activities shall be provided. The written notification shall be provided at least 30 days before the commencement of any construction activities.</p> <p>Future businesses within the WRTP Specific Plan Area that handle and/or store a hazardous material or a mixture containing a hazardous material in amounts greater than the specified threshold quantities in Chapter 6.95, Section 25505 of the California Health & Safety Code shall prepare a Hazardous Materials Business Plan. The plan shall provide emergency plans and procedures that the businesses will follow in the event of a release or threatened release of a hazardous material, along with the other requirements of Section 25505 including an inventory of hazardous materials, site plan showing material storage areas and ingress and egress points for emergency vehicles, and employee safety training.</p>	<p>Timing: Written notification shall be provided at least 30 days before the commencement of any construction activities.</p> <p>For operations requiring a Hazardous Materials Business Plan, the plan shall be approved prior to the issuance of a permit to operate.</p>		
<p>3.9 Hydrology, Flooding, and Water Quality IMPACT 3.9-1. Substantially Increase the Rate or Amount of Surface Runoff Resulting in Flooding, Create or Contribute Runoff Water which would Exceed the Capacity of Existing or Planned Stormwater Drainage Systems, Provide Substantial Additional Sources of Polluted Runoff.</p>	<p>Mitigation Measure 3.9-1: Prepare Additional Storm Drainage Analysis for determining Amount of New Development Acreage Beyond the Previously Identified 80 Residential Acres Allowable in the South Urban Growth Area and Submit to the City for Review and Approval.</p> <p>The WRTP shall be required to fund an additional stormwater drainage analysis that utilizes the revised baseline conditions modeling and includes detailed information defining the operational capacity of the newly-installed infrastructure. A model will then be created that incorporates the pump station, detention, and conveyance improvements that have already been constructed, and then incorporates the full buildout of the Spring Lake Specific Plan Development. At that point, the fully developed acreage of the WRTP Specific Plan will be added to determine the new developable acreage (in terms of stormwater drainage) that can be accommodated with current infrastructure. The additional drainage analysis will also be required to determine what additional storm drainage infrastructure is needed to support full buildout of the WRTP Specific Plan. Building permits for development beyond the identified currently developable acreage will only be approved with confirmation that the required storm drainage and water quality treatment infrastructure is in place.</p>	<p>Implementation: Fee requirements pertain to Project Applicant(s)/ Leaseholder(s) / Developer(s)</p> <p>Implementation of the analysis shall be the requirement of the City of Woodland and its contractor</p> <p>Timing: Prior to issuance of building permits for development beyond the identified currently developable acreage [80 residential acres, per Section 3.9 of the Draft EIR].</p>		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
<p>3.11 Noise and Vibration IMPACT 3.11-1. Generation of a Substantial Temporary (Construction-related) Increase in Ambient Noise Levels in the Vicinity of the Project in Excess of Standards Established in the Local General Plan or Noise Ordinance, or Applicable Standards of Other Agencies.</p>	<p>Mitigation Measure 3.11-1– Implement Construction Noise Reduction Strategies</p> <p>a. Demolition, construction, site preparation, and related activities that would generate noise perceptible at the property line of the subject property are limited to the hours between 7:00 A.M. and 6:00 P.M. on Monday through Saturday and between 9:00 A.M. and 6:00 P.M. on Sunday and federal holidays. The building inspector may issue an exception to this limitation on hours in cases of urgent necessity where the public health and safety will not be substantially impaired.</p> <p>b. Idling times for noise-generating equipment used in demolition, construction, site preparation, and related activities shall be minimized either by shutting equipment off when not in use or reducing the maximum idling time to two (2) minutes.</p> <p>c. Where construction work is within 445 feet of properties with existing, occupied noise-sensitive uses, construction shall be prohibited on weekends and holidays and construction should start no earlier than 8 a.m.</p> <p>d. Where construction work is within 445-feet of an existing off-site residence, installation of continuous noise curtains shall be required between the construction site and those residences.</p> <p>e. Demolition, construction, site preparation, and related activities that do not involve pile driving proposed within 445 feet from the edge of properties with existing, occupied noise-sensitive uses shall incorporate all feasible strategies to reduce noise exposure for noise-sensitive uses, including:</p> <ul style="list-style-type: none"> • Provide written notice to all known occupied noise-sensitive uses within 445 feet of the edge of the project site boundary at least 2 weeks prior to the start of each construction phase of the construction schedule; • Ensure that construction equipment is properly maintained and equipped with noise control components, such as mufflers, in accordance with manufacturers’ specifications; • Re-route construction equipment away from adjacent noise-sensitive uses; • Locate noisy construction equipment away from surrounding noise-sensitive uses; • Use sound aprons or temporary noise enclosures around noise-generating equipment; • Position storage of waste materials, earth, and other supplies in a manner that will function as a noise barrier for surrounding noise-sensitive uses; • Use the quietest practical type of equipment; • Use electric powered equipment instead of diesel or gasoline engine powered equipment; • Use shrouding or shielding and intake and exhaust silencers/mufflers; and • Other effective and feasible strategies to reduce construction noise exposure for surrounding noise-sensitive uses. 	<p>Implementation: Project applicant(s) and contractor(s).</p> <p>Timing: Prior to and during any construction ground disturbing activities.</p>		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
	f. For construction of buildings that require the installation of piles, an alternative to installation of piles by hammering shall be used. This could include the use of augured holes for cast-in-place piles, installation through vibration or hydraulic insertion, or another low-noise technique.			
3.11 Noise and Vibration IMPACT 3.11-2. Generation of a Substantial Permanent (Long-term Operations) Increase in Ambient Noise Levels in the Vicinity of the Project in Excess of Standards Established in the Local General Plan or Noise Ordinance, or Applicable Standards of Other Agencies.	Mitigation Measure 3.11-2– Reduce Noise Exposure from Transportation and Non-Transportation Sources Future development within the WRTP Specific Plan Area shall be required to meet allowable outdoor and indoor noise exposure standards. Noise mitigation measures that may be approved to achieve these noise level targets include but are not limited to the following: <ul style="list-style-type: none"> • Construct facades with sound insulation to achieve acceptable interior noise; • Use sound-rated windows for primary sleeping and activity areas; • Use sound-rated doors for all exterior entries at primary sleeping and activity areas; • Use setbacks and/or sound barriers where applicable, feasible, and reasonable; • Use acoustic baffling of vents for chimneys, attic and gable ends; • Install a mechanical ventilation system that provides fresh air under closed window conditions; and • Maximize site design so that buildings shelter outdoor areas 	Implementation: Project applicant(s) and contractor(s). Timing: Demonstrate compliance with noise standards prior to issuance of a grading permit.		
3.11 Noise and Vibration IMPACT 3.11-3. Generation of Vibration.	Mitigation Measure 3.11-3a – Implement Vibration Reduction Measures g. New development that proposes the use of piles for foundations shall include all feasible measures necessary with the goal to ensure that vibration exposure for adjacent buildings is less than 0.5 PPV and less than 80 VdB for adjacent vibration-sensitive uses and less than 0.2 PPV for adjacent historic buildings. These performance standards shall take into account the reduction in vibration exposure that would occur through coupling loss provided by each affected building structure. If it is determined necessary to avoid damage, the project applicant shall coordinate with the Chief Building Official to implement corrective actions, which may include, but is not limited to building protection or stabilization. h. New developments that would generate substantial long-term vibration shall provide analysis and mitigation, as feasible, to achieve velocity levels, as experienced at habitable structures of vibration-sensitive land uses, of less than 80 vibration decibels.	Implementation: Project applicant(s) and contractor(s). Timing: For construction that would require use of piles for foundations, prior to and during construction. For operations that would generate substantial long-term vibration, demonstrate compliance prior to		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
		the issuance of a permit to operate.		
3.11 Noise and Vibration IMPACT 3.11-3. (Continued) Generation of Vibration.	Mitigation Measure 3.11-3b – Implement Mitigation Measure 3.11-1	Implementation: Project applicant(s) and contractor(s). Timing: Prior to and during any construction ground disturbing activities.		
3.13 Transportation and Circulation IMPACT 3.13-1. Conflict with A Program, Plan, Ordinance, or Policy Addressing the Circulation System, including Transit, Roadway, Bicycle, and Pedestrian Facilities.	Mitigation Measure 3.13-1a: The Draft WRTP Specific Plan Finance Plan shall incorporate a Transit Contribution. While not required as mitigation for a significant impact under CEQA, the following would be required for planning purposes to ensure transit equipment, infrastructure, and service is adequately funded to provide necessary service to the WRTP Specific Plan Area: The project applicant shall contribute its fair-share of the cost associated with providing transit service to the WRTP Specific Plan Area. It is anticipated that new transit vehicles may be required to provide additional service to the WRTP Specific Plan Area. However, the final determination of additional capital equipment or other costs shall be determined by the City of Woodland in coordination with YCTD and as identified in the Master TDM/VMT Program. The fair-share cost or a plan for providing the fair-share cost over time shall demonstrate funding is adequate to provide the necessary transit service or range of services required to meet the demand in the WRTP Specific Plan Area, as determined through the WRTP Specific Plans required coordination with YCTD and UC Davis. The funding mechanism(s) for transit and other TDM measures shall be outlined in the WRTP Specific Plan Finance Plan, and development projects shall be required to commit to contributing fair-share costs prior to the issuance of respective building permits by the City of Woodland.	Implementation: Fee requirements pertain to Project Applicant(s)/ Leaseholder(s) / Developer(s) Timing: Demonstrate binding commitment to contributing fair-share costs prior to the issuance of a building permit by the City of Woodland.		
3.13 Transportation and Circulation IMPACT 3.13-1. (Continued) Conflict with A Program, Plan, Ordinance, or Policy Addressing the	Mitigation Measure 3.13-1b: On-site Transit Stops. While not required as mitigation for a significant impact under CEQA, the following would be required for planning purposes to ensure proposed transit infrastructure provides for adequate service to the WRTP Specific Plan Area: The WRTP Specific Plan calls for development of a shared mobility hub in the Village Center. The project applicant shall develop detailed plans, to be reviewed and approved by the City of Woodland and YCTD [Yolo County Transportation Department] and construct the shared mobility hub improvements in the Village Center and identify the	Implementation: Fee requirements pertain to Project Applicant(s)/ Leaseholder(s) / Developer(s) Timing:		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
Circulation System, including Transit, Roadway, Bicycle, and Pedestrian Facilities.	specific locations of sheltered transit stops with bus turnouts at other locations. It is anticipated that other stops would be located near the business park uses north and west of the Village Center. The City of Woodland and YCTD shall approve the location, design, and implementation timing of the sheltered transit stops and bus turnouts prior to the prior to approval of the first final map or as otherwise required by the City. If transit stops are located on-street for segments of roadways that do not have designated curbside on-street parking that can be designated for a bus stop (i.e., only travel lanes, bike lanes), the street cross-sections shall be modified to provide for a curbside bus stop, or multiple stops if needed for bus operations.	Demonstrate binding commitment to contributing fair-share costs prior to the issuance of a building permit by the City of Woodland.		
3.13 Transportation and Circulation IMPACT 3.13-2. Substantially Increase Hazards Due to a Design Feature or Incompatible Uses.	Mitigation Measure 3.13-2: Implement a Construction Traffic Control Plan Prior to any construction activities for the WRTP Specific Plan, the applicant shall prepare a detailed Construction Traffic Control Plan and submit it for review and approval by the City Department of Public Works. The applicant and the City shall consult with Caltrans, Yolobus, and local emergency service providers for their input prior to approving the Plan. The plan shall ensure that acceptable operating conditions on local roadways and freeway facilities are maintained during construction. A copy of the construction traffic control plan shall be submitted to local emergency response agencies and these agencies shall be notified at least 14 days before the commencement of construction that would partially or fully obstruct roadways. At a minimum, the plan shall include: <ul style="list-style-type: none"> • The number of truck trips, time, and day of street closures • Time of day of arrival and departure of trucks • Limitations on the size and type of trucks, provision of a staging area with a limitation on the number of trucks that can be waiting • Provision of a truck circulation pattern • Provision of a driveway access plan so that safe vehicular, pedestrian, and bicycle movements are maintained (e.g., steel plates, minimum distances of open trenches, and private vehicle pick up and drop off areas) • Maintain safe and efficient access routes for emergency vehicles • Maintain safe and efficient access routes for farming equipment and vehicles • Manual traffic control when necessary • Proper advance warning and posted signage concerning street closures • Provisions for pedestrian safety 	Implementation: Project applicant(s) Timing: Plan preparation and submittal to the City prior to any construction activities. A copy of the construction traffic control plan shall be submitted to local emergency response agencies and these agencies shall be notified at least 14 days before the commencement of construction that would partially or fully obstruct roadways.		
3.14 Utilities IMPACT 3.14-1. Increased Demand for Water Supply	Mitigation Measure 3.14-1: Prepare and Submit a Water Supply Conveyance Improvement Plan in Compliance with Applicable Standards and Construct Water Supply Conveyance Infrastructure Prior to Occupancy.	Implementation: Project applicant(s) Timing:		

Impact	Mitigation Measure	Implementation Responsibility/Timing	Compliance Verification	Date Completed
Conveyance Facilities.	<p>While not required as mitigation for a significant impact under CEQA, the following would be required for planning purposes to ensure the water supply infrastructure is designed and sized to provide adequate service to the WRTP Specific Plan:</p> <p>Before approval of the final subdivision map and issuance of building permits, project applicants for projects proposed under the WRTP Specific Plan shall prepare a detailed water conveyance infrastructure improvement plan that depicts the locations and appropriate sizes of all required conveyance infrastructure, in conjunction with other site-specific improvement plans. Proposed on-site water facilities shall be designed and sized to provide adequate service to the project site for the amount of development identified in the tentative subdivision map, based on City of Woodland Engineering Standards. A final water conveyance infrastructure improvement plan shall be approved by the City of Woodland Engineering Division before approval of the final subdivision map by the City of Woodland Planning Division and issuance of building permits from the City of Woodland Building Division. All required infrastructure shall be in place prior to occupancy of development anticipated under the proposed project.</p>	<p>Plan approval to occur Prior to approval of a final subdivision map and issuance of any building permits for a use proposing to connect to public water utilities.</p> <p>All required infrastructure shall be in place prior to occupancy of development for which the plan applies.</p>		
<p>3.14 Utilities IMPACT 3.14-2. Increased Demand for Wastewater Collection and Conveyance Facilities.</p>	<p>Mitigation Measure 3.14-2: Prepare Additional Analysis to Verify the Spring Lake Specific Plan Pump Station Capacity Prior to Development Beyond 87 Percent of the WRTP Specific Plan Area.</p> <p>While not required as mitigation for a significant impact under CEQA, the following would be required for planning purposes to ensure the existing wastewater conveyance infrastructure has the capacity to provide adequate service to the WRTP Specific Plan Area:</p> <p>Prior to any development beyond 87 percent of the WRTP Specific Plan, the WRTP shall fund additional analysis to verify that the Spring Lake Specific Plan Pump Station has adequate capacity to provide for sewer flows from full buildout of the WRTP Specific Plan. If additional capacity is required, it may be provided by upsizing the pumps as part of the City’s regular maintenance work of replacing the pumps. If the increased capacity is not provided by the City’s maintenance work, then the WRTP Specific Plan will be responsible for funding improvements at the pump station to provide the additional required capacity.</p>	<p>Implementation: Fee requirements pertain to Project Applicant(s)/ Leaseholder(s) / Developer(s) Analysis requirements pertain to the City of Woodland and its contractor(s), leaseholders</p> <p>Timing: Prior to issuance of building permits for development beyond 87 percent of the WRTP Specific Plan development square footage.</p>		

Table Notes:

[1] Technical guidance for designing bioretention facilities is available from the Central Coast LID Initiative. The guidance includes design specifications and plant lists appropriate for the Central Coast climate. (<https://www.centralcoastlidi.org/projects.php>)

AMM = Avoidance and Minimization Measure

Cal-OSHA = California Occupational Safety and Health Administration

CARB = California Air Resources Board

CDFW = California Department of Fish and Wildlife

Central Valley RWQCB = Central Valley Regional Water Quality Control Board

CEQA = California Environmental Quality Act

CFR = Code of Federal Regulations

City = City of Woodland

Conservancy = Yolo Habitat Conservance

dB = decibel

DTSC = California Department of Toxic Substances Control

EIR = Environmental Impact Report

EMD = Environmental Management Department

ESA = Environmental Site Assessment

g/L = grams per liter

HCP = Habitat Conservation Plan

HEPA = High Efficiency Particle Arresting

LID = low impact development

MERV = Minimum Efficiency Reporting Value

NCCP = Natural Community Conservation Planning

NO_x = nitrogen oxide

PM = particulate matter

PM₁₀ = particulate matter with aerodynamic diameter less than 10 microns

PPV = peak particle velocity

SMAQMD = Sacramento Metropolitan Air Quality Management District

TAC = toxic air contaminant]

TDM = Transportation Demand Management

TRU = transport refrigeration unit

UC Davis = University of California, Davis

USFWS = U.S. Fish and Wildlife Service

VdB = vibration decibel(s)

VELB = Valley Elderberry Longhorn Beetle

VMT = vehicle miles traveled

VOC = Volatile Organic Compound

WRTP = Woodland Research & Technology Park

YCTD = Yolo County Transportation Department

YSAQMD = Yolo Solano Air Quality Management District

REFERENCES

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- California Department of Fish and Wildlife. 2012 (May). Staff Report on Burrowing Owl Mitigation. State of California Natural Resources Agency. Sacramento, CA.
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- Yolo Habitat Conservancy. 2018. Yolo Habitat Conservation Plan/Natural Community Conservation Plan – Final. Prepared by ICF. Yolo County, California. April 2018.

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